

# City of Franklin



May 2017





SALTBOX CENTER  
INFORMATION

HILTON  
ISLAND



Mayor's Office  
814-437-1670

City Manager's Office  
814-437-1485

Finance Department  
814-437-1922

Community Services Department  
814-437-1430



**City of Franklin**  
430 Thirteenth Street  
Franklin, Pennsylvania 16323  
[www.franklinpa.gov](http://www.franklinpa.gov)

Community Development Dept.  
814-437-7841

Fire Department  
Business 814-432-3780  
Emergency 814-437-2911

Police Department  
Business 814-437-1644  
Emergency 814-437-2911

**RESOLUTION NO. 65**

**A RESOLUTION CLOSING OUT COMMUNITY CONSERVATION PARTNERSHIPS  
GRANT PROJECT BRC-TAG-20-85**

**WHEREAS**, the City of Franklin has prepared a Pedestrian and Bicycle Connection Master Plan for the City of Franklin area; and,

**WHEREAS**, the purpose of the Plan is to improve the overall pedestrian and bicycle network, by focusing on the safe connections of Franklin's business district to its residential district and nearby recreational trails; and

**WHEREAS**, the Plan was financed in part by a Community Conservation Partnerships Program grant under the administration of the Pennsylvania Department of Conservation and Natural Resources, Bureau of Recreation and Conservation, under contract number BRC-TAG-20-85.

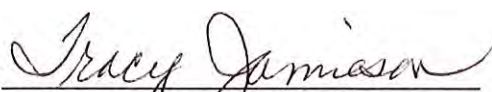
**NOW, THEREFORE, BE IT HEREBY RESOLVED** by the City Council of the City of Franklin that:

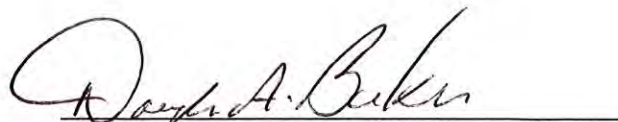
- a. The project was completed in accordance with the Grant Agreement.
- b. All project expenditures have been made and were in accordance with the Grant Agreement.
- c. The Plan and related materials are acceptable to the City of Franklin.
- d. The Plan and related materials will be used to guide future recreation and conservation decisions.

**ADOPTED THIS** 5<sup>th</sup> day of June, 2017, by the

**CITY COUNCIL OF THE CITY OF FRANKLIN**

Attest:

  
Tracy L. Jamieson, City Clerk

  
Douglas A. Baker, Mayor

## Acknowledgments

This project was funded in part by a grant from the Community Conservation Partnership Program (C2P2), administered by the Pennsylvania Department of Conservation and Natural Resources (DCNR), Bureau of Recreation and Conservation.

### Mayor

Douglas A. Baker

### City Council

Donna Fletcher

Jane Dlugos

James Johnson

Samuel Lyons

James Marshall

Ryan Rudegear

### Project Committee

Doug Baker, City of Franklin Mayor

Tracy Jamieson, City of Franklin Manager

Sheila Boughner, City of Franklin Community Development Director

Ronnie Beith, City of Franklin Event Coordinator

Terry Ruditis, City of Franklin Street Department

Mark London, City of Franklin Police Chief

Kevin Saragion, City of Franklin Police Detective

Erin Wiley Moyers, PA DCNR Bureau of Recreation and Conservation

Lyndsie DeVito, PennDOT District 1-0

Kim Mourer, Venango County Planning Commission

Sue Smith, Northwest Commission

Kim Harris, Oil Region Alliance

Caryl Holden, Allegheny Valley Trails

Mike Dulaney, Franklin Area Chamber

Matt Siembida, Central Elementary School Principal

Ron Snyder, Local cycling enthusiast

Jeff Brunton, Local business owner, outdoor recreation

Jeffrey Hall, City resident

This Master Plan was prepared with assistance from:



Mackin Engineering Company  
117 Industry Drive  
Pittsburgh, PA 15275-1015  
[www.mackinengineering.com](http://www.mackinengineering.com)

*Icons made by Freepik from [www.flaticon.com](http://www.flaticon.com)*

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*Liberty Street in the City of Franklin was named one of the "10 Great Streets for 2010" by the American Planning Association (APA) Great Places in America Program*





# Introduction

Roadway corridor design begins with designing for the function of the street. Community streets should be corridors that are comfortable and attractive to everyone, including walkers, cyclists and those driving vehicles. To achieve this, the hierarchy of design must be turned upside down and promote the safety and comfort of the pedestrian first, cyclists second and vehicles third. Such an approach will create a multi-functioning design with traffic calming measures that will reinforce the posted speed limit.

A community corridor serving business and residential districts should provide a comfortable and convenient place to walk, shop, eat, talk and congregate, and inspire the desire to visit again.

The City of Franklin Pedestrian and Bicycle Connection Master Plan ("the Plan") will provide the City of Franklin with the projects and policies necessary to improve the overall pedestrian and bicycle network, with a particular focus on improving the connection between the downtown business district and the nearby recreational trails. The Plan is funded by a grant from the Community Conservation Partnership Program (C2P2), administered by the Pennsylvania Department of Conservation and Natural Resources (DCNR), Bureau of Recreation and Conservation. To facilitate the Plan, the City of Franklin hired Mackin Engineering Company in 2016 as its planning consultant.



*"At the basic level, a livable street is one that feels comfortable to you, it's one that gives you energy."*

- Bruce Appleyard, "Fixing the Great Mistake," a Streetfilms series (source: <http://www.streetfilms.org/revisiting-donald-appleyards-livable-streets/>)

Source: Google Maps



## Background

The City of Franklin is the County Seat of Venango County, Pennsylvania. Nestled along both French Creek and the Allegheny River, as shown in Figure 1 on page 2, it is 4.71 square miles and home to approximately 6,500 residents. Downtown Franklin is located at the intersections of State Route 8, US Route 62 and US Route 322. Located approximately halfway between Pittsburgh and Erie, as shown on Figure 2 on page 3, Franklin is one of five (5) designated Trail Towns along the Erie to Pittsburgh Trail. The Franklin Trailhead is located across the Allegheny River from downtown Franklin, where the Samuel Justus Recreational Trail converges with the Allegheny River Trail.

Elected officials and staff have dedicated a lot of time and effort over the past ten or so years into making Franklin a pedestrian- and bicycle-friendly community. The focus of City leaders is to capitalize on the recreational amenities to increase tourism and boost the local economy.

Figure 1: City of Franklin

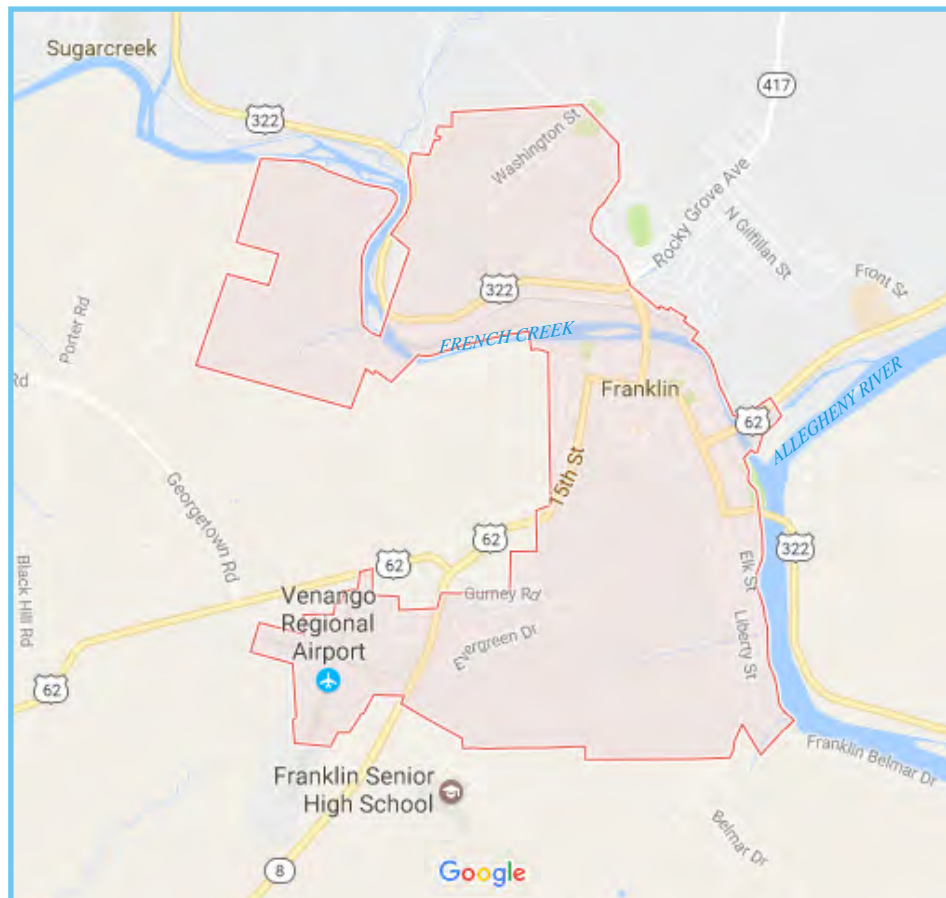




Figure 2: Erie to Pittsburgh Trail Map



## Franklin's Need for a Plan of Action

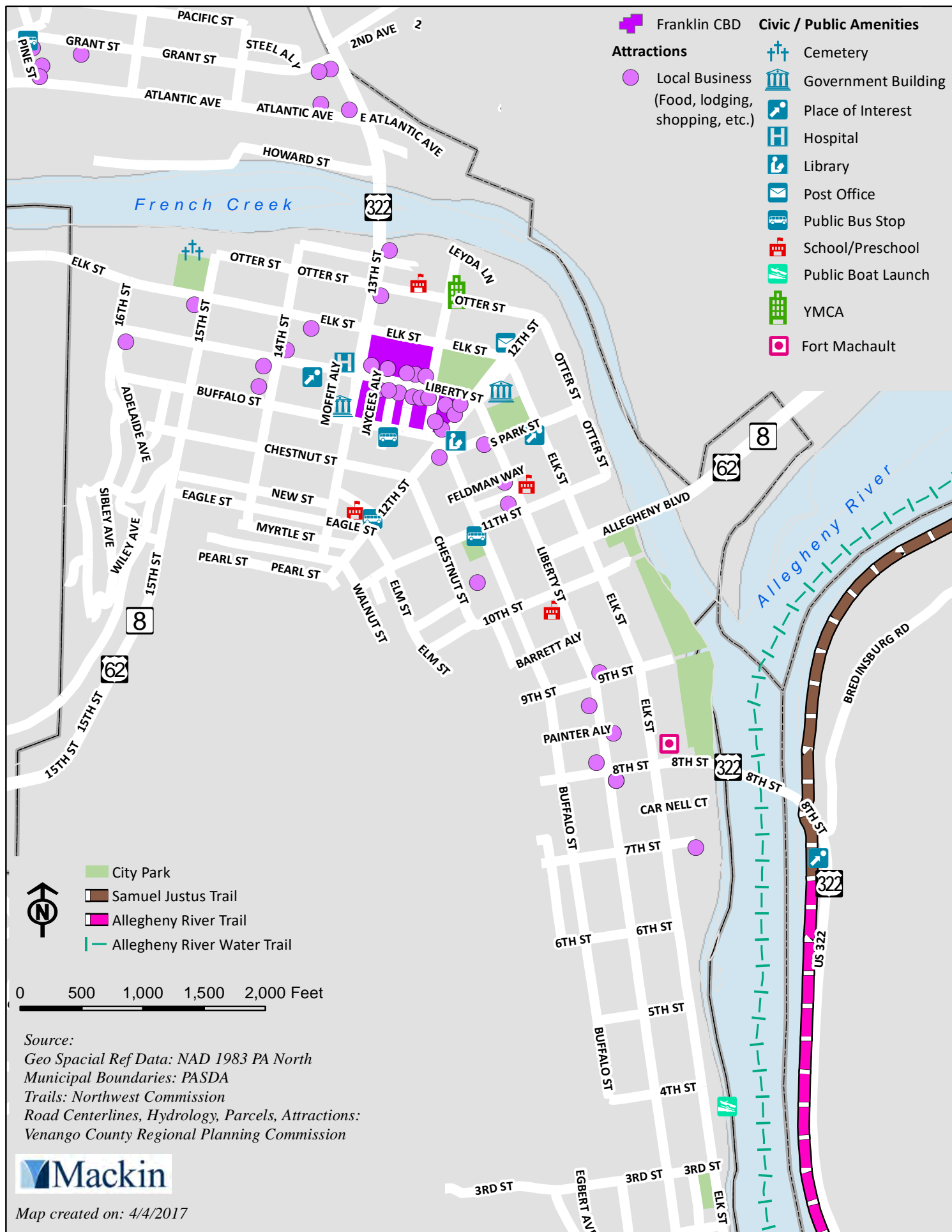
In 2010, Franklin was designated as a Bicycle Friendly Community by the League of American Bicyclists and was re-certified in 2014, attaining the Bronze level status. It was during this process of re-designation that the need for a clear and concise plan of action became evident. In order for Franklin to grow into a more sustainable, livable and healthy community, the City is undertaking this Pedestrian and Bicycle Connection Master Plan in order to determine the following:

- Determine optimum biking and pedestrian routes in the City. While there is a vast sidewalk network, there is not much of a formal bicycle network in the City, despite the Erie to Pittsburgh Trail being located just across the Allegheny River.
- Connect the existing trail system to the City's central business district (CBD) and downtown amenities. As shown on Figure 3 on page 5, the Downtown offers a variety of local and regional attractions, including parks, hotels, bed and breakfasts, eateries, historic sites, shopping and other civic and cultural attractions. Unfortunately, the trail system is located on the other side of the Allegheny River from the downtown and not overly accessible via bicycle or on-foot. This Plan seeks to remedy this so that the local businesses can draw trail users into town.
- Recommend wayfinding improvements to direct pedestrians and bicyclists to local amenities. In addition to physical infrastructure improvements to the pedestrian and bicycle network, there is a real need for improved wayfinding so that people know what is available and how to get there.
- Identify any unmet accommodation and service needs that could be provided by local businesses. In addition to the infrastructure and wayfinding, this Plan seeks to identify other amenities and services that may be warranted to support the City's goal of being a destination for bicyclists and other trail users.
- Prioritize realistic public improvements that will enhance the overall connectivity of the City. Ultimately, this Plan will provide the City with a blueprint for improving the overall connections that identifies the high priority projects, that are publicly supported, and cost estimates for completion.





Figure 3: Map of Existing Amenities in Downtown Franklin



## Previous Plans

There have been a number of previous planning efforts that have gotten the City of Franklin to this point in terms of bicycle and pedestrian connectivity. A summary of these plans is included to provide not only a background on what has already happened in the City, but also to identify relevant recommendations that served as a starting point for this Plan.

### *Liberty Street: A Great Street*

Liberty Street serves as the City's Main Street, which was designated in 2010 as one of ten Great Streets, under the American Planning Association (APA) Great Places in America Program. A Great Street comprises the entire three-dimensional visual corridor, including the public realm and how it relates to the adjacent land uses. Special emphasis is placed on streets that are complete, that is, streets that service and take into account all users — not just motor vehicles (<https://www.planning.org/greatplaces/streets/characteristics.htm>).



Liberty Street is characterized by its brick and stone buildings and features the 1867 twin-spire Italianate Venango County Courthouse. APA commended the City's officials and residents for their commitment, strategic planning and labor to protect the street's historical integrity and economic viability. The City invested approximately \$3.7 million dollars in streetscape enhancements since the 1980's. The Facade Improvement Program helped finance over 60 facade improvements and laid the groundwork for the downtown revitalization efforts. Home to the Barrow-Civic Theatre, a revamped 500-seat venue that was formerly an out-of-business retail sports outlet and idyllic, public greenspace (Bandstand Park and Fountain Park), Liberty Street remains a focal point of county commerce and social activity.

### *A Bicycle Friendly Community*

Recognizing the growing popularity of pedestrian and bicycle friendly downtowns, the City of Franklin applied for and received designation as a Bicycle Friendly Community (Bronze Level) in 2010 by the League of American Bicyclists; the BFC Report Card for 2014 is included on page 7. What makes a Bicycle Friendly Community (BFC)? The BFC program evaluates communities based on the following five E's:

- Engineering - creating safe and convenient places to ride and park
- Education - giving people of all ages / abilities the skills and confidence to ride
- Encouragement - creating a strong bike culture that welcomes and celebrates bicycling
- Enforcement - ensuring safe roads for all users
- Evaluation and planning - planning for bicycling as a safe and viable transportation option

The bronze level means that the community exhibited a strong commitment to cycling that is still in its beginning stage. There is room to grow but notable steps are being made in the right direction. Bronze-level BFC's tend to be strong in one or two of the five categories of the application. A community at this level may not yet feel bicycle-friendly to the average cyclist.







# FRANKLIN, PA

## TOTAL POPULATION

6,545

## POPULATION DENSITY

1393

## TOTAL AREA (sq. miles)

4.6

# OF LOCAL BICYCLE FRIENDLY BUSINESSES

0

# OF LOCAL BICYCLE FRIENDLY UNIVERSITIES

N/A

## 10 BUILDING BLOCKS OF A BICYCLE FRIENDLY COMMUNITY

	Average Silver	Franklin
Arterial Streets with Bike Lanes	45%	0%
Total Bicycle Network Mileage to Total Road Network Mileage	30%	1%
Public Education Outreach	GOOD	GOOD
% of Schools Offering Bicycling Education	43%	95%
Bike Month and Bike to Work Events	GOOD	SOME
Active Bicycle Advocacy Group	YES	VERY
Active Bicycle Advisory Committee	YES	SOMEWHAT
Bicycle-Friendly Laws & Ordinances	SOME	VERY GOOD
Bike Plan is Current and is Being Implemented	YES	SOMEWHAT
Bike Program Staff to Population	PER 70K	PER 6K

## CATEGORY SCORES

<b>ENGINEERING</b> <i>Bicycle network and connectivity</i>	2 / 10
<b>EDUCATION</b> <i>Motorist awareness and bicycling skills</i>	3 / 10
<b>ENCOURAGEMENT</b> <i>Mainstreaming bicycling culture</i>	1 / 10
<b>ENFORCEMENT</b> <i>Promoting safety and protecting bicyclists' rights</i>	4 / 10
<b>EVALUATION &amp; PLANNING</b> <i>Setting targets and having a plan</i>	2 / 10

## KEY OUTCOMES

	Average Silver	Franklin
<b>RIDERSHIP</b> <i>Percentage of daily bicyclists</i>	3.5%	1.00%
<b>SAFETY MEASURES CRASHES</b> <i>Crashes per 10k daily bicyclists</i>	180	428
<b>SAFETY MEASURES FATALITIES</b> <i>Fatalities per 10k daily bicyclists</i>	1.4	0
<b>PUBLIC RATING</b>	SILVER	BRONZE



## KEY STEPS TO SILVER



- » Develop a comprehensive bike plan.
- » Develop a bike network through the use of different types of bike lanes. On-street improvements coupled with the expansion of the off-street system will encourage more people to cycle and will improve safety.
- » Increase the amount of high quality bicycle parking at popular destinations throughout the community.
- » Expand encouragement efforts during Bike Month. Host, sponsor and encourage bicycle-themed community events, campaigns and programs. Increase your efforts on Bike to Work Day and Bike to School Day. Ensure to widely advertise all bicycle-themed community events and programs.
- » Promote cycling throughout the year by offering or

supporting additional family-oriented community and charity rides, free bike valet parking at events, and a bicycle-themed festival.

- » Encourage local businesses, agencies, and organizations to promote cycling to their employees and customers and to seek recognition through the Bicycle Friendly Business program.
- » Ensure that police officers are initially and repeatedly educated on traffic law as it applies to bicyclists and motorists.
- » Ensure that police officers report cyclist crash data and potential hazards to the public works department, traffic engineers and transportation planners to timely identify sites in need of safety improvements for cyclists.

## Complete Streets

After receiving designation as a BFC, Franklin adopted a Complete Streets Policy in 2010.

What does Complete Streets mean? Started in 2003, the complete streets movement was in response to car-centric planning and turned the attention of streets back to ALL users of streets, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Complete Streets feature ample sidewalks, improved standards for street tree planting and other landscaping, bike lanes, dedicated bus lanes, accessible transit stops, crosswalks, median islands and curb extensions (<https://www.planning.org/planning/2014/may/completestreets.htm>).

The policy was adopted for the “promotion of safe, convenient and healthy transportation choices for citizens and visitors” within the City. It ensures that all new construction and reconstruction of roadways and other transportation projects within the City rights-of-way consider, where possible, the accommodation of all users of the transportation system (pedestrians, bicyclists, people with disabilities, elderly, motorists, emergency responders, freight providers, etc.) and make use of the latest and best design standards, policies and guidelines.

In 2012, the Sugarcreek / Franklin Route 8 Complete Streets Study was completed to evaluate pedestrian, bicyclist, motorist and transit access through the Route 8 Study Corridor from Two Mile Run Creek in Sugarcreek Borough to the Samuel Justus Recreational Trail in the City of Franklin. While only a portion of the study area falls within the City, the plan did offer the following considerations within the City (as shown in Figure 4 on page 9):

- Decorative lighting along Franklin’s designated bicycle trail from the Franklin Trailhead to the intersection of Liberty Street and Allegheny Boulevard.
- The construction of a seasonal trail (due to high water conditions) connecting Riverfront Park to Route 8 north of Elk Street.
- Signalized improvements with pedestrian countdown timers at intersections along Liberty Street and American with Disabilities Act (ADA) compliant ramps.
- Addition of two bus shelters near the intersection of Liberty Street and Allegheny Boulevard to better accommodate pedestrian access to transit.
- Rehabilitation of the abandoned railroad bridge over French Creek so it can be used as a mixed use pedestrian and bicycle trail.
- Wayfinding signs to attract trail users and visitors entering the Franklin from the eastern limits on SR 0322 to businesses and attractions within the City.

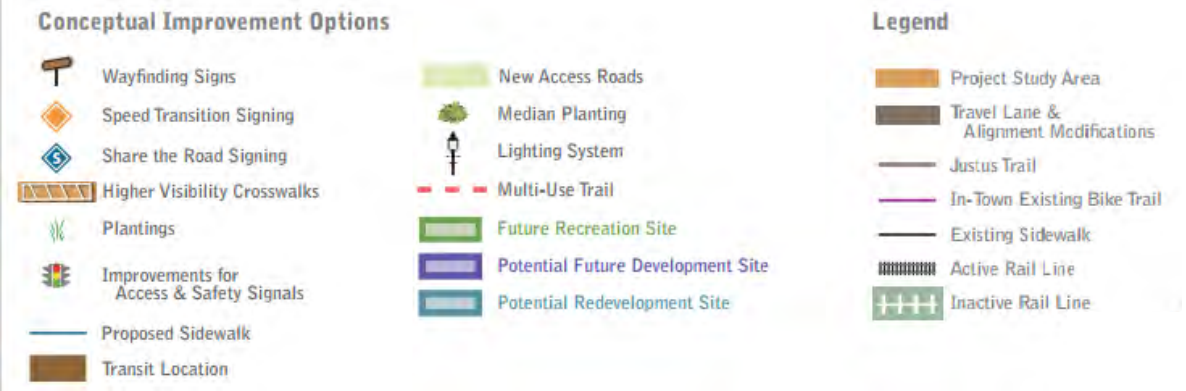
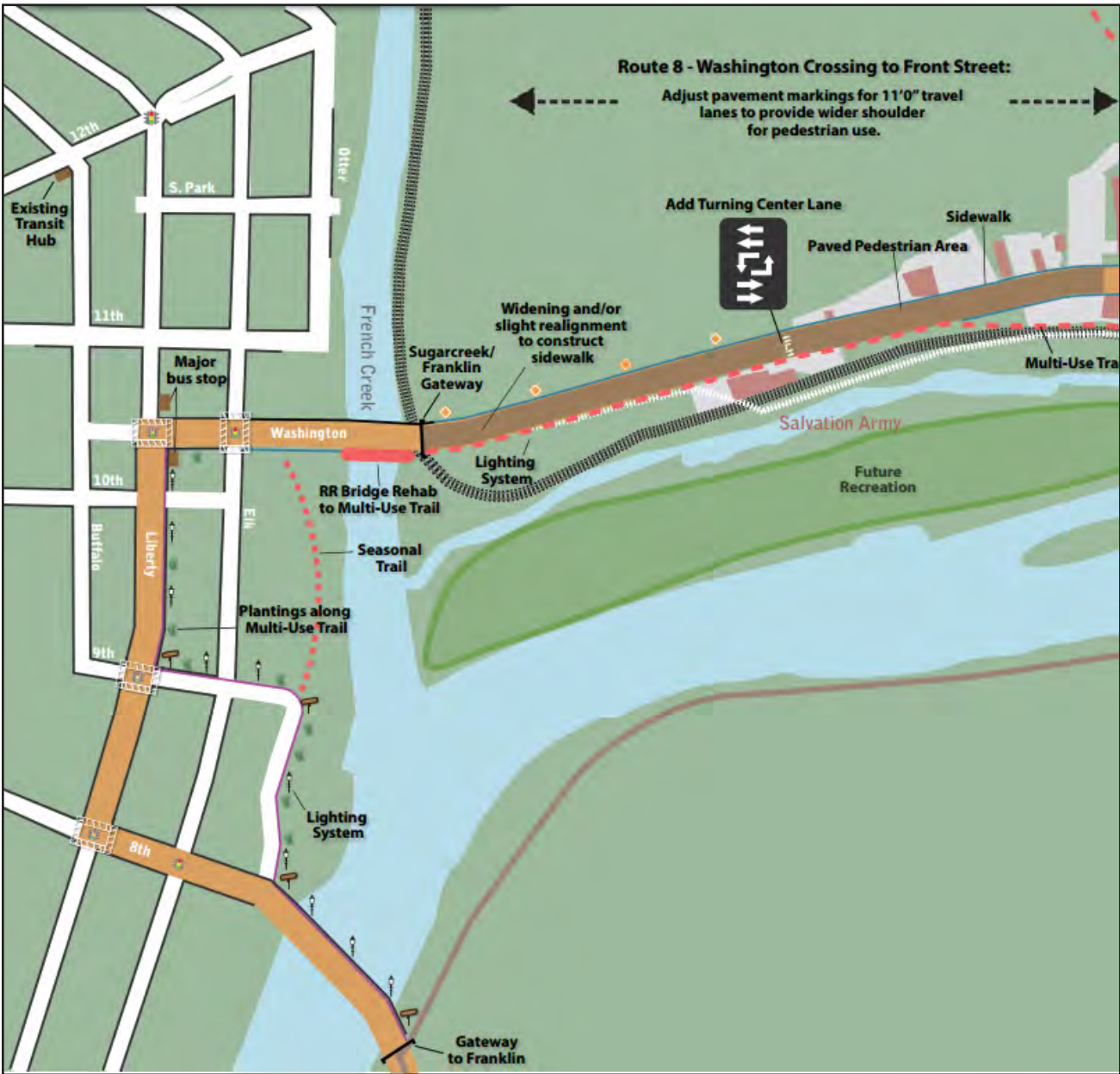
**Complete Streets are streets for everyone. They are designed and operated to enable safe access for all users. People of all ages and abilities are able to safely move along and across streets in a community, regardless of how they are traveling. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work. They allow buses to run on time and make it safe for people to walk to and from train stations.**

Source: <http://www.smartgrowthamerica.org/complete-streets/complete-streets-fundamentals>





Figure 4: Preferred Blended Corridor Alternative Complete Streets Study Route 8



## A Trail Town

As a designated Trail Town, the Progress Fund completed a Trail Town Assessment on May 18, 2016. The report identifies the top priority to improve the Franklin Trailhead so that there is better connectivity between the trail and the business district. Local amenities and the connection between the business district and the trail are shown in Figure 5 on page 10. Recommendations from the Assessment include:

- Install wayfinding signage: direct people to the downtown and trailhead
- Improve ADA connections, restrooms, bike racks, bike fixit stations, picnic amenities, kiosk, landscaping, etc.
- Improve the bulletin board and information available at the Saltbox Visitors Center.
- Improve the engineering and design of the parking area for the Samuel Justus Recreational Trail to delineate parking area from trail, spaces, etc.
- Replace the road signage and add a stop sign for trail users at the road/trail intersection; trim trees; and widen roadway to parking area

Note: the Franklin Trailhead and the Saltbox Visitors Center are located in Cranberry Township.

Figure 5: Franklin Bicycle Friendly Community Map



## Now what?

The purpose of the Bicycle-Pedestrian Connection Master Plan is to build upon the existing studies already completed to provide a comprehensive study of the multimodal network and offer improvements and upgrades to safely connect pedestrians and bicyclists to community services, businesses and shops, schools and parks and recreation.

## The Planning Process

The overall planning process consisted of the following:

### Project Team

The Project Team consisted of representatives of the City of Franklin, elected officials and staff (including members of the police and street departments), as well as the Venango County Planning Commission, Northwest Commission, Franklin Area Chamber of Commerce, PennDOT District 1-0, Oil Region Alliance, Allegheny Valley Trails, PA DCNR Bureau of Recreation and Conservation, Central Elementary School and local business owners and cycling enthusiasts.

The City of Franklin hired Mackin Engineering Company (Mackin) as its consultant to facilitate and complete the Pedestrian and Bicycle Connection Master Plan. The Project Team provided Mackin with background data, assisted with identifying and reviewing proposed connections and routes, assisted with public participation events and provided overall guidance of the process.

### Public Participation

In order to gather public input into the proposed improvements and projects related to the Pedestrian and Bicycle Connection Master Plan, the Project Team coordinated and facilitated the following events. Summaries of the meetings and events are included in the Appendices.

- Kick Off Meeting (July 20, 2016) - the meeting defined the study area (see Figure 6 on page 13), identified stakeholders and discussed initial opportunities and concerns that the Project Team wanted to see this Plan address.
- Project Team Meeting #2 (August 4, 2016) - Mackin presented conceptual bicycle routes (Route A and Route B) for connecting the trail to town. Route A utilized lower volume and lower speed local roads- Buffalo and Elk, and Route B extended the trail through the Riverfront Park to Washington Crossing Bridge and utilized Liberty Street to access town.
- “Tour de Franklin” (August 20, 2016) - as part of Franklin’s Rockin’ Riverfest, Mackin and the Project Team organized a public bicycle ride. The ride began at the Saltbox on the Samuel Justus Recreational Trail at 11:30 a.m. and cycled through the downtown. The route crossed the 8th Street Bridge and entered Riverfront Park, then continued onto 9th Street, Buffalo Street, 14th Street and Elk Street and back again through the Park and across the bridge to the Saltbox. Approximately 25 people attended the tour.





- Stakeholder Interviews - a variety of stakeholders, as identified by the Project Team, were interviewed by Mackin to identify potential opportunities, issues and concerns that the Pedestrian and Bicycle Connection Master Plan should address. Stakeholder groups included public safety (City police, fire and EMS); City public works and municipal staff; local hospital/healthcare providers; local bicycle riders and outdoor outfitters (OARS); the Franklin Area School District; VenanGo Bus (transit); Allegheny Valley Trails Association; and other local businesses and services.
- Applefest (October 7-9, 2016) - a large poster was on display at the City's Applefest festival to provide the public with conceptual plans for the initial bicycle and pedestrian connections.
- Project Team Meeting #3 (October 12, 2016) - the meeting included a presentation showing why this type of project is needed, the benefits from implementing the infrastructure improvements, how the bicycle and pedestrian projects could be implemented and construction budgets and maintenance costs.
- Phone Conference with City Manager and Community Development Director (November 9, 2016) - the meeting provided edits to the plan based on committee review, and public input. Some edits include detailing the route to show safety improvements, traffic calming recommendations at 12th and Liberty – potentially a roundabout design and adding a bus stop location in the Third Ward, at Grant and Pine.
- PennDOT Meeting (December 20, 2016) - the draft plan was discussed with PennDOT to obtain early input and support for priority projects. PennDOT's new program PennDOT Connects provides for early review and design collaboration with communities.
- Public Meeting (January 19, 2017) - Mackin and the Project Team presented the draft findings and recommendations to the public at a meeting on January 19, 2017 at 5:00p.m. at City Hall. Approximately 34 people attended the meeting.



*Poster that was displayed at Franklin's Applefest - October 7-9, 2016*

## The Study Area

One of the first items of business was to determine the limits of the study area for this Plan. Through discussions with City officials and the Project Team, the study area extents were identified as Route 8 / 62 / 15th Street to the west, Grant Street/US 322 to the north, the Franklin Trailhead to the east and the Third Street Playground to the south, as shown below.

Figure 6: The Study Area



## Project Goals

The study area is based upon the overall goals of this Plan, which were established by the Project Team:

- Create safe and convenient places to bicycle and enhance the walkability and bikability in the City.
- Identify public destinations and provide safe and efficient connections to them.
- Provide recommendations that will help Franklin achieve BFC Silver Status.
- Improve the Trail to Town connection to increase the economic impact.
- Improve the pedestrian and bicycle connection from the Third Ward to Downtown.
- Coordinate the existing bus service with the planned bicycle and pedestrian routes.

**It is the goal of the City of Franklin to not only provide safe pedestrian and bicycle connections for its residents, but also to improve the connection between the downtown business district and the Samuel Justus Recreational Trail in order to attract trail users into town.**

**A 2013 User Survey and Economic Impact Analysis was completed for the Erie to Pittsburgh Trail between Titusville and Parker, PA and documented the following *local economic* impact of trails:**



- 158,507 annual multi-use trail users (up from 138,281 in 2006)
- \$7,479,348 economic impact (up from \$4,310,000 in 2006)
- \$6,928,620 directly into the local economy
- More than 87% spent an average of \$337.50 on hard goods (bikes, racks, helmets, etc.) bought here and prior to the trip
- Soft goods (meals, snacks, bike rentals) were purchased by 76.8% for an average of \$21.62 per person per visit
- 13% paid for overnight lodging in the area, with an average of 2.6 nights per stay

*This study utilized a survey methodology previously tested on Pennsylvania trails and documented in RTC's Trail User Survey Workbook available on the RTC Trail Building Toolbox. This report was financed in part by a grant from the Community Conservation Partnerships Program, Environmental Stewardship Fund, under the administration of the PA DCNR, BRC.*





## Who lives here?

The demographics for the City of Franklin were reviewed and pertinent data related to the bicycle and pedestrian master plan are summarized below. The following statistics are from the United States Census Bureau, American Community Survey (ACS), unless otherwise noted.



**Population: 6,302 (2015 est.)**



**Median age: 42.6 (est. 2015)**



**Land area: 4.6 sq. miles**



**18.8% of population aged 65+**



**Median household income (est. 2015): \$35,602**

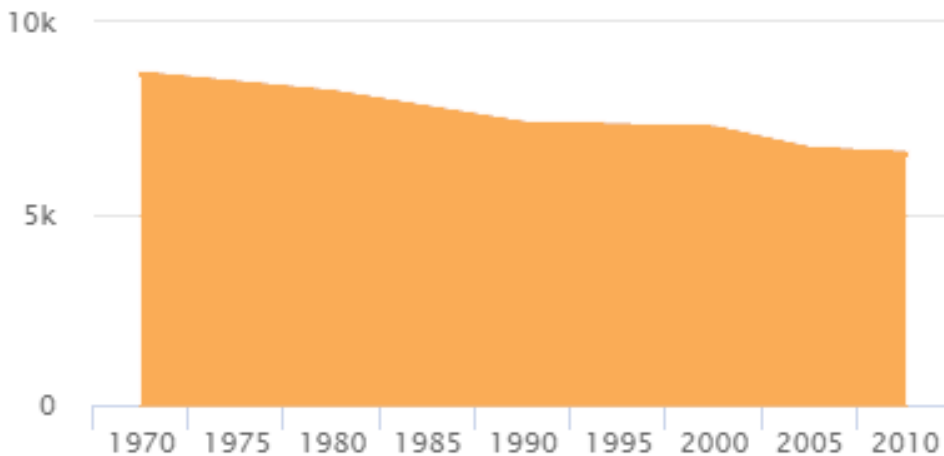


**45.7% of families have children under 18 years**

### *The population is shrinking.*

After reaching a high population of 10,254 in 1930, the City's population has been declining, with most of that resulting after 1970.

Figure 7: Franklin Population: 1970-2010



Source: <http://www.city-data.com/city/Franklin-Pennsylvania.html>



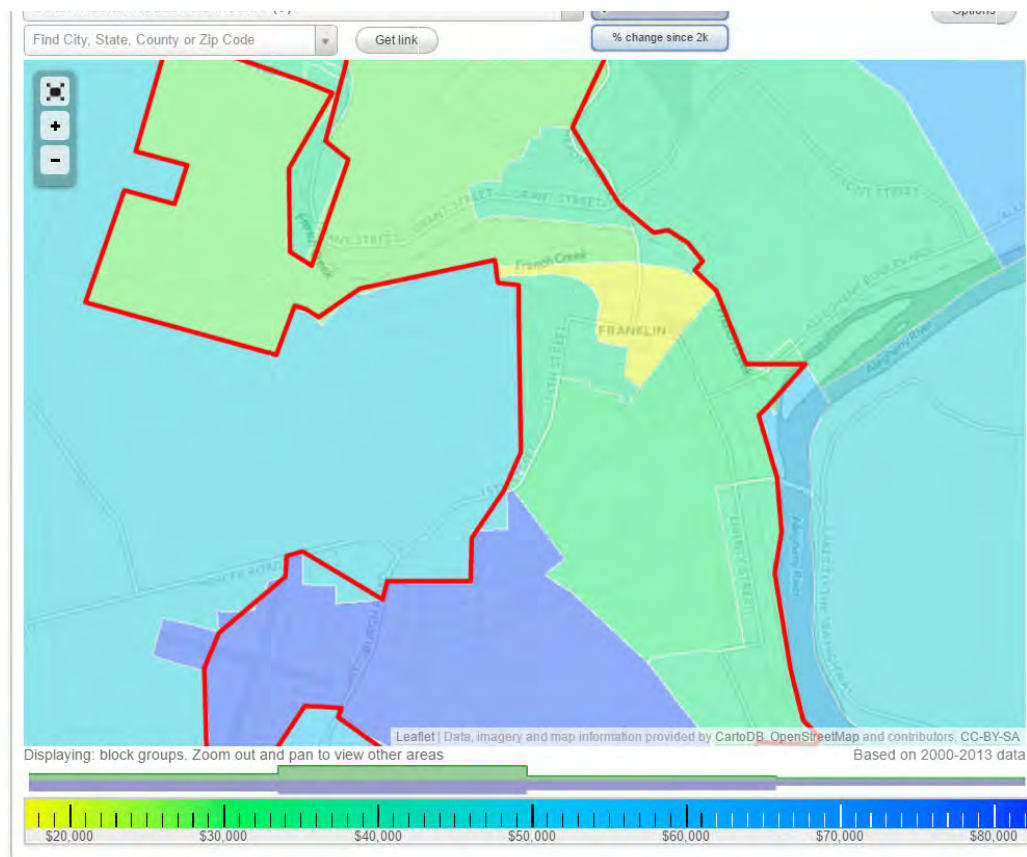
### *There is a large number of both elderly and youth that live in the City.*

With almost 20% of its population aged 65 and older, the City of Franklin has a large elderly population. In addition, almost half of all families living in the City have children living at home under the age of 18. Approximately 98 people reported living in nursing homes in the 2010 Census.

### *Low household income is primarily found in the downtown and the Third Ward.*

Overall household income is lower in the City (estimated at \$35,602 in 2015) than Venango County (\$43,644) and Pennsylvania (\$55,702). The lowest concentrations of income can be found in the downtown and the Third Ward, as shown in Figure 8 on page 16. The downtown area has a median household income of \$18,386 with 27% of residents living below the poverty level, a median resident age of 61 and is 98% white. The Third Ward area (north of French Creek) has a median household income of \$28,182 with 23% of residents living below the poverty level, a median resident age of 41.6 and is 86% white.

Figure 8: Median Household Income



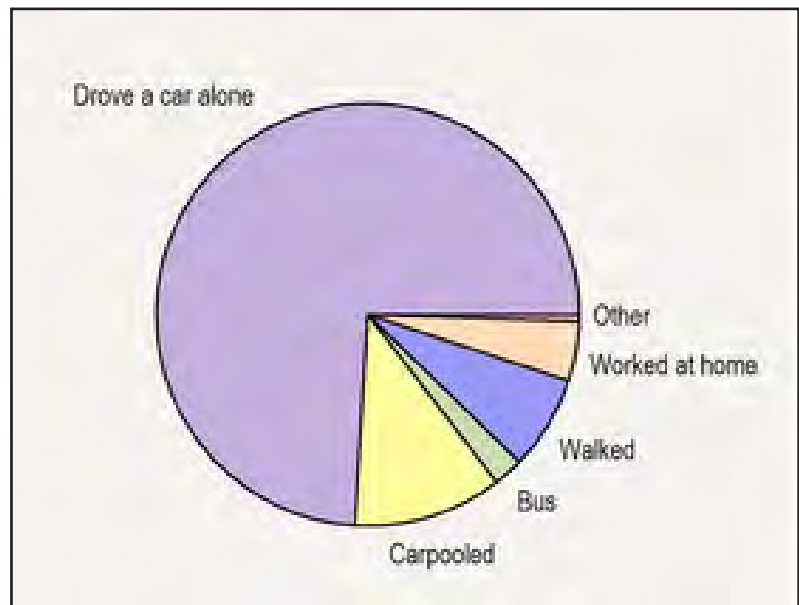
Source: <http://www.city-data.com/city/Franklin-Pennsylvania.html>

### 10.1% of residents walk to work.

Per the United States Census Bureau 2011-2015 American Community Survey 5-Year Estimates, the following commuting statistics for Franklin are true:

- 10% of residents walk to work
- 1.6% of residents take public transportation to work.
- No residents reported bicycling to work.

Figure 9: Mode of Transportation to Work



Source: <http://www.city-data.com/city/Franklin-Pennsylvania.html>

### Franklin is a “Very Walkable” Community.

Walk Score measures the walkability of any address based on the distance to nearby places and pedestrian friendliness. The City of Franklin has a Walk Score of 77, as shown in Figure 10 on page 17, which ranks it as a “Very Walkable” community where most errands can be accomplished on foot.

Figure 10: Franklin Walk Score



Source: <https://www.walkscore.com/score/city-of-franklin-pa>







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EDWARD JONES  
STOPHER PARKER, BRIAN MOORE, DENAALIE OBERNADER  
FROGGY MAJIC WUZZ  
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October  
7, 8, & 9, 2016

# APPLEFEST

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*Communities that create great places to live, shop, work and play will be most attractive to new residents. And as we move into the future, more people will decide they can work almost anywhere - and choose places that provide nearby convenience and mobility options for walking, bicycling, bus transit and driving.*



# Inventory & Assessment

Mackin Staff evaluated the existing conditions within the City of Franklin for walking and bicycling as a basis for developing the recommendations for future improvements and facilities to meet the overall project goals.

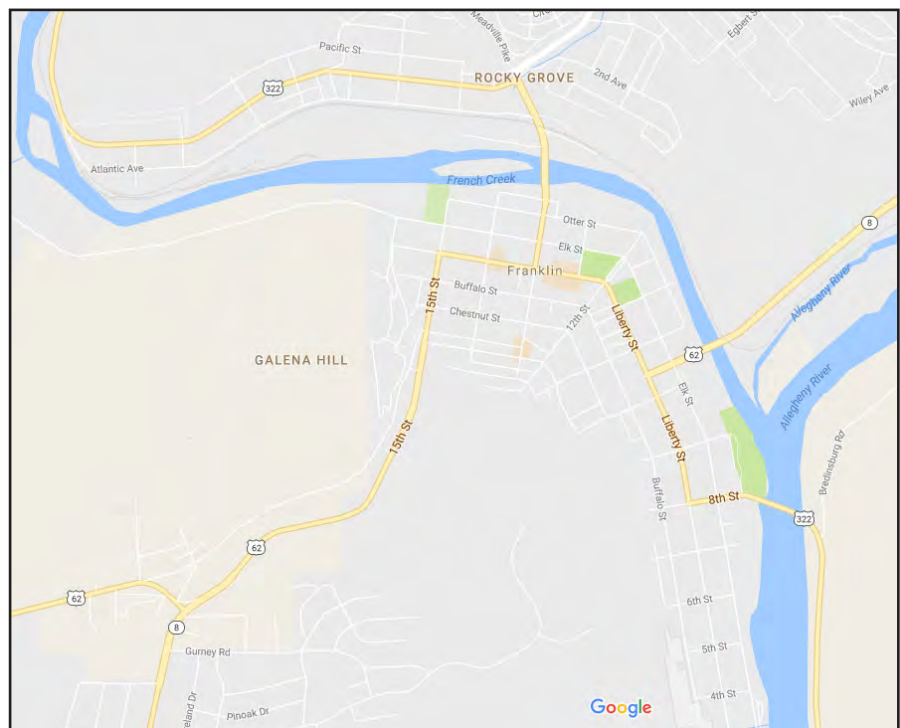
## The Road Network..

The roadways in Franklin serve as both as inter-city connections and as intra-city connections. Many cars and trucks travel through Franklin via State Route 8, US Route 62 and US Route 322. Route 8 provides connections to Oil City and other points north and Interstate 80 south; US 62 provides connections to Interstate 79 to the west and the Allegheny National Forest to the east; while US Route 322 provides connections to Meadville and Interstate 79 to the northwest and Clarion and other points east.

The roadways within the City were analyzed to identify the volume of traffic (average daily traffic, or ADT), road widths, right-of-way widths (which is often wider than that of the actual road), direction of traffic, and percentage of daily traffic that is trucks.

“Table 1: Average Daily Traffic (ADT) and Roadway Conditions” on page 20 provides this data for key sections of roadways within the City to determine what types of improvements are needed, and suitable, for each road to better accommodate bicyclists and pedestrians.

Figure 11: Local Road Network



Source: Google Maps



**Table 1: Average Daily Traffic (ADT) and Roadway Conditions**

Route #	Segment	Road Name	Road Width / ROW (feet)	Location Description	ADT	Direction of Traffic	% Trucks
US 62 / PA 8	260	15th Street and Liberty Street		15th Street and Liberty to 13th Street	16,610	Both	8%
US 62 / PA 8	280	Liberty Street	66/100	Between 12th and 13th Street	11,290	Both	9%
US 62 / PA 8	280	Liberty Street	40/100	Between 12th and Washington Crossing	11,290	Both	9%
US 62 / PA 8	310/311	Allegheny Blvd	varies, 44/70	Washington Crossing Bridge, Rt-8	11,060	Both	8%
		Elk Street	30/50	Between 13th Street and 8th Street	4,987	One-way	5%
		Elk Street	26/50	South of 8th Street	4,987	Both	5%
		Otter Street	50' ROW		1,393	One-way	5%
		Buffalo Street	30/50	Between 11th Street and 14th Street	2,862	One-way	5%
		Buffalo Street	30/50	South of 11th Street	2,862	Both	5%
		Eagle Street	50' ROW		1,733	One-way	5%
		13th Street	50' ROW	South of Liberty Street	1,530	Both	2%
		14th Street	30/50		3,566	Both	5%
US 322	280/281	13th Street	44.5/60	13th Street at Liberty and North	14,103	Both	6%
US 322	280/281	13th Street	varies/90		14,103	Both	6%
US 322		8th Street		8th Street Bridge	8,930 WB 6,450 EB	Both	
US 322	300	Liberty Street	varies/100	Between Washington Crossing and 8th Street	9,442	Both	5%

WB = Westbound; EB = Eastbound

Note: there is no ADT available for either Liberty Street or Elk Street south of 8th Street.





Overall, the busiest roadway is US Route 62 / State Route 8 (15th Street and Liberty Street). Liberty Street is the widest street in the City and carries a high volume of daily traffic. It consists of four travel lanes (two lanes each way) and on-street parallel parking within the central business district. Pedestrians crossing at the intersection of 12th and Liberty must traverse a very wide roadway in front of the County Courthouse, where the road's alignment bends. This bend, the substantial width and the speed of motorists all combine to create a challenging situation for people trying to cross at this point. According to the Franklin Police Department, there have been a total of 92 accidents at this intersection between 2004 and 2017; seven have involved a pedestrian. The most recent accident involving a vehicle hitting a pedestrian was on February 16, 2013. There have been even more numerous near-misses, not counted, but which are known anecdotally.

Other roads experiencing high traffic volumes include 13th Street and the 13th Street Bridge (US 322 / Meadville Pike), Washington Crossings Bridge (US 62 / SR 8 / Allegheny Boulevard) and Liberty to 8th Street.

Within the City, many of the local roads that run parallel to Liberty Street consist of a one-way travel lane with parallel parking on both sides

The speed limit within most of downtown Franklin is 25 miles per hour (MPH); however, it is not observed by many. The limit along Liberty increases from 25 MPH to 35 MPH at 11th Street and continues multiple blocks toward Third Street. The



*Liberty Street (US Route 62 / State Route 8)*  
Source: Google Streetview, Image capture: 2015



*Elk Street at 12th Street (local road, one-way)*



*Buffalo Street (local road, one-way)*





speed limit is posted at 35 MPH on 13th Street in the Third Ward and at the 8th Street Bridge outside of the city limit. The Washington Crossing Bridge and Route 8 north is posted at 40 MPH and turns to 45 MPH extending north to Oil City.

### Priority Connections

Moving out of the downtown, two of the priority roadways include bridges: the 13th Street Bridge (US 322 / 13th Street), which crosses French Creek and connects the downtown to the Third Ward; and the 8th Street Bridge (US 322), which crosses the Allegheny River and connects the downtown to the Samuel Justus Recreational Trail.

13th Street / US 322 is a four-lane road (two lanes of traffic in both directions) with sidewalks on both sides, as shown in the top photo to the right. The 13th Street Bridge is similar, with a raised median in the center.

8th Street in the City is a two-lane road (one lane of traffic in both directions) with sidewalks on both sides. The 8th Street Bridge, as shown in the middle photo to the right, is a two-lane road with a separated sidewalk on the north side of the bridge (traveling westbound). On the Cranberry Township side of the bridge, the road becomes the Lakes to the Sea Highway, with a left-turning lane for Bredinsburg Road, which is utilized to access the Samuel Justus Recreational Trail via bicycle or walking. There is no signage to direct either cyclists or pedestrians to the trail access.

Vehicles can access the Franklin Trailhead at the Saltbox Visitors Center by continuing on US 322, where a directional sign is located that says “Allegheny River & Justus Trails” near the entrance.



US Route 322 / 13th Street Bridge (looking south towards downtown)  
(Source: Google Streetview, Image capture: 2015)



8th Street Bridge / US 322 (looking east from downtown)



US 322 Access to Samuel Justus Recreational Trail  
(source: Google Streetview, Image capture: 2015)



# The Pedestrian and Bicycle Network...

Franklin is a Very Walkable city, according to WALK SCORE. The streets are flat, tree lined, attractive and accessible to businesses, shops, schools, parks, the library and other municipal services. The pedestrian and bicycle network was inventoried and analyzed in terms of accessibility (compliance with the Americans with Disabilities Act [ADA]) as well as overall connectivity to major destinations.

## Sidewalks

For the most part, Franklin exhibits an excellent sidewalk network, with sidewalks found along almost all streets. The width and condition of the sidewalks vary throughout the City; with the wider sidewalks found in the central business district and narrower ones in the residential areas. In Franklin's 1200 block of Liberty Street, the concrete sidewalks are in satisfactory condition, though the adjoining decorative concrete pavers that were installed in 2000 are not holding up, thus becoming not only a safety concern, but detract from the aesthetics as well. Additionally, the City has experienced the accelerated rusting of decorative light poles that were installed in the early 1980's. This deterioration of the metal light poles has led to seven being removed for safety reasons and the toppling of one mast arm traffic signal pole in the heart of downtown.

Mackin's Traffic Engineers and Landscape Architects conducted site visits and a review of 25 intersections along the various roadways and proposed connection routes within the City. Each of these intersections was analyzed for ADA-compliance in terms of ramps (with detectable warning surface), missing ramps, traffic signals with pedestrian push buttons and signal heads along with other conditions that may inhibit pedestrian activity. The detailed inventory is provided in the Appendices.

There are 60 curb ramps that comply with ADA standards, 48 ramps that require only detectable warning strips and 59 ramps that are non-compliant. Of the 25 intersections, there are 12 curb ramps that are missing.



*Liberty Street Sidewalks*



*Elk Street Sidewalks*



*Example of sidewalk without curb ramps -  
Buffalo Street / 14th Street*





## Trails

The City of Franklin lies along the planned Erie to Pittsburgh Trail, which starts at the Perry Monument on Presque Isle and heads south along Oil Creek to the Allegheny River to the “Point” in downtown Pittsburgh. The trail section adjacent to Franklin is part of the Allegheny Valley Trails Association trail system, an eight-foot multipurpose trail for bicyclists and pedestrians that includes the 32-mile Allegheny River Trail (from Franklin to Emlenton), the 5.8 mile Samuel Justus Recreational Trail (from Franklin to Oil City) and the 12-mile Sandy Creek Trail (from Fisherman’s Cove to Van). The Allegheny River Trail and the Samuel Justus Recreational Trail converge at the Franklin Trailhead, located just across the Allegheny River in Cranberry Township, at the east end of Franklin’s 8th Street Bridge off PA Route 322, as shown on Figure 13 on page 25. The trailhead features the Saltbox house, which is now a visitors center along the trail. The Saltbox was built in 1871 on Liberty Street and was moved to its current location to preserve it.

A paved trail is also located in Franklin’s Riverfront Park, as shown in Figure 12 on page 24. The trail is approximately 0.2 miles long, providing an off-road walking/biking path through the park. The trail is accessible from 9th Street and 8th Street (near the bridge).

## Bike Lanes / Share the Road

Currently, there are no bike lanes or designated (either via signage or pavement markings) share-the-road connections.

## Bike Racks

The City offers bike racks at many locations in the downtown, as shown on Figure 13 on page 25, including multiple locations on Liberty Street, at Central Elementary School, the Franklin Public Library, Fountain Park and Riverfront Park.

## Public Transportation

Franklin is served by the Venango Bus Service including three routes that travel through and around the City. The Franklin School District also routes school bus service through the City. The public transportation routes and stops are shown in blue, with school bus stops shown in red, on Figure 13 on page 25.



*Samuel Justus Recreational Trail*

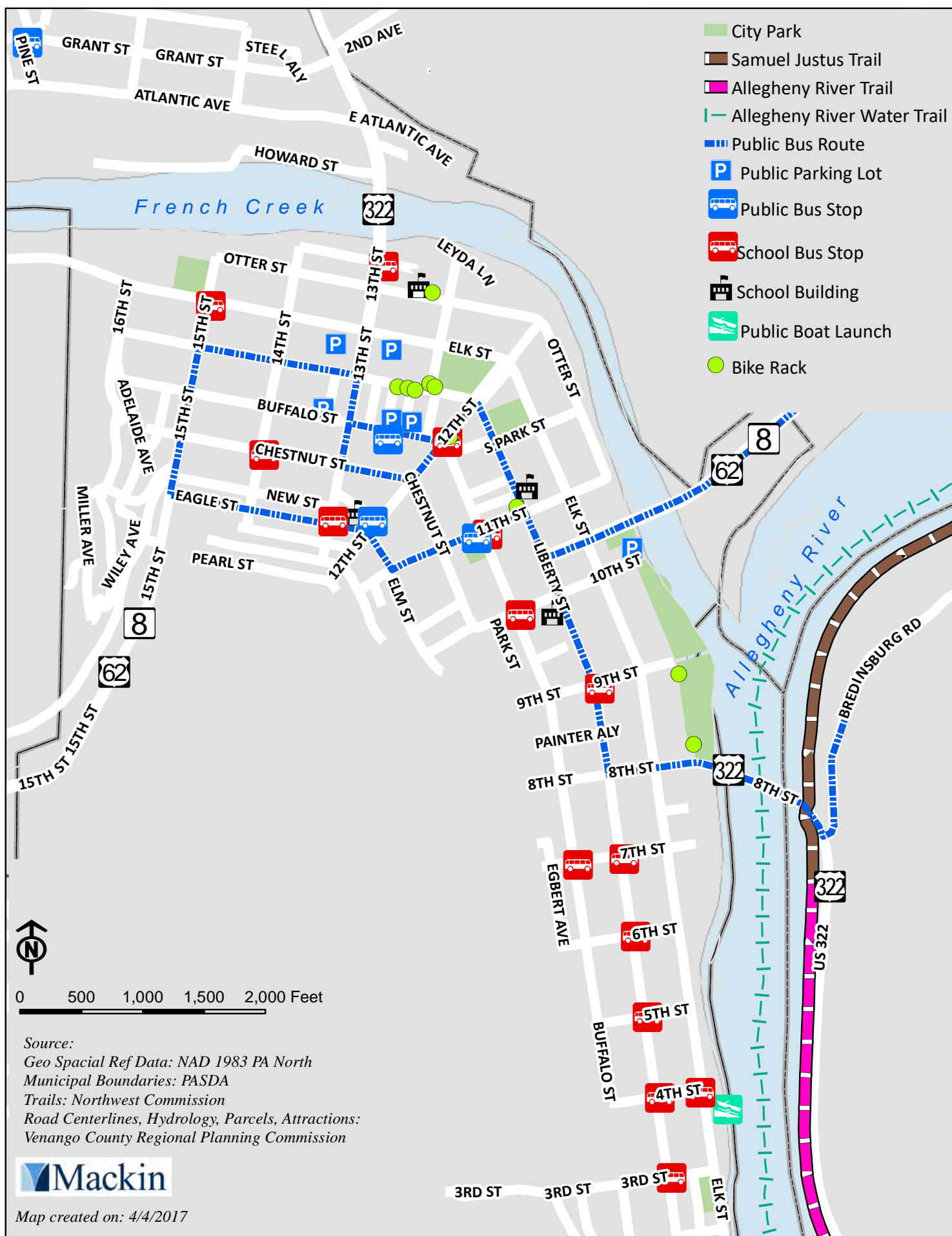
Figure 12: Riverfront Park Trail Map



*Bike Racks at the Library*



Figure 13: Map of Existing Trails, Bicycle and Public Transportation Network









# Connection Concept Plan

With the inventory and analysis of the current conditions (pedestrian, bicycle, transit, etc.) complete, the next step was to develop the Connection Concept Plan, based on the following project goals:

- Create safe and convenient places to bicycle and enhance the walkability and bikability in the City.
- Improve the Trail to Town connection to increase the economic impact.
- Develop bike and pedestrian connections to local and regional attractions, including parks, hotels, bed and breakfasts, eateries, historic sites, schools, shopping and other civic and cultural attractions.
- Improve the pedestrian and bicycle connection from the Third Ward to Downtown.
- Coordinate the existing public transit (bus service) and school bus stops with the planned bicycle and pedestrian routes.
- Identify wayfinding improvements to direct pedestrians and bicyclists to local amenities.

The recommendations for the concept plan are organized by the project guiding principles (listed to the right).

## The guiding principles...

- **Walking:**  
Create a safe and inviting network for residents of all ages and abilities.
- **Bicycling:**  
Create the framework to improve bikeability in Franklin.
- **Signage:**  
Install pedestrian and bicycle wayfinding signage throughout the City.
- **Transit:**  
Improve the bicycle and pedestrian connections to the public transportation system.
- **Programs:**  
Expand and increase the programs offered.
- **Art:**  
Utilize public art to promote Franklin as a bicycle- and pedestrian-friendly City.



# Walking:

## Create a safe and inviting network for residents of all ages and abilities.

**In a walkable community, residents are likely to walk to places such as markets, restaurants, stores, places of worship, public parks and other establishments. Pedestrian-oriented communities create a “front porch culture” where residents interact more with their surroundings and feel connected to and responsible for their physical community, which increases the success of initiatives meant to support local businesses.<sup>1</sup>**

Walkable cities also improve overall public health. Approximately doubling the proportion of neighborhood residents walking to work decreases a resident’s risk of obesity by almost 10%. Pedestrian-friendly street networks have shown lower risks of obesity for their residents.<sup>2</sup> In addition, the pedestrian network should be accessible for people of all ages and abilities.

### ***W-1: Upgrade the sidewalk network to be ADA-compliant.***

Based on the site visits, inventory and analysis of the sidewalk network and intersections along the proposed bike routes in the City, recommendations were developed to address compliance and safety concerns. Examples of ADA and pedestrian considerations are provided in Figure 14 on page 29. There are 60 curb ramps that comply with ADA standards, 48 ramps that require only detectable warning strips, and 59 ramps that are non-compliant. Of the 25 intersections, there are 12 curb ramps that are missing and should be constructed to provide access for all users. Other upgrades include adding pedestrian signal heads and accessible push buttons. The full list of recommendations for the 25 intersections is included in the Appendices. In addition to intersections improvements, the City needs to improve the conditions of sidewalks where the concrete is crumbling.

<sup>1</sup> “Social Capital and the Built Environment: The Importance of Walkable Neighborhood.” *American Journal of Public Health*. September 2003, Volume 93, Issue 9, Pages 1546-1551. Retrieved online at <http://ajph.aphapublications.org/doi/full/10.2105/AJPH.93.9.1546>.

<sup>2</sup> “Walkability and Body Mass Index. Density, Design and New Diversity Measures.” *American Journal of Preventive Medicine*. September 2008, Volume 35, Issue 3, Pages 237-244. Retrieved online at [http://www.ajpmonline.org/article/S0749-3797\(08\)00514-X/abstract](http://www.ajpmonline.org/article/S0749-3797(08)00514-X/abstract).

Figure 14: ADA and Pedestrian Considerations

Curb Ramps	Detectable Warning Surface	Crosswalks	Pedestrian Controls
			
<p>Curb ramps consist of a level landing on the sidewalk, a ramp, and a level landing on the street level and may also include flares on either side of the ramp. There must be a smooth transition into the street. Curb ramps should be aligned with a marked crosswalk (if present) and perpendicular to the roadway being crossed (when possible).</p>	<p>Tactile warning surfaces sometimes called “truncated domes” or “warning domes” are applied to provide pedestrians physical notice that they are about to enter the roadway environment; these applications need to be detectable underfoot or by a long cane. The tactile warning should be complimented with a visual warning.</p>	<p>A crosswalk extends the sidewalk network across a street at an intersection or mid-block location. The crosswalk should be clearly marked through appropriate pavement markings. The crosswalk should be perpendicular to the roadway to create the shortest exposure time and distance and to accommodate the visually impaired.</p>	<p>Pedestrian signal heads provide special types of traffic signal indications exclusively intended for controlling pedestrian traffic. Pedestrian detectors may be pushbuttons or passive detection devices. If pedestrian pushbuttons are used, they should be capable of easy activation and conveniently located near each end of the crosswalks.</p>

Source: *Unsignalized Intersection Improvement Guide. A Community of Transportation Professionals*. Retrieved online at [www.ite.org](http://www.ite.org).

Further specific recommendations on sidewalks are found in the individual Priority Areas (beginning on page 35) of the Bicycling section of this Concept Plan.

### ***W-2: Develop a signed walking route in the Downtown.***

The City of Franklin should develop a walking route within the downtown. The route should highlight historic, natural and recreational features. The route could be marked on the ground and supported with a sign (signed for destination, distance, direction). The City should prioritize sidewalk improvements along this route to address any hazardous or deficient areas.

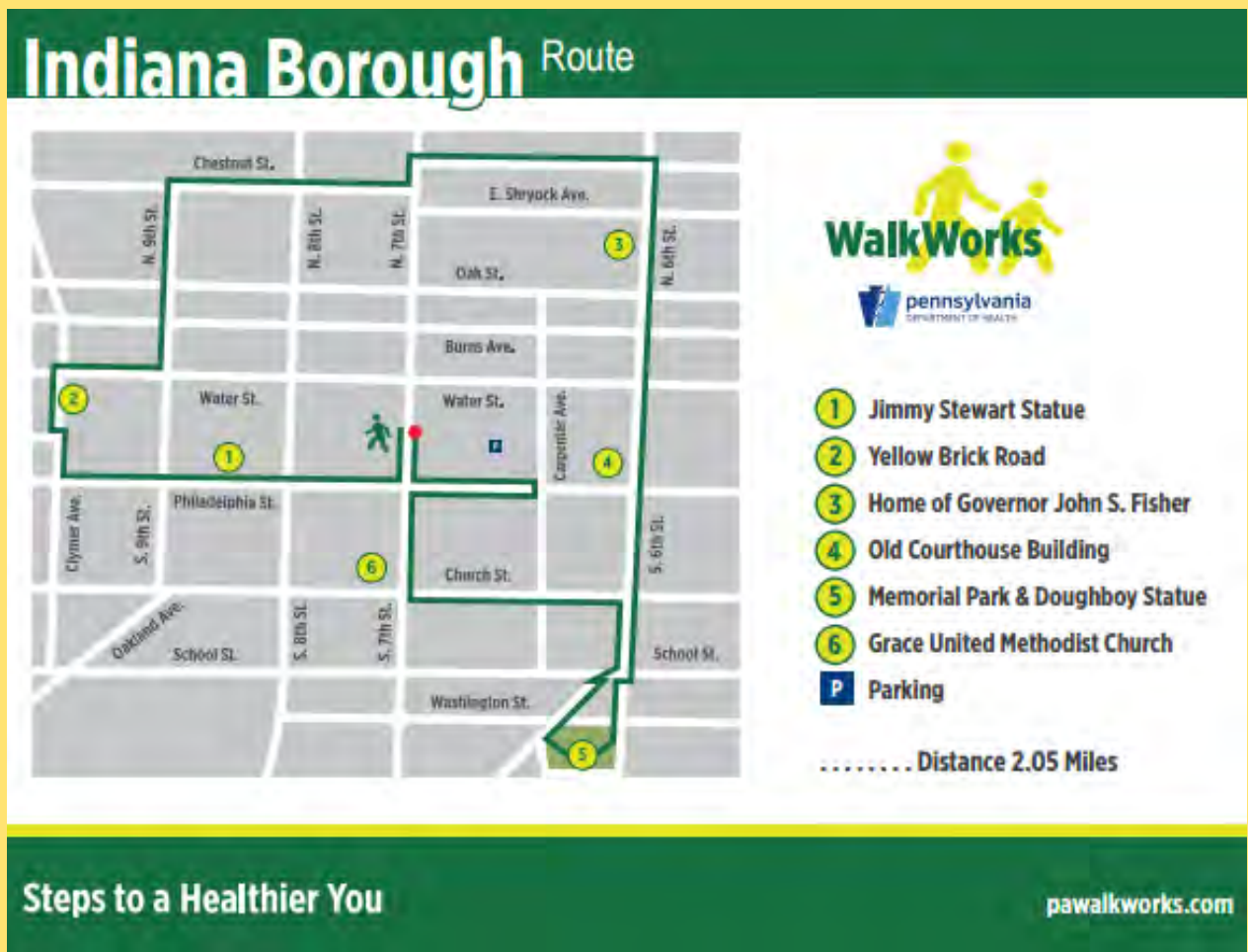
One option would be to partner with Venango County and WalkWorks to develop the route as a WalkWorks route. WalkWorks is a partnership between the Pennsylvania Department of Health and the University of Pittsburgh Graduate School of Public Health Center for Public Health Practice, whose mission is to “create a network of fun, fact-filled, community-based walking routes and walking groups.”<sup>3</sup> WalkWorks can identify and promote safe walking routes; offer social support through community-based walking groups; help schools develop walk-to-school programs; and address local policies to increase safe walking routes. Several counties in Pennsylvania, including Indiana County, have established programs and routes vary in length and type (locations include business/industrial parks, hospital and health centers, downtowns, historic neighborhoods, trail networks, etc.). An example of a WalkWorks route is provided in Figure 15 on page 30.

<sup>3</sup> “WalkWorks.” Pennsylvania Department of Health. Retrieved online October 2016. [www.health.pa.gov/WalkWorks](http://www.health.pa.gov/WalkWorks)





Figure 15: Example of a WalkWorks Route



Indiana Borough (Indiana County, Pennsylvania) was selected to participate in the WalkWorks program in 2015 and the Indiana County Office of Planning & Development worked with its municipalities to establish WalkWorks Routes in Blairsville, Ernest, Glen Campbell, Indiana Borough, Homer City, Indiana/Getty Heights, Penns Manor and the Indiana Regional Medical Center. Each of the WalkWorks routes have brochures that provide a map of the route and a description of the various sites and attractions along the route. The County's website also provides additional information related to WalkWorks, walking safely, health benefits of walking and other resources.

Source: <http://www.icopd.org/indiana-county-walkworks-program.html>.



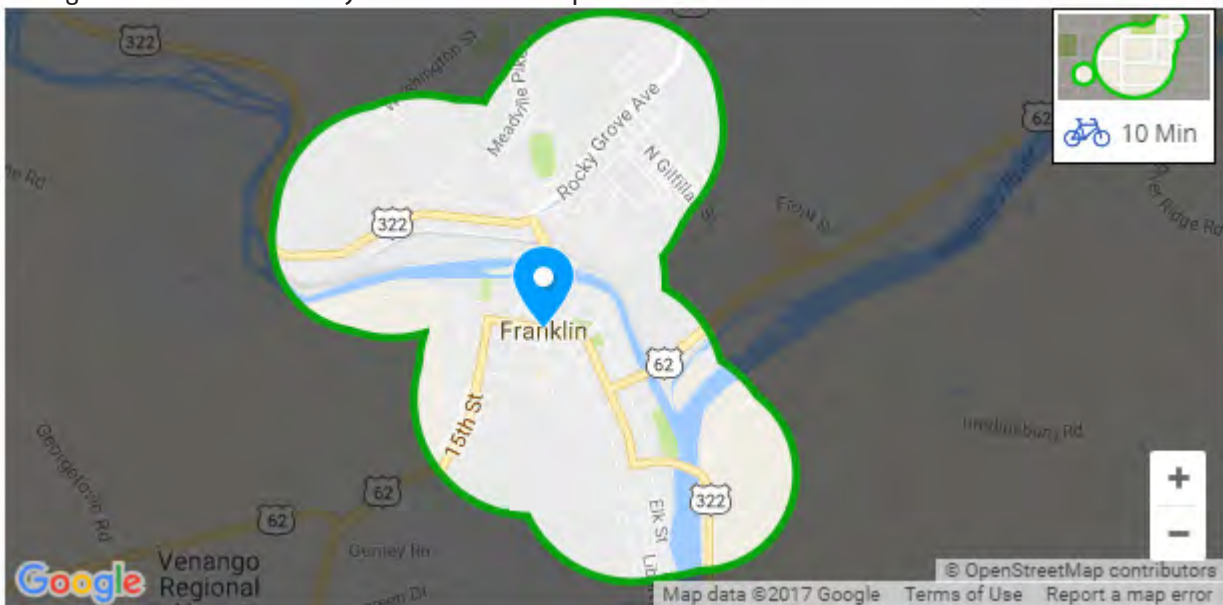
# Bicycling: Create the framework to improve bikeability in Franklin.



**The City's Bicycle Friendly Community (BFC) designation will be up for renewal in 2018. If the City wants to achieve the Silver level, it needs to implement the key recommendations provided in their Report Card.**

The BFC provided recommendations that will help Franklin further promote bicycling and improve the overall community for bicyclists. The recommendations were organized under each of the five "E's" - Engineering, Education, Encouragement, Enforcement and Evaluation & Planning. We have provided specific improvement projects that will implement the key recommendations from their 2014 Report Card. Not only will these recommendations help the City to obtain BFC Silver, but it will also encourage more residents to bike to work, attract trail users into the downtown (which will increase the economic impact locally) and provide safe connections for cyclists of all ages and abilities. Figure 16 on page 31 shows the area that can be reached within a 10-minute bike ride from downtown Franklin.

Figure 16: 10-Minute Bicycle Travel Time Map



Source: Google Maps, Bicycle Travel Time Map



Figure 17: Types of Bike Lanes

### Conventional Bike Lane



Bike lanes designate an exclusive space for bicyclists through the use of pavement markings and signage. The bike lane is located adjacent to motor vehicle travel lanes and flows in the same direction as motor vehicle traffic. Bike lanes are most helpful on streets with  $\geq 3,000$  motor vehicle average daily traffic and a posted speed  $\geq 25$  mph.

### Buffered Bike Lane



Buffered bike lanes are conventional bicycle lanes paired with a designated buffer space separating the bicycle lane from the adjacent motor vehicle travel lane and/or parking lane. Buffered bike lanes increase the distance between cyclists and vehicles, contributing to the perception of safety among users of the bicycle network.

### One-Way Raised Cycle Track



Raised cycle tracks are bicycle facilities that are vertically separated from motor vehicle traffic. Cycle tracks are most beneficial along streets where bicyclists feel stress because of factors such as multiple lanes, high traffic volumes, high speed traffic, high demand for double parking and high parking turnover.

### Shared Lane Markings



Shared Lane Markings (SLMs), or "sharrows," are road markings to indicate a shared lane environment for bicycles and automobiles. SLMs reinforce the legitimacy of bicycle traffic on the street and recommend proper bicyclist positioning. SLMs are not a facility type; rather they are used to support a complete bikeway network.

Source: National Association of City Transportation Officials ([nacto.org](http://nacto.org))

## B-1: Provide bike lanes to increase safety and encourage more users.

The best way to encourage new cyclists and less experienced bicyclists to ride is to provide a separate lane for bicycle use. There are multiple types of bike lanes, each offering its own benefits and the suitability of each is based on the specific conditions of individual roadways. The types of bike lanes that are recommended for Franklin are shown and described in Figure 17 on page 32. Shared Lane Markings (SLMs) are also included; although not bike lanes, they are recommended on some of the smaller, less traveled roads. It should be noted that the League of American Bicyclists ("The League") recommends that SLMs should only be used sparingly and only on low speed roads.

The following bike infrastructure is recommended for the City of Franklin:

- Trail to Town Connection - bike lanes on 8th Street Bridge
- Riverfront Park - bike path and mid-block crossing - flashing warning device
- Liberty Street - raised cycle track and bike lanes
- Connection to the 3rd Street Playground - bicycle and walking route
- Elk and Buffalo Streets - one-way traffic; shared lane markings

All roadway improvements for cycling and pedestrian infrastructure will require approval by the roadway owner (PennDOT/County/City).





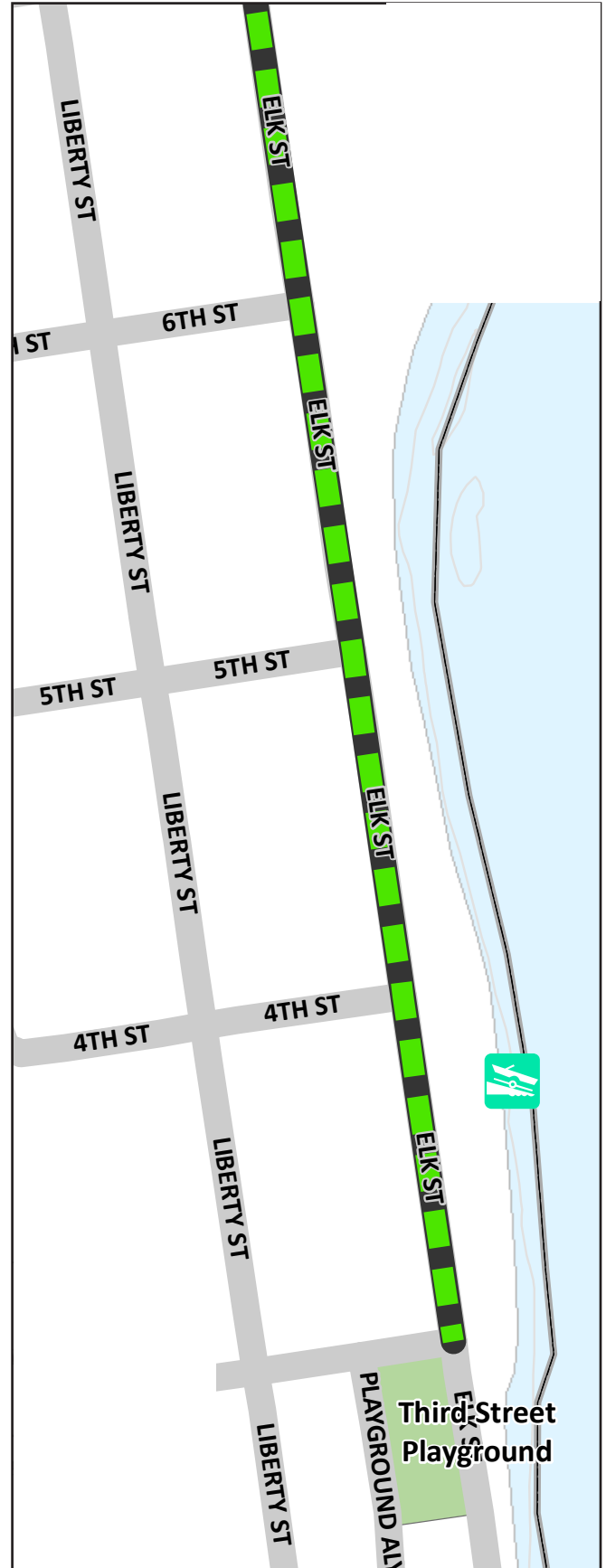
Figure 18: Proposed Connections & Amenities



Figure 19: Connection to the Third Ward



Figure 20: Connection to the 3rd St Playground



# Priority Area #1: Trail to Town Connection

The following improvements should be considered for the Trail to Town Connection:

- Coordinate with PennDOT for bike lanes along 8th Street- from Riverfront Park to north side of bridge. See Figure 22 on page 39.
  - » The existing Bridge is 30.5' wide, with a 5' sidewalk behind an abutment wall.
  - » Remark travel lanes (Lane Diet) at 10' wide and add 2- bike lanes at 5' each. Add bike lane signs and bike lane markings; sign bridge sidewalk as 'walk bike on sidewalk'.
  - » This allows cyclists to use the existing narrow walk or to use the bike lanes on the bridge.
  - » Extend the EB bike lane to the current trailhead driveway entrance.
- Post the north side of the 8th Street Bridge as 25 MPH, near the Bredinsburg Road area.
- This will help to slow traffic speeds across the bridge and entering the city. Currently, the road is posted at 35 MPH at north end and 25 MPH at the south end of the bridge.
- Consider installation of transverse pavement markings at approach lane to alert motorists of the new speed limit.
- Add Wayfinding Signs along the route, starting at the Franklin Trailhead, to direct users into Franklin- services, shops, restaurants, parks, overnight accommodations.



*8th Street Bridge - looking west towards downtown*





### ***Alternative Designs for Bridge-***

*Investigate and coordinate with PennDOT for Bicycle Infrastructure alternatives:*

- Alternative A- Add 5' Bike Lane into town (WB) via Riverfront Park, shift lanes south and remark lanes at 11'; reduce south shoulder to 3' wide. Install Shared Lane Markings exiting town (EB). See Figure 23 on page 41.
- Alternative B- Reconstruct the north side of the bridge deck as a Shared Use Path (SUP). Build a 12' wide concrete SUP for bikes and pedestrians. Install a new barrier to separate the SUP from the road. The SUP to be marked with a centerline for EB/WB bike-ped travel direction. See Figure 24 on page 43.



Figure 21: Priority Connection #1: Trail to Town

LEGEND

- Bike Lane
- Samuel Justus Recreational Trail







Figure 22: Cross Section of Proposed Bike Lanes on 8th Street Bridge

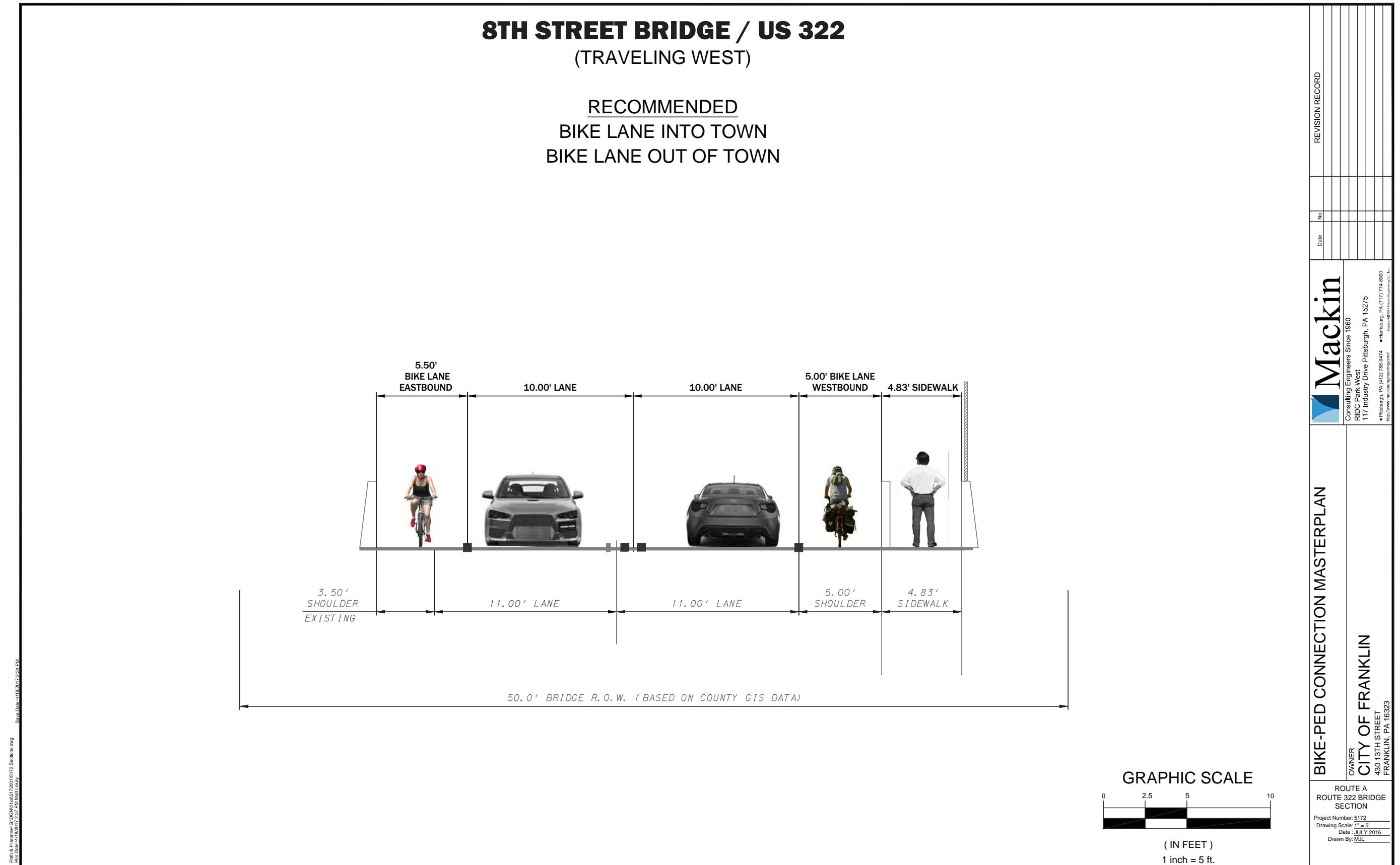




Figure 23: Cross Section of 8th Street Bridge Alternative A

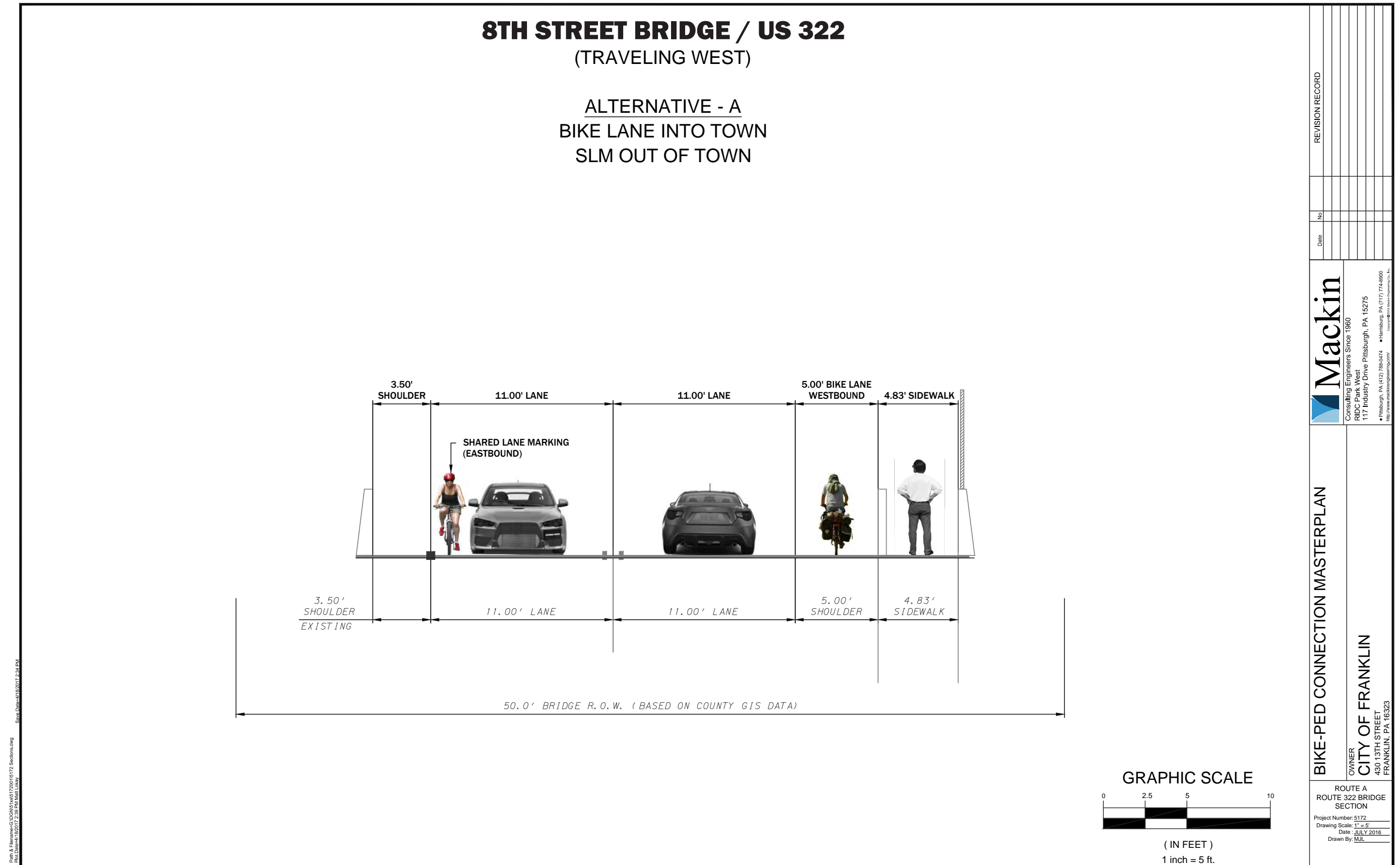






Figure 24: Cross Section of 8th Street Bridge Alternative B

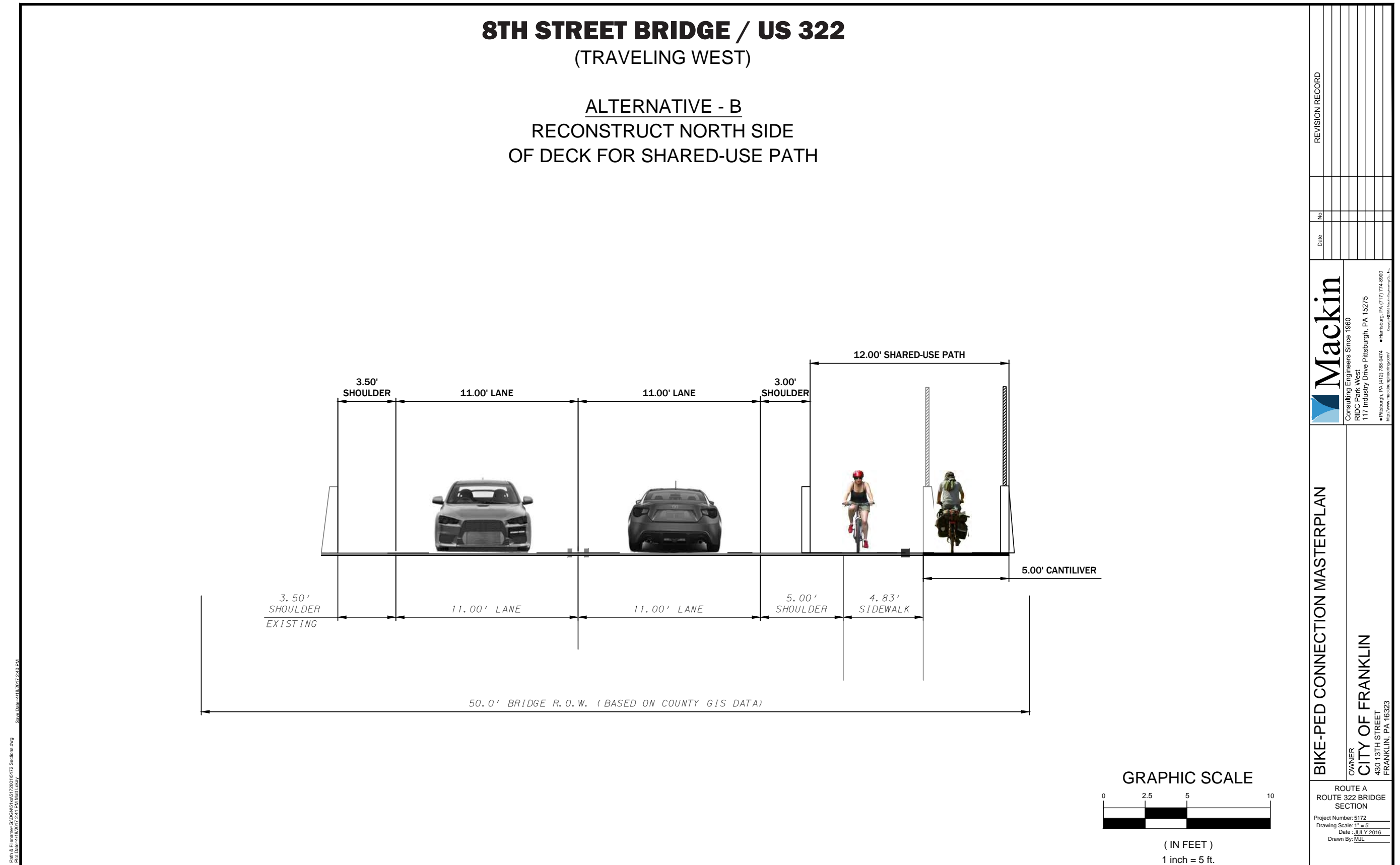


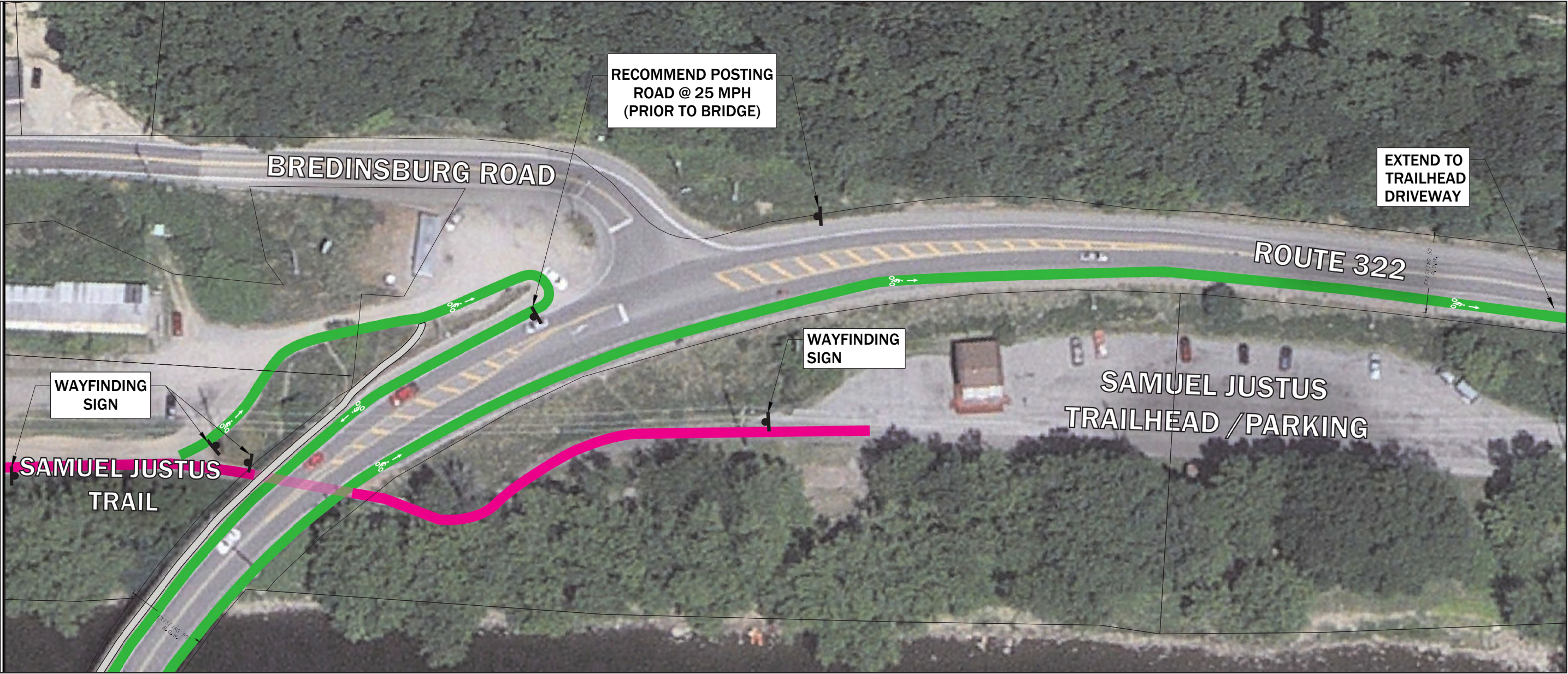




Figure 25: Priority Connection #1: Trail to Town

LEGEND

- Bike Lane
- Samuel Justus Recreational Trail







# Priority Area #2: Riverfront Park

The following improvements should be considered for the Riverfront Park Bike Path and mid-block crossing:

- Construct a separate bike trail along 'back of park' to avoid ped-bike collisions. The trail to be made of crushed limestone (AASHTO #10) to minimize costs.
- Delineate a westbound bike lane in the parking lot, uphill, exiting the park. Mark and sign the eastbound route as 'Share the Road' for travelers entering the park from Elk and 9th Streets.
- Reconstruct 8th Street sidewalks from Riverfront Park entrance to Elk Street
- Coordinate with PennDOT for a mid-block crossing, flashing warning device at 8th Street. The mid block crossing is required to exit Riverfront Park to 8th Street and will allow users to safely travel back to the trail. All mid-block crossing signs and pavement markings are to comply to MUTCD standards and must be reviewed and approved by PennDOT since this is a State ROW crossing
- Add Wayfinding Signs to direct users into the downtown for shops, services, restaurants and parks.



*Examples of a mid-block trail crossing with flashing warning beacon and signs.*





### ***Alternative Designs for Mid-Block Crossing at 8th St.-***

*Investigate and coordinate with PennDOT for Bicycle/Pedestrian Crossing for eastbound travelers.*

- Recommended Alternative A, Overhead Flashing Warning Device (FWD) at Crossing for EB travelers- the crossing to include an overhead flashing warning signal and either a remote or an actuated control (this FWD crossing type is most easily seen, especially with truck traffic). All design and construction must comply with PennDOT standards and permit requirements.
- Alternative B- Post mounted FWD pedestrian signals in lieu of an overhead device.
- Alternative C (not recommended)- Sign EB users to cross 8th Street at the intersection of at Elk and 8th St. (this is approximately 320' away from the park entrance) and upgrades to the existing crossing, sidewalk repairs and a barrier to block pedestrian/bicycle users from crossing at park. Not recommended due to far distance from the park entrance; users must travel over 300' in the opposite direction and 300' back to make the crossing.

## **Priority Area #3: 9th Street**

The following improvements should be considered for 9th Street:

- Install “Share the Road” signs and pavement marking signs (“sharrows”) along the alignment.
- Improve the safety of the crossing at the park driveway and Elk/9th Street. The uphill route from the park, the speed of motorists along Elk and the limited sight distance exiting the park make this a potentially dangerous crossing.
  - » Investigate adding a Stop Sign at Elk Street. This is needed for users to safely exit Riverfront Park and cross the street to the bike route.
  - » Extend and stripe out ‘No Parking’ at corner of at Elk/9th intersection to open up sight distance for users exiting the park.
- Improvements also include upgrades to sidewalks, crossings and ADA curb ramps.
- Add Wayfinding Signs to direct pedestrians and cyclists to the Downtown Business District for shops, services, restaurants, parks and other amenities.





Figure 26: Riverfront Park

LEGEND

Bike Lane

Crosswalk

Share the Road (sharrows)

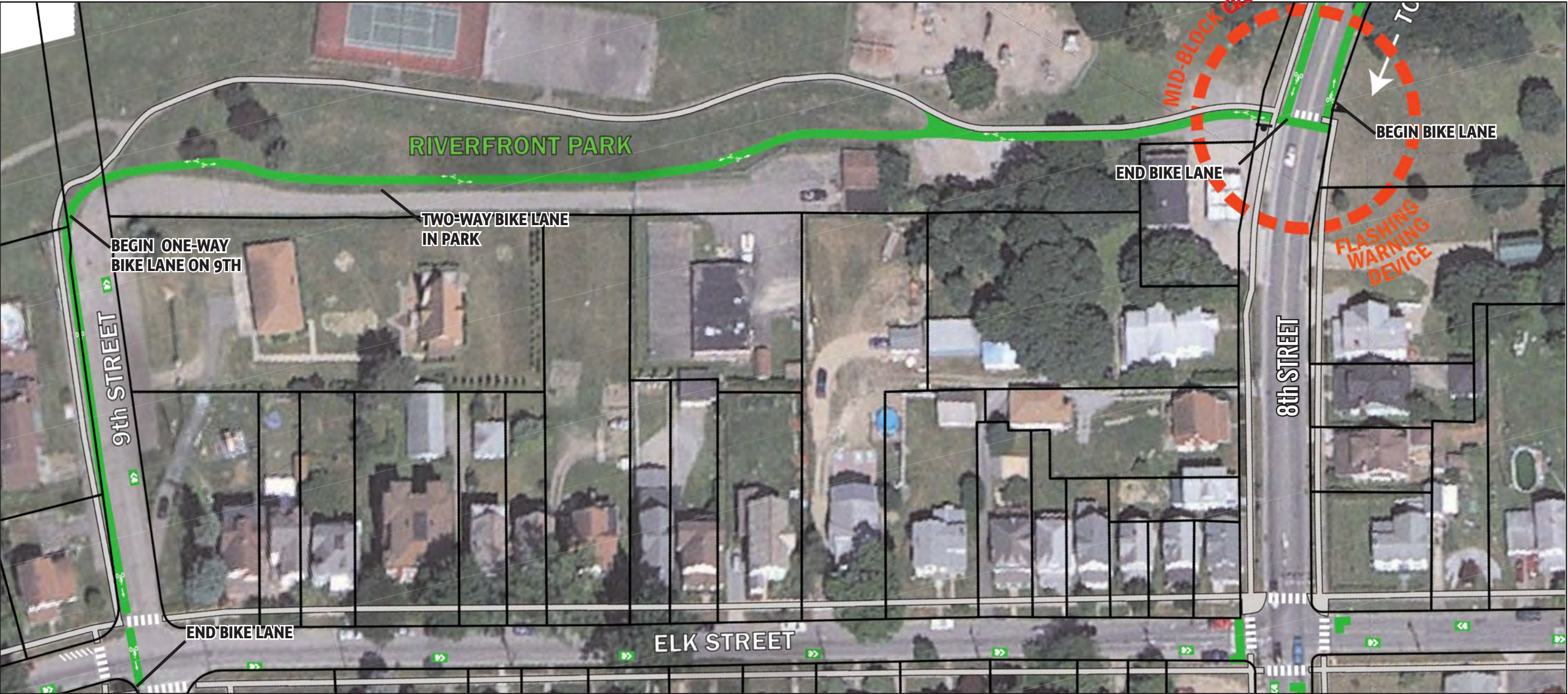








Figure 27: 9th Street

LEGEND

- Bike Lane
- Crosswalk
- Share the Road (sharrows)

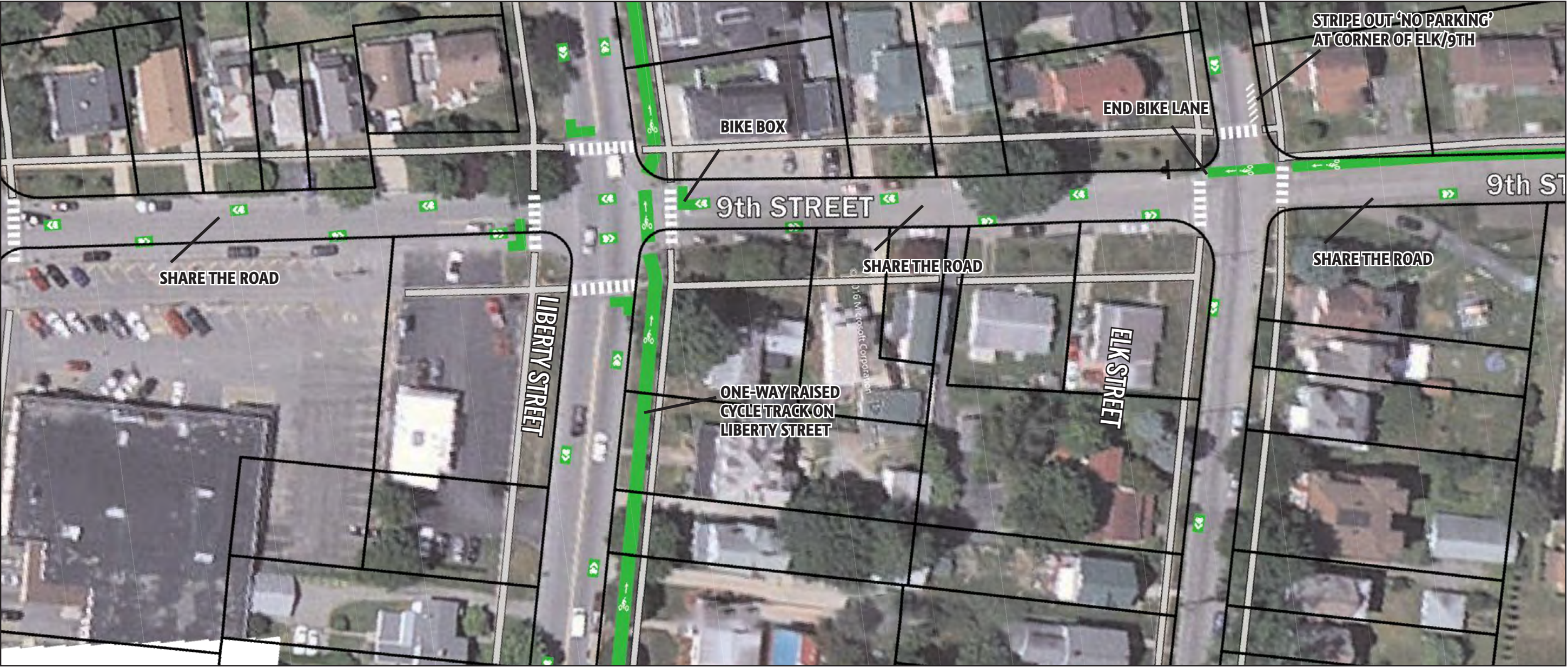
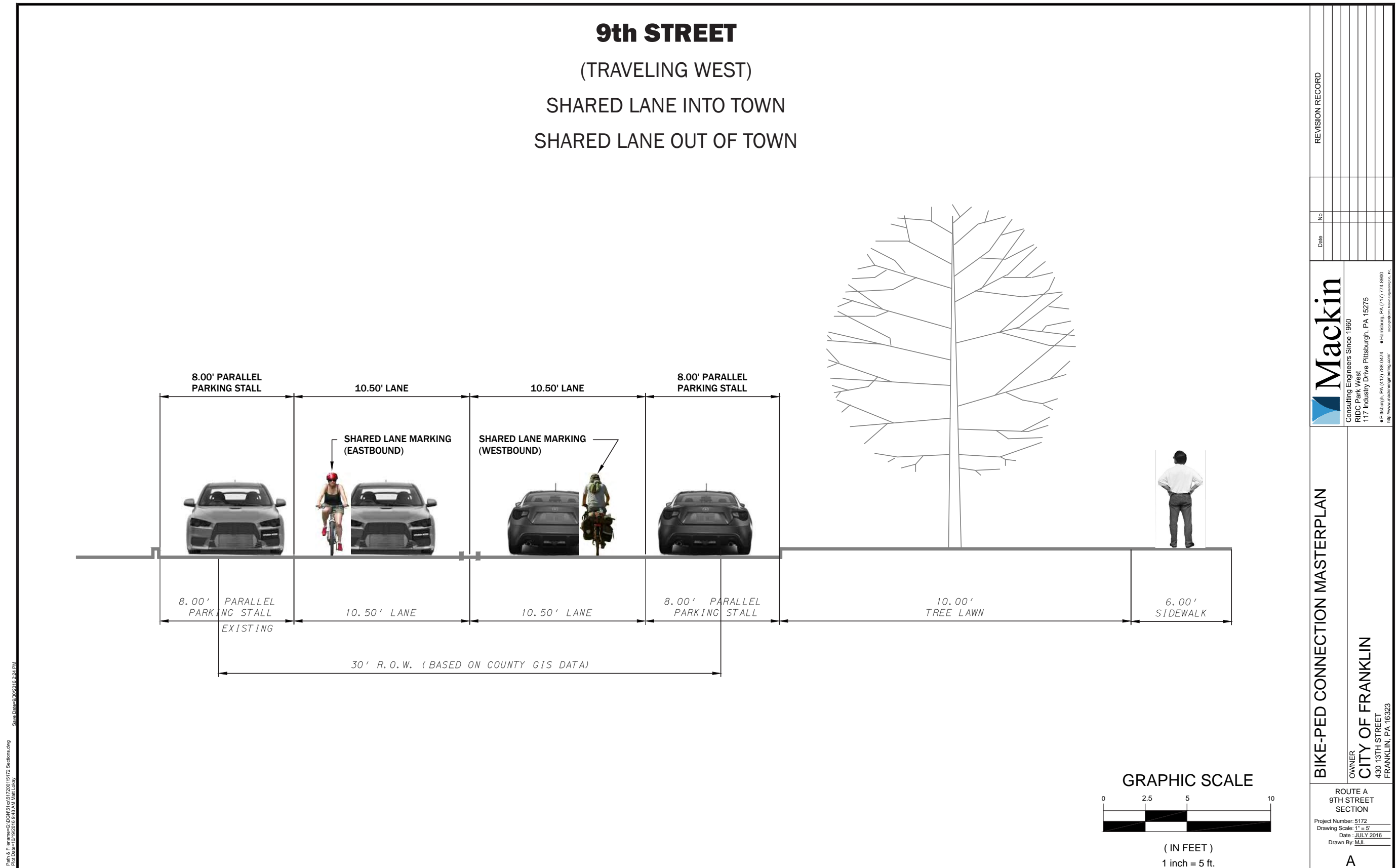






Figure 28: Cross Section of Proposed Improvements to 9th Street



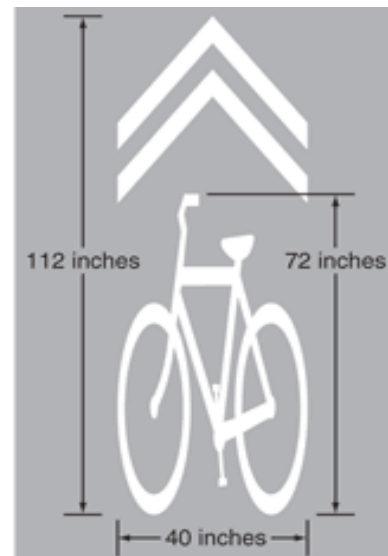




# Priority Area #4: Elk Street & Buffalo Street Loop

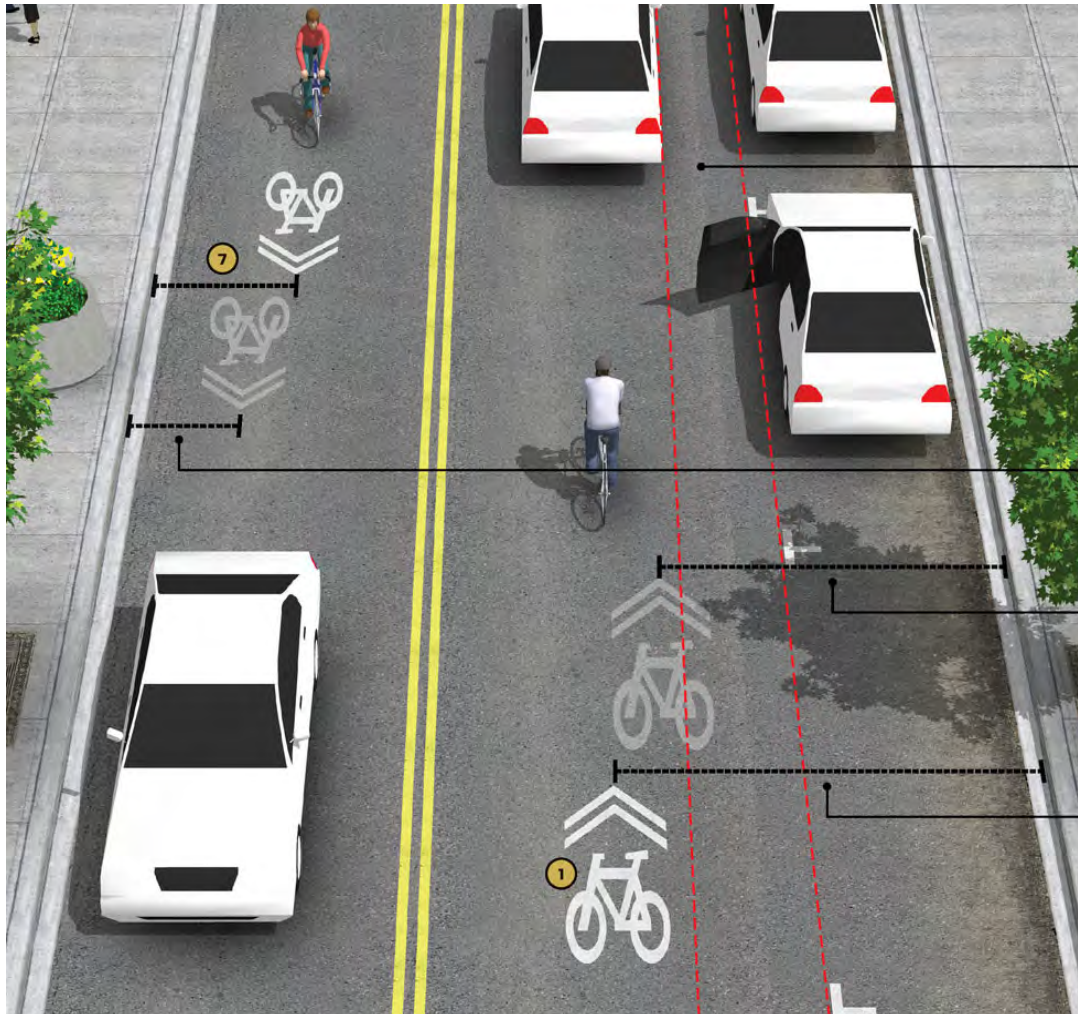
The following improvements should be considered for Elk and Buffalo Streets from 14th Street to 8th Street:

- Install “Share the Road” signs and Shared Lane Markings (SLM) for Bicycles along the alignment.
  - » Elk and Buffalo are 25 MPH, low volume, one-way traffic (Elk - between 8th and 14th Streets) and SLM will provide a one-way loop around the downtown between 8th and 14th Streets.
  - » Other options considered a Shared Use Trail above the curb- river side of Elk; however, it was not recommended by the committee due to impacts to residents and construction costs.
- Improvements will also include upgrades to the sidewalks, crossings and ADA curb ramps.
- Add Wayfinding Signs to direct users into the downtown for shops, services, restaurants and parks.



Example of a road with SLM. *Manual of Uniform Traffic Control Devices (MUTCD) Chapter 9B, MUTCD 2009 Edition, for Shared Lane Markings.*





4 Lateral placement is critical to encourage riders to avoid the 'door zone.'

7 Minimum placement: 4 feet

4 Minimum placement: 11 feet

5 Preferred placement on 25mph streets: center lane of travel

Source: National Association of City Transportation Officials (nacto.org)





Figure 29: Elk Street & Buffalo Street - One Way Loop

LEGEND

 Share the Road (sharrows)

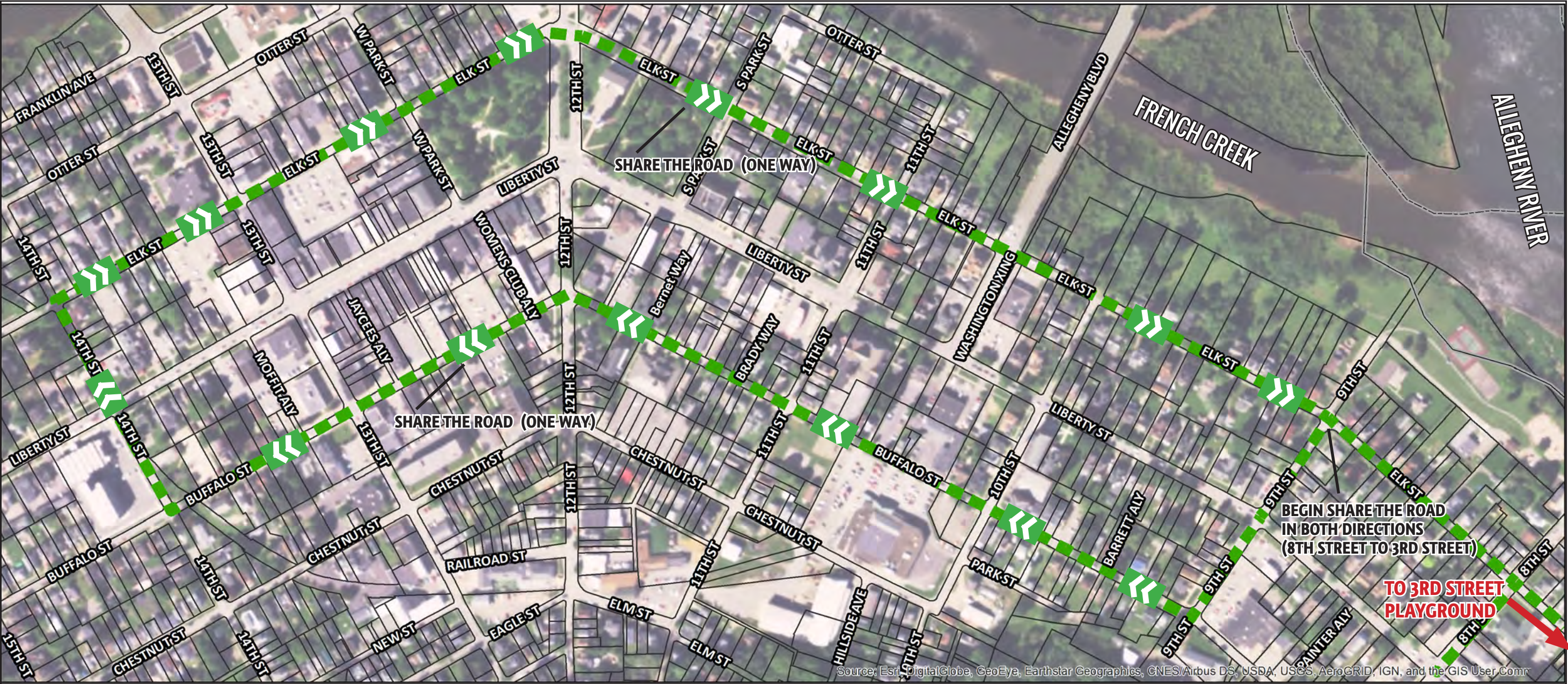






Figure 30: Cross Section of Proposed Improvements to Elk Street between 14th and 8th Streets

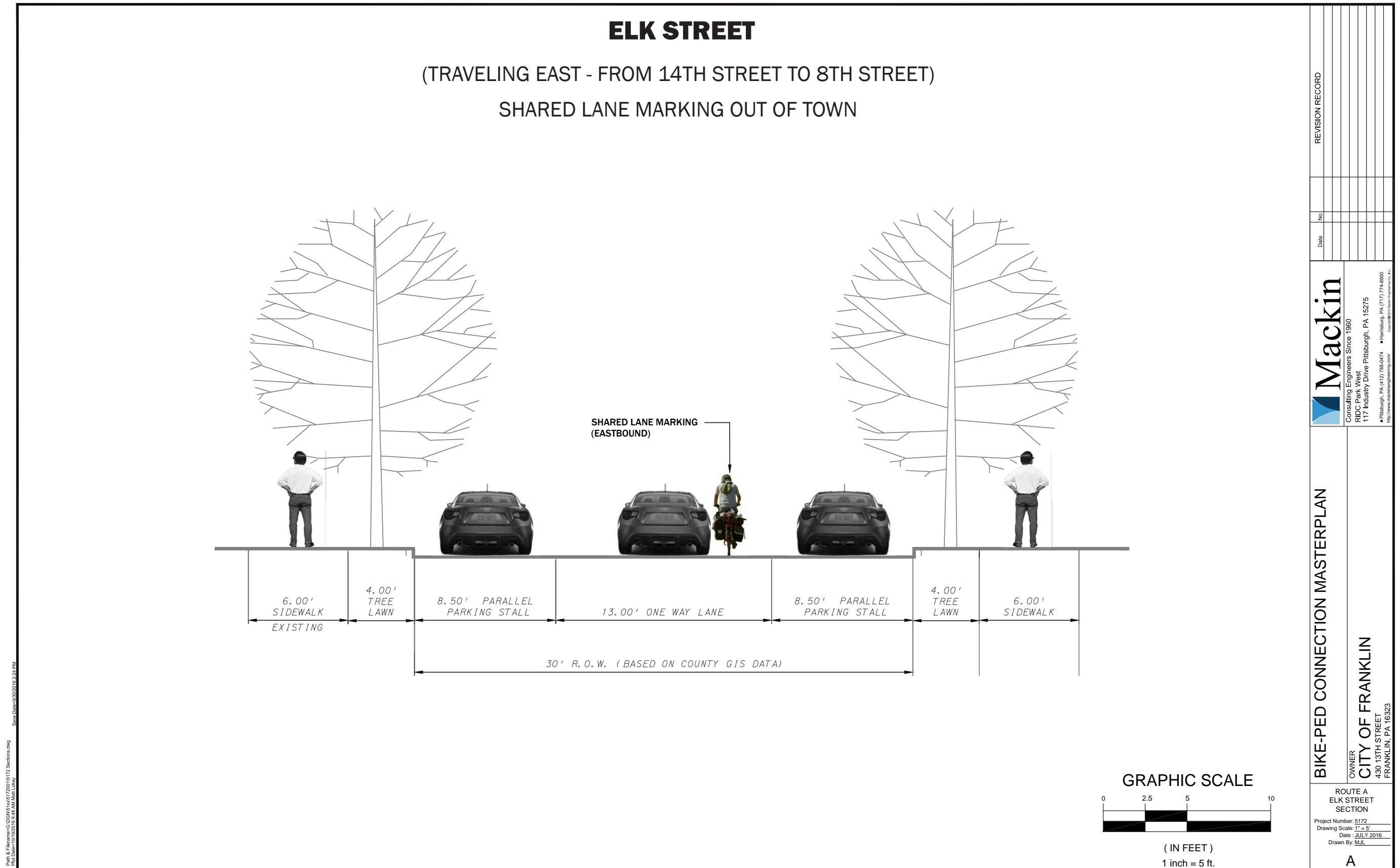






Figure 31: Cross Section of Proposed Improvements to Buffalo Street between 14th and 8th Streets

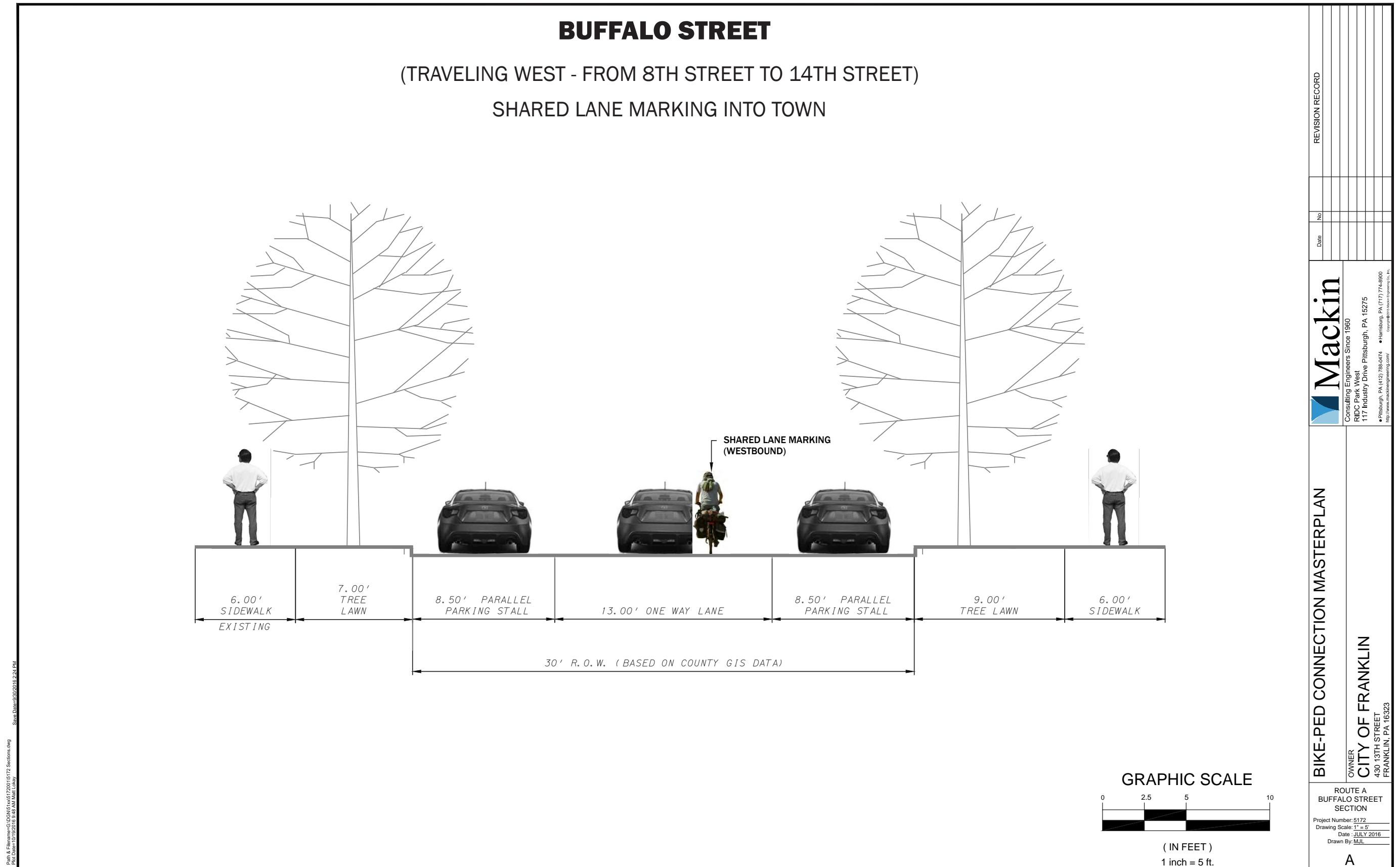
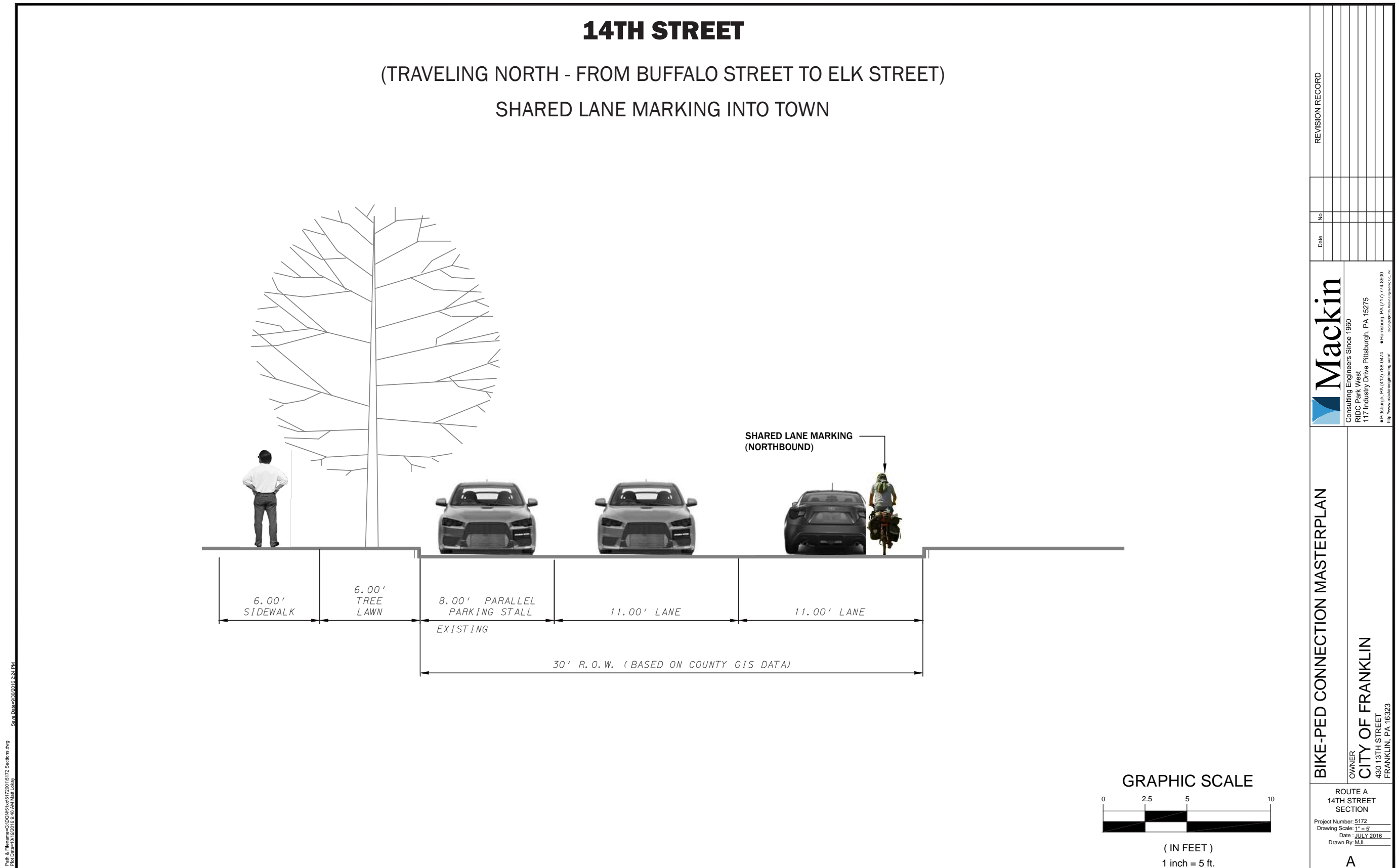






Figure 32: Cross Section of Proposed Improvements to 14th Street between Buffalo and Elk Streets





# Priority Area #5: Intersection of Liberty Street & 12th Street

The following improvements should be considered to improve the safety of the Liberty Street and 12th Street intersection:

- Complete a traffic study at the intersection, including a focus on pedestrian safety, sight distance and vehicle design speed. Options for improvements may include:
  - » Curb extensions at pedestrian crossing routes- to limit crossing distance at Liberty Street
  - » Road realignment to increase sight distance at bend in intersection
  - » Roundabout design to eliminate traffic signal, improve traffic calming, increase pedestrian mobility and cross street access.



*Liberty Street & 12th Street Intersection - looking east on Liberty Street across 12th Street  
(source: Google Maps Streetview, Image date: 2015)*







*Example of bicycle infrastructure at hi- visibility crossings:*

- » *Stop bar, bike box/crossing, bike traffic signal*
- » *Pedestrian crossing*
- » *Pedestrian safety island at large intersection-*



# Priority Area #6: Liberty Street (13th Street - 12th Street)

The following improvements should be considered for Liberty Street between 13th Street and 12th Street:

- Investigate a vehicle lane reduction from 4 lanes to 3 lanes between 12th Street and 13th Street for bike lane installation and for traffic calming. Design recommendations include:
  - » Remark roadway for two travel lanes
  - » Create a 14' wide center turning lane
  - » Increase parallel parking to 9' wide along curb
  - » Add 5' bike lane to each side of travel lane
- Franklin to request PennDOT to conduct a traffic study for a lane reduction, to include 13th Street, from Grant to Liberty and Liberty Street from 13th to 12th.
- Replace or reconfigure deteriorating sidewalk pavers in this block.
- Replace and upgrade decorative lighting with energy efficient ballasts and rust-proof bases.

## **Alternative Designs-**

*Investigate and coordinate with PennDOT for Bicycle Infrastructure alternatives:*

- Bike Lane WB on Liberty and a Shared Lane EB on Liberty.
- No lane reduction is required- remark roadway as 7' parking each side, 4-travel lanes at 11.75' each, 5' WB bike lane.
- Negative- This design does not provide for an eastbound bike lane and limits the size of the parking spaces to 7'.







Figure 33: Liberty Street - 13th Street to 12th Street

LEGEND

- Bike Lane
- Shared Use Path (pedestrians and bicycles)  
& Sign Road 'Share the Road'
- Crosswalk
- Share the Road (sharrows)

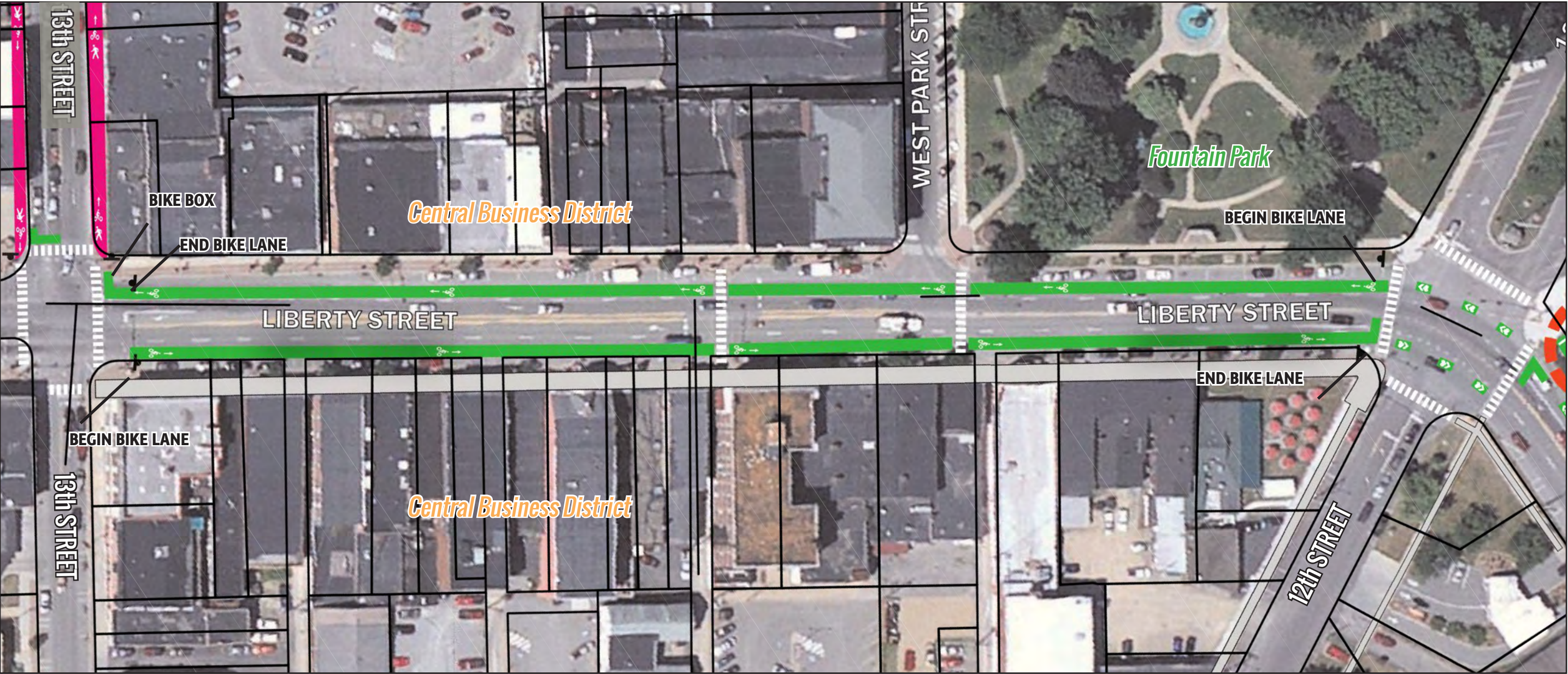
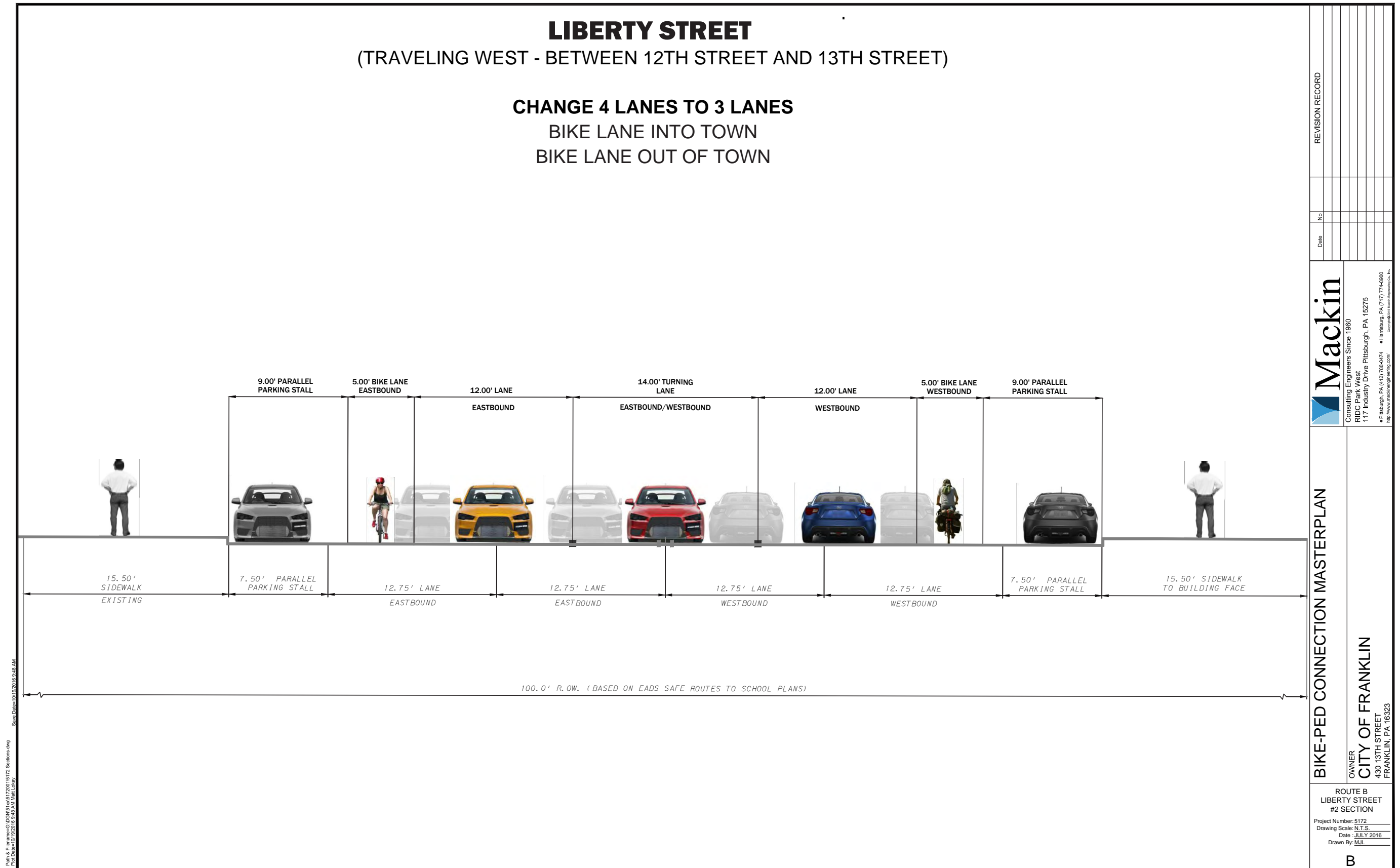






Figure 34: Cross Section of Proposed Improvements to Liberty Street between 12th and 13 Streets







# Priority Area #7: Liberty Street - 12th Street - 9th Street

The following improvements should be considered for Liberty Street between 12th Street and 9th Street:

- Conduct field survey and ROW plan for the north side of Liberty Street (river side). Design the 1-way (WB) cycle track as a separated, above the curb installation to include a 5' bike lane with a 2' buffer space between cycle track and 5' sidewalk.
- Reconstruct the walk to accommodate cycle track, especially at intersections and to coordinate grading. Portions of the existing sidewalk may be maintained. Design should be planned to minimize impacts to utilities and valuable street trees.
- Sign and install pavement markings for travel direction at the cycle track and the sidewalk.
- Add Wayfinding Signs to direct users into the downtown for shops, services, restaurants and parks.



*Separated- vertically and horizontally between road and track  
Pavement delineation- between walking lane and cycle lane  
Walking lane on building side to access homes and shops  
Cycle lane on road side to access intersections*

## **Alternative Designs-**

*Investigate and coordinate with PennDOT for Bicycle Infrastructure alternatives:*

Alternative Design would reduce the cost of building a block of the cycle track by investigating the removal of parallel parking along north side of Liberty Street between 9th Street and 10th Streets. The alternative design proposes the parking lane be converted into a Buffered Bike Lane.



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Figure 35: Liberty Street - 12th Street to 11th Street

LEGEND

One-Way Raised Cycle Track

Crosswalk

Share the Road (sharrows)

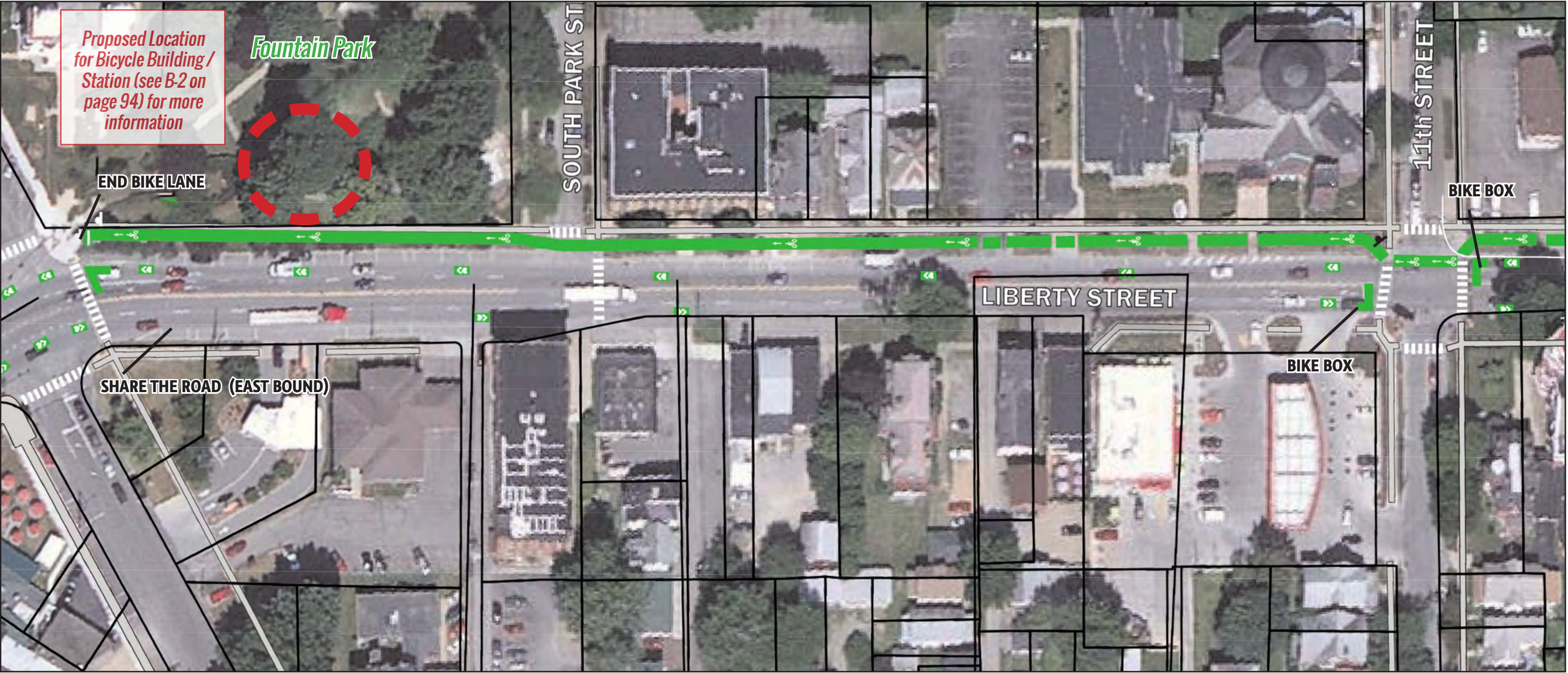






Figure 36: Liberty Street - 11th Street to 9th Street

LEGEND



One-Way Raised Cycle Track



Share the Road (sharrows)



Crosswalk

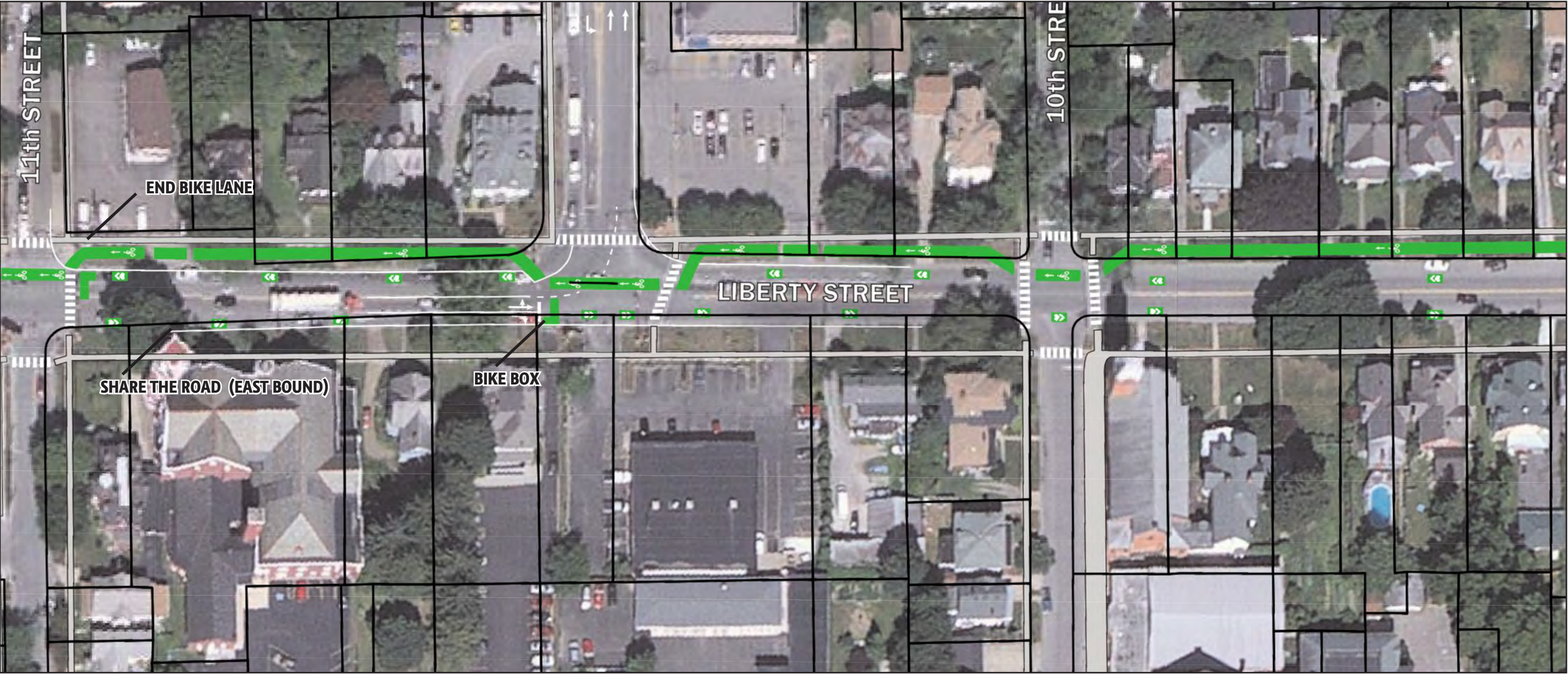




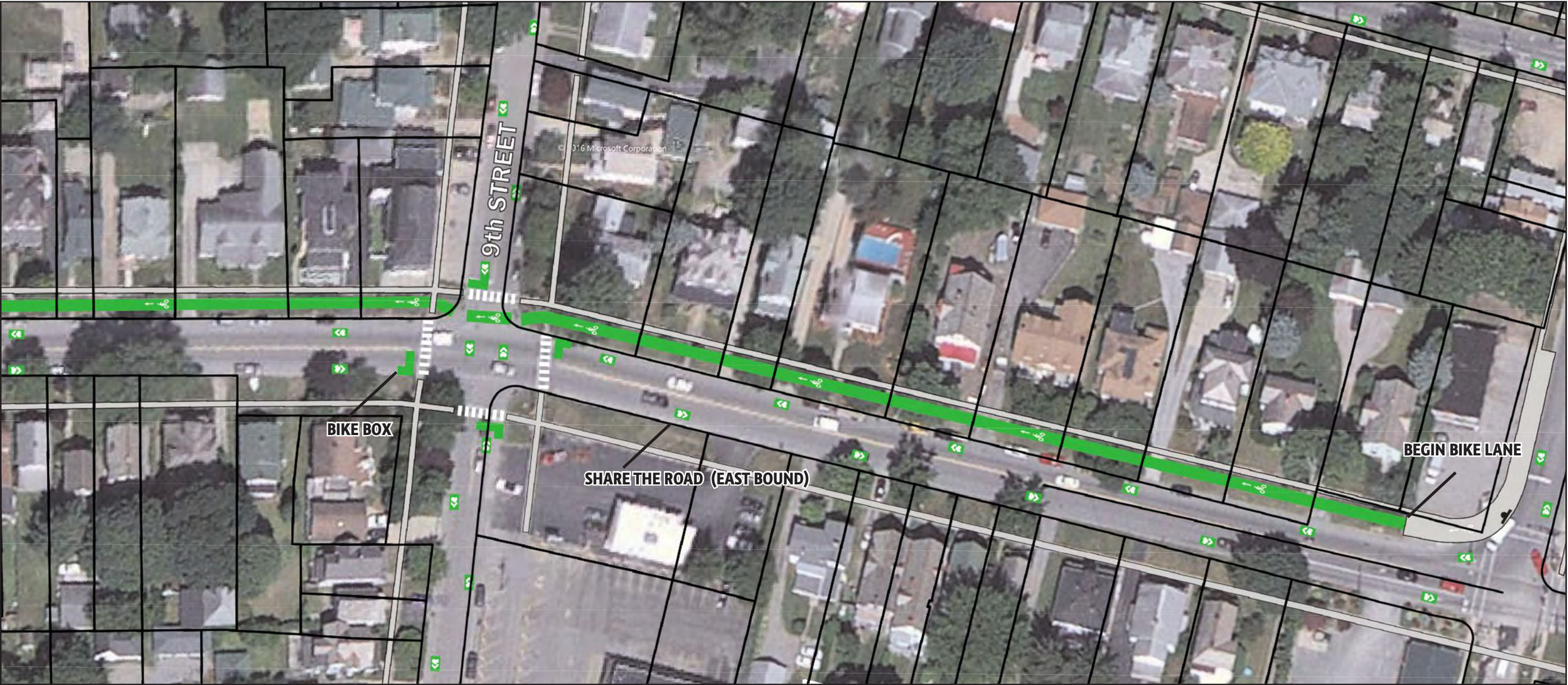




Figure 37: Liberty Street - 9th Street to 8th Street

LEGEND

- One-Way Raised Cycle Track
- Crosswalk
- Share the Road (sharrows)









# Priority Area #8: Liberty Street - 9th Street - 8th Street

The following improvements should be considered for Liberty Street between 9th Street and 8th Street:

- Conduct field survey and ROW plan for the north side of Liberty Street (river side). Same as above Cycle Track design.
- The cycle track design is to minimize impacts to utilities and large trees.
- Sign and install pavement markings for travel direction.
- Add Wayfinding Signs to direct users into the downtown for shops, services, restaurants and parks.

## **Alternative Designs-**

*Investigate and coordinate with PennDOT for Bicycle Infrastructure alternatives:*

Alternative Design would reduce the cost of building a block of the cycle track. Investigate the removal of parallel parking along north side of Liberty Street between 8th Street and 9th Street and replace with a Buffered Bike Lane.

- This space can be used for the installation of a low-cost WB- buffered, bike lane. A buffered bike lane would provide protection and added comfort for cycling.
- This will provide an economical solution and a separated, buffered bike lane into town.
- Users would then enter the Cycle Track at 10th, through the busy intersection of Washington Crossing and continue on the Cycle Track to 12th Street and downtown.



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# Priority Area #9: 13th Street - Connection to the Third Ward

The following improvements should be considered for the connection to the Third Ward via 13th Street:

- Coordinate with PennDOT for a bike route along 13th Street and the 13th Street Bridge.
  - » Mark and Sign 13th Street as 'Share the Road' with delineated travel lanes at 10' wide. Remark the travel lanes and the road edge for wider shoulders from Liberty Street to Otter Street.
  - » From Otter Street to Grant Street, repair the sidewalk on each side of the road. Widen the sidewalk system north of the bridge- from the curb to 12' wide. Sign and mark the sidewalks as a 'Shared Use Path' to delineate travel direction. Allow bicycles and pedestrians to share the path; delineate a centerline with direction arrows and signs to mark travel directions.
  - » Add hi-visibility crossings to all pedestrian crossing routes; improve and upgrade curb ramps.
- Post 13th Street as 25 MPH from the Grant Street Intersection, across the bridge, and into the City. Currently, the road is posted 25 MPH at Franklin Avenue, south end of bridge.
- Add Wayfinding Signs to direct users into the downtown for shops, services, restaurants and parks.



North 13th Street - looking south towards 13th Street Bridge and downtown  
(source: Google Maps Streetview, Image date: 2014)



### **Alternative Designs-**

*Investigate and coordinate with PennDOT for Bicycle Infrastructure alternatives:*

#### **Alternative Design A-**

- City of Franklin to request PennDOT to conduct a traffic study for a lane reduction from 4 to 3 lanes including a center turning lane. This same study should include both 13th Street, from Grant to Liberty, and Liberty Street. The proposed improvement is for traffic calming purposes and the addition of NB/SB bike lanes.
- Reduce Lanes from 4 to 3 lanes, add NB/SB bike lanes on 13th Street and 13th Street Bridge from Grant to Liberty.
  - » Lane reduction
  - » Create a center turning lane
  - » Add 5' bike lanes to each side of road
- This design does not require sidewalk widening since bikes will have a lane.

#### **Alternative Design B-**

- City of Franklin to request PennDOT to conduct a traffic study for a lane diet from 11' to 10' lanes- keep 4 lanes. The proposed improvement is for traffic calming purposes and the addition of SB bike lane.
- Remark 4- travel lanes as 10' wide and shift the lanes east to provide space for a 5' southbound (SB) bike lane and a NB shared lane marking on 13th Street and 13th Street Bridge from Grant to Liberty (does not include sidewalk widening).
  - » Lane diet, keep 4- travel lanes
  - » Add 5' bike lane SB (entering town)
  - » Add Shared Lane Marking NB (exiting town)





Figure 39: Connection to the Third Ward: Grant Street to Howard Street

LEGEND

Shared Use Path (pedestrians and bicycles)  
& Sign Road 'Share the Road'

Crosswalk

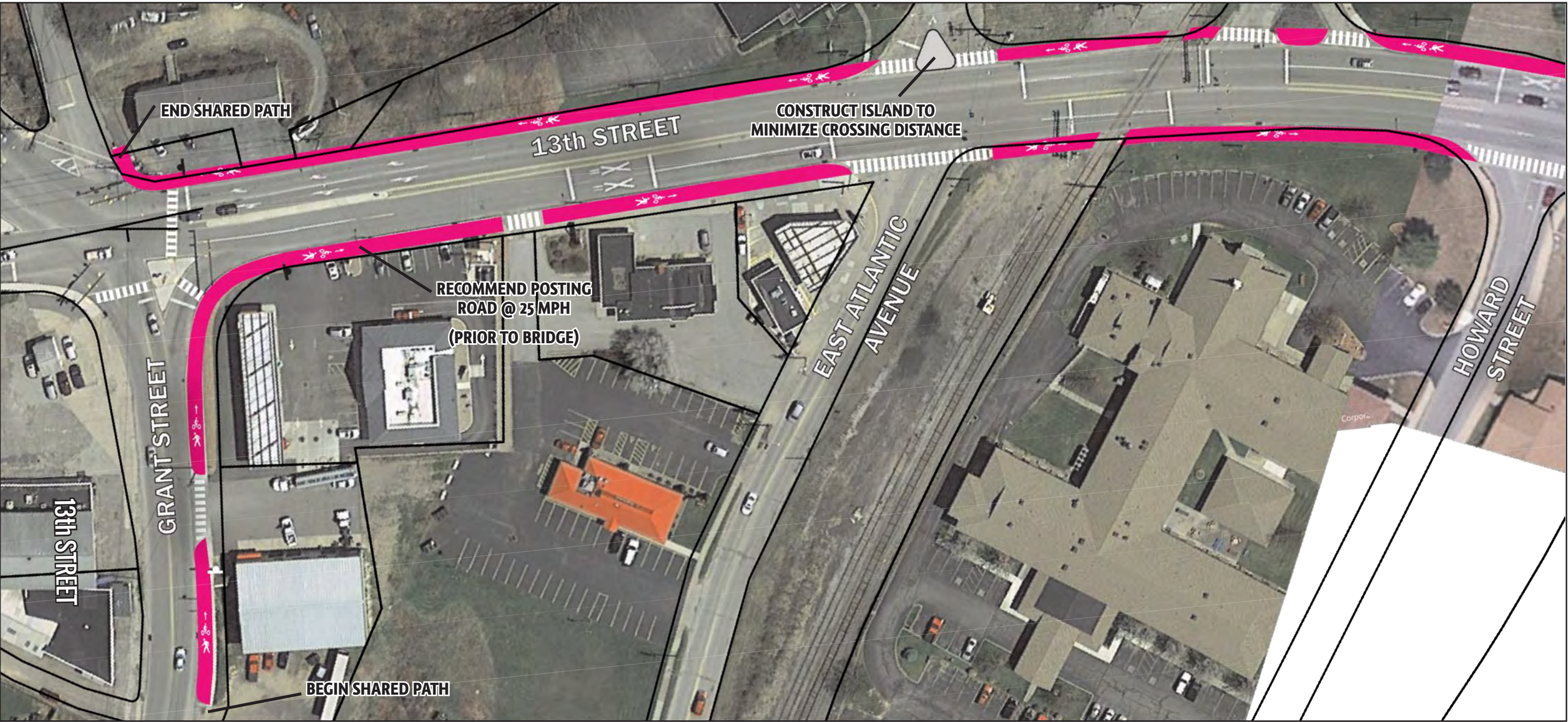








Figure 40: Connection to the Third Ward: Third Street Bridge

LEGEND

 Shared Use Path (pedestrians and bicycles  
& Sign Road 'Share the Road'

 Crosswalk

 Share the Road (sharrows)

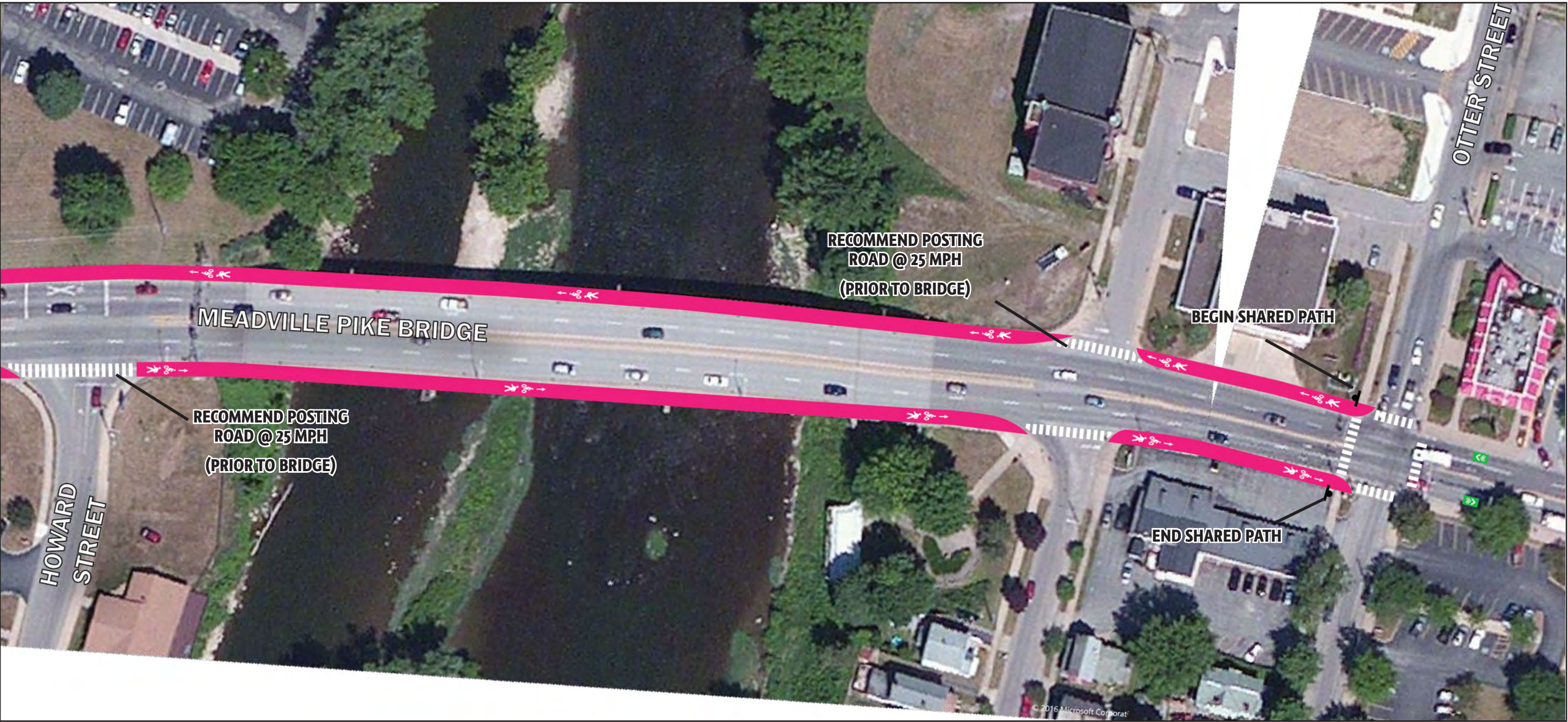








Figure 41: Connection to the Third Ward: Otter Street to Liberty Street

LEGEND

- Bike Lane
- Shared Use Path (pedestrians and bicycles)  
& Sign Road 'Share the Road'
- Crosswalk
- Share the Road (sharrows)









# Priority Area #10: Elk Street Extension (8th St - 3rd St)

The following improvements should be considered for Elk Street from 8th Street to 3rd Street:

- Since Elk Street, between 8th Street and 3rd Street, is posted at 25 MPH and has lower traffic volumes; a shared lane design is recommended
- Shared Lane Markings (SLM) for both northbound and southbound (two-way traffic), with the sidewalk separate for pedestrian travel.



*Elk Street - looking south from 8th Street  
(source: Google Maps Streetview, Image date: 2014)*



## *B-2: Develop additional bicycle infrastructure to enhance the bicycle experience.*

- Bicycle Building and Shelter
  - » A recommended public space improvement to welcome cyclists into the downtown is a centrally located bicycle building; a possible location is Bandstand Park, next to the Venango County Courthouse.
  - » The bicycle building would be a multi-functional unit and should be designed to provide a leasable vendor space, bicycle repair station, bike parking and bike lockers, information kiosk, and Wi-Fi access. At a minimum, the building should provide an overhead cover, storage lockers for bikes and adjacent bike racks.
  - » The final design of the bike building, bike shelter, bike racks and bike lockers should consider the context and character of the surrounding architecture; the buildings, building materials and color. Examples of potential facilities are shown on Figure 42 on page 95.

*Cyclists are more likely to park where their bicycles are safe and protected from weather. Prolonged exposure to rain can rust a bike's metal frame and components and the sun's ultraviolet rays can deteriorate a bike's soft seat and tires.*

*At least 50% of long-term bicycle parking should be covered. Take advantage of existing overhangs or awnings - this is a creative, low-cost way of providing protection and cover.*

*Enclosed bicycle lockers (see Figure 42 on page 95) are a good solution for parking areas where security is in question or where there is limited opportunity to provide weather protection.*

*Daily bicycle commuters are generally willing to walk a short distance, about three blocks, if they are confident the parking area is secure.*



Figure 42: Examples of Bicycle Facilities



*Bike building at Colorado University*



*Food vendor in a community park*



*Bicycle locker - good for multi-day trail users or cyclists with expensive bikes and equipment.*



*Bicycle repair station - can be a stand-alone unit near the trail, park, bus stop or as part of the bicycle building.*

*Bike racks with covered shelters - can be stand-alone or attached to a building.*





### *B-3: Review and update, as necessary, the City's zoning ordinance and design plans to encourage more bicycle-oriented amenities.*

- Consider offering incentives to businesses to offer bicycle parking. Most zoning codes require a minimum supply of automobile parking based on the land use. As the need grows, Franklin should consider offering incentives for businesses along the bike route to substitute a portion of the required automobile parking. An example from Portland, Oregon is provided in Table 2. The City should review its zoning ordinance to determine if adjustments could be made to better accommodate cyclists within the downtown.
- Encourage more outdoor café spaces. Establish a “build-to” line that requires fencing/wall around open spaces along the sidewalk. Permit sandwich signage boards along the sidewalks. Permit vendors to locate on the sidewalk or in public spaces. Encourage people to “buy local” and for businesses to sell locally made/grown/produced products.
- Help support more businesses to cater to bicyclists by upgrading landscaping, sidewalks, lighting, signs, pavement and markings.



*One car parking space can fit 10 bicycle parking spaces; that's 9-more customers to the door.*

<b>Table 2. Portland's Bicycle Parking Zoning Requirements</b>	
Primary or secondary school	10% of the number of students, plus 3% of the number of employees.
Commercial – retail or office	One space per 3,000 sq. ft. of commercial space or 5-10% of the number of automobile spaces.
Sport and recreation center	10-20% of the number of automobile spaces.
Movie theater or restaurant	5-10% of the number of automobile spaces.
Industrial	2-5% of the number of automobile spaces.
Multi-unit housing	1 space per 1-2 apartments.
Public transit stations	Varies, depending on usage.



#### ***B-4: Calm traffic along priority roadways to increase the safety and visibility of cyclists and pedestrians.***

Each item is to be considered as part of pedestrian and bicycle improvement projects.

- Coordinate the timing of traffic signals along Liberty Street, 13th Street and 8th Street with PennDOT traffic study.
- Consider curb extensions at the intersections of Liberty Street and 13th Street and Liberty Street and 12th Street; Liberty Street and Washington Crossing and Liberty Street and 9th Street. Curb extensions limit the crossing distance for pedestrians, create a narrow view of the roadway- to slow traffic to the posted speed; and protect parallel parking areas.
- Investigate the potential to design a roundabout at 12th Street (PennDOT Project)
- Improve intersections to include re-markings and hi-visibility crosswalks on Liberty Street at 13th Street, 12th Street, 9th Street and 8th Street.
- Delineate the road edge / parallel parking spaces along the bike route.
- Move the 25 mph speed limit sign for the 8th Street Bridge (RT322) from the west side to the east side of the bridge to slow traffic entering the City and provide a safer travel for cyclists using the road bridge.
- Move the 25 mph speed limit sign for the 8th Street Bridge (RT322) from the south side to the north side of the bridge to slow traffic entering the City and provide a safer travel for cyclists and walkers using the road bridge.

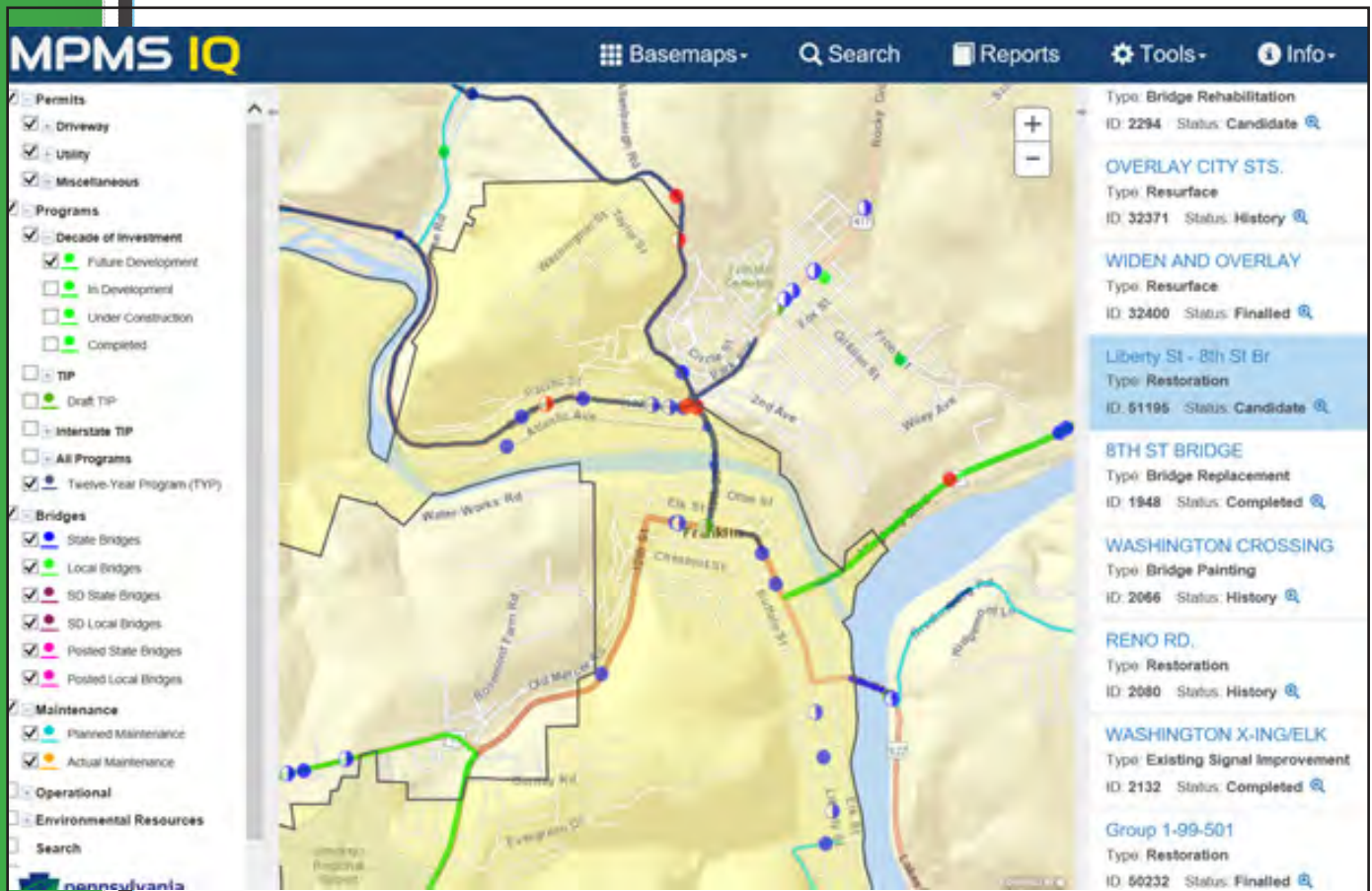
#### ***B-5: Coordinate with the City's Street Department and other agencies for road improvements.***

As road projects are planned (at the City, County and State levels), any projects involving resurfacing, repairs and utility extensions or replacements should take the proposed bike lanes and infrastructure recommendations from this Plan into account.

- The City of Franklin's Road Supervisor stated that the 900 block of Buffalo St, 1000 block and 800 block of Elk St may get paved in 2017.
- Work with PennDOT to modify bridges and state routes to incorporate traffic calming and bicycle lanes, or shared lane markings and signs (refer to report for recommended project corridors).
- Assign the task to review and monitor PennDOT's MPMS IQ system. This is a web-based GIS mapping application for accessing highway and bridge project data for the Commonwealth of Pennsylvania. Figure 43 on page 98 shows the upcoming DOT projects including road resurfacing, and bridge maintenance projects in the Franklin area. It is very important to understand when and where the next PennDOT project will happen, and to continually communicate with DOT for implementation of recommended bicycle infrastructure projects.



Figure 43: PennDOT MPMS IQ



Source: [http://www.dot7.state.pa.us/MPMS\\_IQ/Mapping](http://www.dot7.state.pa.us/MPMS_IQ/Mapping)





# Signage:

## Install pedestrian and bicycle wayfinding signage throughout the City.



Franklin, Pennsylvania  
Always Eventful

**Wayfinding can be defined “as spatial problem solving. It is knowing where you are in a building or an environment, knowing where your desired location is, and knowing how to get there from your present location.”<sup>1</sup>**

One of the priority issues identified in previous plans, including the Franklin/Sugarcreek Route 8 Complete Streets Plan and the Franklin Trail Town Assessment, was the lack of signage, particularly directing people (cyclists and pedestrians) to and from the downtown business district and the Samuel Justus Recreational Trail. The Assessment noted that many people were unaware of the Saltbox Visitors Center. One of the priorities for this Plan is to improve the overall connection from the Trail to downtown Franklin. The heart of downtown Franklin is approximately 1.5 miles away from the Samuel Justus Recreational Trail Visitor Center (Saltbox) and the Franklin Trailhead.

### *S-1: Install wayfinding signs along the proposed bicycle and pedestrian routes.*

Trail and Bike Route destination wayfinding signs should be designed as a hierarchy network, similar to the highway system. First alerting users for major destinations ahead, then signing for food, services, comfort station, recreation or cultural resource destination, etc. It is recommended that signs use standard steel post and aluminum sheet material, reflective letters/graphics and comply with MUTCD standards for materials, design and location. The sign should be a single color, simple style, with easily read letters and supported by a directional arrow, a universal graphic and may include a mileage distance. Proposed locations for wayfinding signs are shown on Figure 44 on page 100.

<sup>1</sup> <http://www.umich.edu/~wayfind/supplements/moreinfo/main.htm>



Figure 44: Proposed Locations for Trail Wayfinding Signs (to and from Franklin Trailhead/Downtown)



1 | FRANKLIN



2 | BERKELEY, CA



3 | CITY OF GRESHAM



Examples of proposed wayfinding signage from the Complete Streets Route 8 Franklin/Sugarcreek Plan

// City of Franklin Pedestrian and Bicycle Connection Master Plan

Signage placement and pavement markings within PennDOT road right-of-way will need approval. It is recommended that a pre-application meeting be scheduled to review the proposed improvements; then, the City should submit a site plan showing improvements along with an agreement for maintenance. The priorities for wayfinding signage are as follows:

- Samuel Justus Recreational Trail / Franklin Trailhead -
  - » Add destination and service signs at ahead and at turns- leading into Franklin
  - » Sign the Samuel Justus Recreational Trail with ahead and turn signs for Services, Shops, Food and Destinations, similar to the highway road signs on interstates that alert travelers for upcoming attractions and destinations. Continue the signs into Franklin as bike route wayfinding signs.
  - » We recommend the City coordinate with the Erie to Pittsburgh Trail Alliance (EPTA). The Great Allegheny Passage Trail has generously allowed the EPTA to adopt parts of its signage system.
- In Town-
  - » Add Bicycle Route Wayfinding and Pedestrian Route Wayfinding to direct pedestrians and cyclists to the Samuel Justus Recreational Trail, the Saltbox Visitors Center and other destinations (civic, cultural, recreational, historic).
- Bike Route Mapping -
  - » The bike route mapping should be available for public consumption- city website; windows of trail town business and community centers; and parks and public spaces.
  - » Kiosks should include the bike route map and should be installed at the Franklin Trailhead; both entrances to the Riverfront Park trail; and throughout the central business district.



*Examples of trail town signage along the Great Allegheny Passage trail (source: <https://www.trailtowns.org/2014/10/08/heading-the-right-direction-on-the-great-allegheny-passage-trail>).*





## *S-2: Install gateway signs at the primary entrances into the City.*

- A recommended primary gateway is on the north side of the Allegheny River; at the intersection of the 8th Street Bridge (RT322) and Bredinsburg Road; in Cranberry Township. A gateway design at this location provides a significant opportunity to calm traffic for bicycles and pedestrians traveling between trail and town.
  - » The gateway design would include moving the posted speed limit of 25 mph from the south end of the bridge to the north end (the north end is currently posted at 35 mph).
  - » The gateway design would be reinforced by the new speed limit, and create a sense of arrival for travelers entering Franklin.
  - » Additionally, a new gateway would enhance the current farmer's market at the corner of Bredinsburg Road and the 8th Street Bridge.
- A second recommended gateway and corridor redesign project is at the current welcome sign along Route 8 northbound (15th Street).
  - » The northbound entrance into the City could be enhanced with landscaping and lighting and reinforce the sense of arrival to Franklin.
  - » A 'Bicycle Friendly Community' placard is recommended to be hung under the current Franklin Welcome Sign.
- A third gateway is at the intersection of 13th Street and Franklin Avenue at French Creek.
  - » A gateway design would reinforce a sense of arrival for travelers entering the city from a SB direction.
  - » Additionally, lowering the speed limit from 35 mph to 25 mph at the north side of the 13th Street Bridge, at Grant Street, would help to calm traffic before crossing French Creek into town.
  - » This would help to calm traffic and minimize conflicts between automobiles, bicycles and pedestrians traveling between the Third Ward and downtown.

*Example of a landscaped gateway welcoming visitors to a community. Mackin Engineering designed the gateway as part of the Streetscape Plan for the Municipality of Murrysville, Westmoreland County. (Photo credit: Google Maps Streetview)*



### *S-3: Install bike route signs and pavement markings along the designated bike routes throughout the City.*

Bicycle route signs alert motorists that cyclists are in the corridor and provide wayfinding for cyclists. Bike route signs are critical at turns, both ahead of the turn and after each turn. Bicycle regulatory signs (Share the Road) and bike route signs should be installed at approximately 300-500 feet on-center and ahead and beyond intersections and turns. Each route should be designed individually, since each route is unique and requires safety and accessibility considerations.

If the route is marked as a shared lane or bike lane, then signs should be supported with a pavement marking. These pavement markings are typically spaced between 200'-250' on-center and ahead and beyond each turn.



*Pavement markings and signs are typically installed at each turning point with a marking and sign ahead of the turn and a marking and sign directly following the turn. This will enhance the wayfinding ability for cyclists at decision points.*

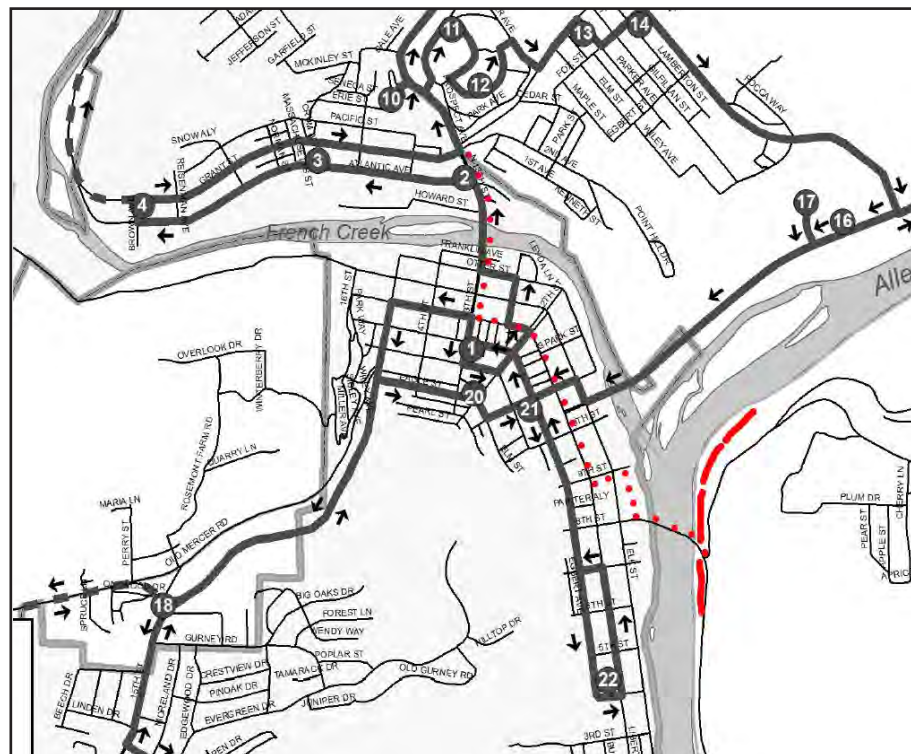


# Transit:

## Improve the bicycle and pedestrian connections to the public transportation system.



**Venango County provides public transportation for the City of Franklin via VenanGo Bus. VenanGo Bus operates both a Franklin Route as well as an inter-city route to regional attractions including the Cranberry Mall, Oil City, Sugarcreek Town Center and other areas.**



*Venango County Public Transportation - Franklin Bus Route (shown in black) with the existing and proposed trail connections (shown in red)*



### *T-1: Coordinate with VenanGo Bus to improve the connection between the transit system and the pedestrian/bicycle network.*

Existing bus stop locations within the project area include:

1. Buffalo Street Gazebo, near 13th Street
  2. Buffalo and 11th Street Shop
  3. Railroad Street and 12th Street
  4. Atlantic Avenue and North 13th Street- Third Ward
  5. Save a lot / Family Dollar Shopping Center, at Atlantic Avenue- Third Ward
- Establish new bus stops at the Shop n Save plaza at the corner of 10th and Buffalo Streets and in the Third Ward at the Save a lot / Family Dollar Shopping Center at the corner of Grant and Pine Street.
  - Outfit all buses with bike racks, to encourage people to use transit who may bike to/from their bus stop destination.

### *T-2: Create transfer stations at bus stops.*

The City of Franklin should coordinate with the County in order to improve the accessibility and visibility of the public bus stops with the pedestrian and bicycle network. Having accessible, visible and safe transfer stations are important infrastructure to encourage people to try bicycling as part of their daily commute and to support a year-round bicycle network. Existing bus stops should be improved to offer the following:

- ADA accessible approaches and surfaces, overhead shelters, bicycle lockers or racks, lighting and smart technology, with 'real time' route schedules and arrival times.
- Each stop should be outfitted with adjacent car parking and transit pick-up/drop off areas. Final locations, property right-of-way and final design for the stations is to be coordinated with the transit authority, landowners and the City.



*Example of combination bus/bike shelters to facilitate multi-modal transportation. (Source: Velodrome Shelters, <http://www.prweb.com/releases/2014/07/prweb11985995.htm>)*



### *T-3: Coordinate with the Northwest Commission regarding the feasibility of developing a ride-share.*

- The Northwest Commission is currently undertaking a ride-share feasibility study for the region. Bike Share Programs are where bicycles are available to rent at a variety of locations within a community. While historically, only major cities have operated bike share systems, smaller cities have been developing them in recent years.
- Potential partnerships could be developed with UPMC as well as local large employers.
- Locally, the City of Pittsburgh began a bike share program a few years ago called Healthy Ride, partnering with Highmark and the Allegheny Health Network. Smaller cities that have instituted bike share systems have used different companies, such as Zagster and Social Bikes, which cater to smaller cities, towns and universities. Zagster offers feasibility analyses as a complimentary service for cities and towns that are considering bike share. The feasibility analysis includes a bike share overview, demand analysis, comparative analysis, scope and phasing plan.<sup>1</sup>

<sup>1</sup> "Get a Bike Share Feasibility Analysis." Zagster. <http://www.zagster.com/feasibility>. Retrieved online October 2016.



(Photo credit: <https://www.galvanizeit.org/project-gallery/gallery/healthy-ride-pittsburgh>)



# Programs: Expand and increase the programs offered.

**The BFC Report Card identified several areas that the City should address with regards to programs related to the education, encouragement, enforcement and evaluation and planning that will make Franklin a more bicycle (and pedestrian) friendly community.**

While the BFC program is focused on bicycling, the City should also take into account the need to offer pedestrian-oriented programs as well.

## *P-1: Develop New Bicycle Programs and enhance existing programs that increase bicycle use, safety, infrastructure, measurement and maintenance of the system.*

Recommendations meeting the 5E categories will be helpful in applying for the BFC Silver status level. Note: the first “E” is Engineering, which is addressed under the Bicycle goal and associated recommendations.

### **E**ducation - giving people of all ages and abilities the skills and confidence to ride.

- A bicycle rodeo focusing on bicycle skills and safety was held in the summer of 2016 for children ages 5 to 10. The event was sponsored by the City of Franklin, PennDOT, the Venango County Sheriff’s department and the Franklin Area Chamber of Commerce. Helmets were provided to some children who did not have them and two bicycles were given away at the event, thanks to the Venango Chapter of Alliance of Bikers Aimed Toward Education. The event – which drew about 15 participants – was regarded by organizers as a success. It was their first such effort and they expect more children to participate next year and in coming years as public awareness of the event grows. Similar bicycle rodeos were previously sponsored by the Franklin Knights of Columbus in cooperation with PennDOT, the Sheriff’s department and Safe Kids Venango.





- This event should be held on an annual basis. Sessions should be conducted in the parks - children can bring their bikes to ride and police can lead a discussion about bicycle safety, rules of the road (for both bicycles and vehicles) and bicycle awareness for automobile drivers.
- Traffic Skills Training - Emphasize bicycle safety and bicycle riding rules - begin at schools, continue with Drivers Training Courses for senior and for new drivers.

### **Encouragement - creating a strong bike culture that welcomes and celebrates bicycling.**

- Safe Routes to School and Safe Routes to Work Program - the school district has a walk safety program. The City could partner with the School District to offer a similar bicycle safety program.
- At one time, several local residents were certified League of American Bicyclists instructors. They taught some classes at some of the local schools but had trouble attracting adults to the Smart Cycling classes. There may be an opportunity to restart a similar program.
- Start a bike repair program where kids can repair their bikes and people can drop off old bikes. A potential location would be at a current auto repair, a bike shop or a trade school.
- Celebrate Bicycling During National Bike Month with Community Rides, Bike to Work Day and Media Outreach.
  - » Among the 51 largest U.S. cities, 43 hosted Bike to Work Day events in 2010. The City of Denver reported the highest rate of participation with one out of every 28 adults participating in its 2010 Bike to Work event. That effort makes a difference: Many people who participate in their Bike to Work Day promotion as first-time commuters become regular bike commuters (source: <http://bikeleague.org/bikemonth>).
  - » Promotion – Take advantage of the promotional materials provided by The League of American Bicyclists
  - » Information giving - communicate where to ride
  - » Provide an incentive program- cash, clothes, equipment for 'doing the right thing'
  - » Establish a Get up and Ride program that kicks off during Bike To Work Day/Week. Ask participants to form teams getting their friends and coworkers to participate and track mileage.
- Host Community Cycling events or rides throughout the year
  - » Link rides to current festivals and events such as the Tour De Franklin bike ride, which was held as part of this Plan, but can be expanded to become an annual or semi-annual event, as part of either Rockin' Riverfest or Applefest.
  - » Promote cycle club rides and bike programs to the public.
  - » Offer a Mother's Day and/or Father's Day Ride.
  - » Continue to promote and expand Bike to School Activities. Since 2004, the City of Franklin has sponsored a Walk/Bike to School Day in both the





*The start of the first “Tour de Franklin,” which was held as part of the Rockin’ Riverfest in 2016 and could be expanded to become an annual or semi-annual event.*

spring and the fall in conjunction with National Walk to School Day. Partners on the event have included the Allegheny Valley Trails Association, the Franklin Area School District, the Venango County Sheriff’s Department and the Franklin YMCA. For the event, children who live in the City are encouraged to walk or bike along three routes to school – at Central Elementary School or St. Patrick School. The children are accompanied by adults and law enforcement personnel assist at major intersections. The event has been a success since its inception. At the most recent event last spring, more than 100 children participated, 61 of them on bicycles.

- Coordinate with regional resources, using digital and print media, to promote the City of Franklin as ‘Very Walkable’, ‘Bicycle Friendly’ and a ‘Trail Town’ community along the Erie to Pittsburgh Trail System.
- Create a campaign to promote the City of Franklin to the tri-state region and the east coast through digital media and magazines promoting recreation, outdoor/lifetime sports, cycling, trails, eco-tourism, leisure, and retirement living.
- Set a day for an Open Streets initiative where only bikes and pedestrians are allowed to travel through town center via Liberty Street. This may be coordinated with local festivals- Applefest, or a spring and fall event.
- Establish a Bike Friendly Businesses Program. With the help of the Bicycle Advocacy Group identify businesses which are strategically located along the planned bike route. Provide free or discounted bike racks in exchange for advertisement on wayfinding signage and mapping. Encourage cycle events to stop at these businesses during rides and events. Prepare a marketing portfolio to present to local businesses stating the benefits for a business promoting bicyclist clients. Look in the business community to find a volunteer to be the Bicycle Friendly Business Bike Coordinator.



## **E**nforcement - ensuring safe roads for all users

- Communicate with law enforcement with the rules of the road for cycling (bicycles are allowed to ride in the lane and on the road, and 'ride on the right, walk on the left')
  - » Review and enforce the 4 feet 'Cushion of Safety' law when passing a cyclist
  - » Review 'The Rights and Responsibilities of All Road Users- bicyclists and motor vehicles'
  - » Review penalties and the process for issuing warnings and fines for not obeying the rules of road- for both cyclists and for motorists. (i.e. motor vehicle driving or parking in a bike lane; cyclist not stopping at stop intersection, etc.)
  - » Discuss bicycle use on sidewalks with local officials and law enforcement, establish a clear law for the city.
- Partner with the City Police, PennDOT and the American League of Bicycles for training assistance - emphasize cycling awareness, the law as it pertains to bicycle on the road, and rules of cycling; educate and enforce:
  - » Strong laws
  - » Targeted enforcement

## **E**valuation and Planning - planning for bicycling as a safe and viable transportation option.

- Start a Bicycle and Pedestrian Advisory Committee (BPAC) to help the City with:
  - » Consensus building related to programs, policy and projects
  - » Act as a review panel on bicycle related proposals
  - » Perform a watchdog role on policy, projects, maintenance and operations
  - » Coordinate with related state agencies, park-rec and outdoor associations, and business groups
  - » The BPAC must have 'political teeth' to be effective; such as an active role on council, participate in the planning approval process, etc.
- Collect data in order to evaluate the effectiveness of programs and improvements annually.
  - » Count the number of users on certain bike routes, bike lanes, and trails throughout the year.
  - » Conduct program evaluations with participants to track the effectiveness of programs, policy and projects.





## *P-2: Develop programs aimed at increasing pedestrian activity.*

- Encourage the school district to continue its walking programs: Safe Routes to School programs, Walking School Bus and Walk to School Days.
- Create a Walking Friendly Map similar to the Bicycle Friendly map already completed. The map should include a legend where pedestrians can estimate walking times to popular destinations.
- Develop partnerships with other organizations to:
  - » Local historic organizations to hold organized historical or educational walking tours in conjunction with local events
  - » American Heart Association to support business based walking programs, fund raiser competitions and local walking events ([http://www.pedbikeinfo.org/programs/promote\\_strategies.cfm](http://www.pedbikeinfo.org/programs/promote_strategies.cfm))
  - » Veteran organizations to organize and promote a PA HERO WALK (<http://paherowalk.com/PHW.php>)
  - » Senior organizations to organize and promote A Walk to End Alzheimer's (<https://act.alz.org/>)
- The City should also contact local and regional healthcare providers to determine potential partnerships for future projects. Having accessible, safe and convenient bicycle and pedestrian infrastructure that connects the community can be a key element in meeting the community's health goals. Healthcare is another source of potential funding and should be engaged to provide for mutual programs and infrastructure that benefit community health. Examples of potential partnerships include:
  - » Health Advisory Committee comprised of healthcare practitioners, nutritionists, hospital foundation, local municipal leaders (City and County) and the BPAC.
  - » Recruit a health care liaison to coordinate between bike-ped projects and the local hospital.
  - » The Affordable Care Act requires all charitable 501(c)(3) hospitals to conduct a community health needs assessment (CHNA) and adopt an implementation strategy at least once every three years. This presents an opportunity for the City to work with the hospital to achieve some of the community health goals identified in the CHNA.
  - » A healthcare program promotion called Health in All Policies (HiAP) involves partners from different departments - parks, planning, housing and transportation agencies to address health, social equity, nutrition, housing and transportation.



# Art:

## Utilize public art to promote Franklin as a bicycle- and pedestrian-friendly City.

**ART can make a difference. The use of graphic art and large sculpture in the landscape can alter the public conscious toward bicycling in Franklin, change public perception and can act as a form of promotion that Franklin really is a Bicycle (and pedestrian!) Friendly Community.**

There are many examples of communities who have utilized public art to bring bicycling and walking to the forefront of the public perception of the community. This can be done through murals, sculptures, signing and other art installations. Locations for public art should be highly visible.

### *A-1: Develop partnerships with schools and local artists to prepare a plan for public art in the City*

- Local school districts and artists should be approached to assist the City in determining possible locations and projects for public art.
- Murals could be painted on the sides of buildings along bikeways in the downtown to make them more visible.
- Bike racks could be developed as public art projects to make them more visually appealing and stand out.
- Possible locations could include entrances to trails or bikeways, public parks, City buildings, the Library, schools and/or the sides of buildings adjacent to open lots (corners, vacant lots, parking areas, etc.). Partnerships with property owners is crucial.





*Examples of public art installations to bring awareness and interest to bicycling and trails in towns.*

*Photo credits:*

*Top photo: Building along North Clyborn Avenue in Chicago (John J. Kim / Chicago Tribune, February 23, 2017)*

*Photo to left: Bike racks as art (Richard Hamm, OnlineAthens, December 17, 2009)*

*Bottom photo: Great Allegheny Passage in Rockville, PA (Cathy McCollom / McCollom Development Strategies)*







*Vision without action is merely a dream. Action without vision just passes the time. Vision with action can change the world.*

*- Joel A. Barker*

# Action Plan

## Increase capacity to implement the Plan.

In order to assist the City with implementation and maintain the momentum from the Project Team and this Plan, we recommend establishing an Action Team. The Action Team can be created using the Project Team as a basis and expanding to include additional volunteers as needed (and interest warrants). The Action Team can become the Bicycling and Pedestrian Advisory Committee (BPAC) to begin project implementation. The Action Team should be responsible for:

- Building support and build partnerships- tourism, health care, economic development, recreation, local governments, County, PennDOT, schools, cycle clubs, historic councils, trail associations, environmental groups, etc.
- Seeking out funding sources and/or writing grants
- Partnering with the City, PennDOT and other organizations to implement projects
- Determining methods to measure success
- Monitoring status for Bicycle Friendly Community (BFC) improvements
- Promoting and advertising the City as a BFC and Trail Town
- Coordinating and/or administering the projects with the City and the below agencies:
  - » Public Works Departments- for City, County, and State Routes
  - » Venango County Planning Commission
  - » Venango County Public Transportation (VenanGo Bus))
  - » PennDOT Planning, Bridge and Roadway Departments
  - » Utilities- along project recommended routes (when utilities are relocating or repairing systems, this is the time to get shoulders widened, roads re-striped, bike lanes added, etc.)
- Engage the BPAC to begin volunteer projects such as wayfinding sign design/formatting; route marking and signing along local streets, bicycle and walking programs and community activities identified in this Plan.





## Priority Projects

These projects are listed in order of importance to meet the overall goals of the Franklin Pedestrian and Bicycle Connection Master Plan. The implementation strategy for infrastructure projects should be based on the following priorities:

1. First focus on safe connections between the trail and the town- a convenient connection is an economic generator.
2. Then, concentrate on connections between resources within the City- using Buffalo and Elk Streets. These are low cost improvements along low volume, low speed roadways.
3. Safety Improvements along Liberty Street- including pedestrian crossing and traffic calming improvements at the intersection of Liberty and 12th Street. This work to include a Traffic Study and Alternatives Analysis for Solutions. PennDOT may consider a Roundabout as an alternative design to increase pedestrian safety and provide traffic calming.
4. Finally make the connection between Liberty Street and the Third Ward via 13th Street and the 13th Street Bridge.

The highest priority project is the critical trail-town link; and will require long-term planning, grant writing for federal and state funds and DOT coordination. This project is vital for connecting the Erie to Pittsburgh Trail System to the City bikeway and pedestrian system. All efforts should be employed to make a safe and accessible connection for all users to walk and cycle into the City of Franklin.

The City of Franklin should consider all utility reconstruction, new infrastructure projects and roadway maintenance projects as an opportunity to improve community connections for all modes of transportation. All roadway projects should consider provisions for bike lanes, bicycle 'share the road' markings and sidewalk extensions to provide safe travel choices for its citizens.





Table 3: Implementation Matrix

Priority Ranking	Project Name	Brief Description (refer to Priority Project Descriptions)	Estimated Budget for Construction	Refer to Pages # for Project Details, Maps and Cross- Sections	Permits Required	Potential Funding Sources	Contacts	Next Steps	
1	8TH STREET AND 8TH STREET BRIDGE  (TRAIL TO TOWN ROUTE)	8th Street and 8th Street Bridge- Lane Diet and Add Bike Lanes eastbound/westbound (EB/WB)	\$18,000	“Priority Area #1: Trail to Town Connection” on page 35	PennDOT HOP	TAP, MTF, TIGER, STP, CMAQ	PennDOT- Lyndsie DeVito, 814.678.7140	Meet with District 1-0 PennDOT for Bridge Modifications	Investigate MPH change at Bridge
		Alternative- Shared Use Path Modification at Bridge	\$350K-\$450K						
2	RIVERFRONT PARK  (SEPARATE BIKE TRAIL)	Riverfront Park- Bike Trail	\$29,000	“Priority Area #2: Riverfront Park” on page 47	City Land Dev.	TAP, MTF, DCNR, GTRP, NGBG, ARC	DCNR- Erin Wyle Moyers, 814.226.2329	City Review and Approval Process	Field survey and design
		Mid-Block Crossing	\$30,000		PennDOT HOP	TAP, MTF, DCNR	PennDOT- Lyndsie DeVito, 814.678.7140	Meet with District 1-0 PennDOT	Review alternatives for crossing with DOT
3	9TH STREET  (SAFETY AT PARK EXIT AND SHARED LANE MARKING TO BUFFALO ST)	9th Street- Shared Lane Markings (Sharrows) and Signs	\$5,000	“Priority Area #3: 9th Street” on page 48	City Streets Approval	TAP, MTF, DCNR		City Review and Approval Process	Coordinate with DPW for maintenance and utility repairs- great opportunity to mark lanes and SLM's
4	ELK STREET AND BUFFALO STREET LOOP  (SHARE THE ROAD)	Buffalo Street, 14th Street and Elk Street- Shared Lane Markings (Sharrows) and Signs	\$45,000	“Priority Area #4: Elk Street & Buffalo Street Loop” on page 55	City Streets Approval	TAP, MTF, DCNR	PennDOT- Lyndsie DeVito, DCNR- Erin Wyle Moyers	City Review and Approval Process	Coordinate with DPW for maintenance and utility repairs- great opportunity to mark lanes and SLM's
5	LIBERTY STREET AND 12TH ST INTERSECTION IMPROVEMENTS  (TRAFFIC CALMING AND PEDESTRAIN SAFETY AT CROSSING)	Intersection Improvements for traffic calming and pedestrian safety	\$50,000	“Priority Area #5: Intersection of Liberty Street & 12th Street” on page 65	PennDOT HOP	TAP, MTF, TIGER, STP, CMAQ	PennDOT- Lyndsie DeVito	City Rev/Approval; PennDOT	Field survey and design
		Alternative- Roundabout Design	\$250,000 - \$450,000						Traffic study
6	LIBERTY STREET, 13TH STREET TO 12TH STREET  (LANE REDUCTION AND BIKE LANES)	Liberty Street Lane Reduction- from 4 to 3 lanes with center turning lane. Work includes pavement marking removal and replacement, installation of EB and WB bike lanes with signs and pavement markings plus a traffic study	\$35,000 and \$8,000	“Priority Area #6: Liberty Street (13th Street - 12th Street)” on page 67	PennDOT HOP	TAP, MTF, TIGER, STP, CMAQ	PennDOT	City Rev/Approval; PennDOT	Field survey and design
		Alternative- No reduction of travel lanes; WB Bike Lane, EB SLM	\$18,000						Traffic study
7	LIBERTY STREET, 12TH STREET TO 9TH STREET  (WB CYCLE TRACK)	Liberty Street Cycle Track from 9th to 12th St- 1-way, WB Cycle Track (this is a separated, above the curb bike lane- called a 1-way bike lane or cycle track)	\$400,000	“Priority Area #7: Liberty Street - 12th Street - 9th Street” on page 73	PennDOT HOP	TAP, MTF, TIGER, STP, CMAQ	PennDOT	City Rev/Approval; PennDOT	Field survey and design
		Alternative- Replace parking with WB Buffered Bike Lane	\$18,000						
8	LIBERTY STREET, 9TH TO 8TH STREET  (WB CYCLE TRACK)	Liberty Street Cycle Track from 8th to 9th street (this is a separated, above the curb, WB, bike lane- called a 1-way bike lane or cycle track)	\$100,000	“Priority Area #8: Liberty Street - 9th Street - 8th Street” on page 81	PennDOT HOP and City Streets	TAP, MTF, TIGER, STP, CMAQ	PennDOT	City Rev/Approval; PennDOT	Field survey and design
		Alternative- Replace parking with WB Buffered Bike Lane	\$18,000						

TAP- Transportation Alternatives Program, PennDOT; MTF- Multimodal Transportation Fund, PA Commonwealth Finance Authority; DCNR- C2P2 PA DCNR Trail Grants; GTRP- Greenways, Trails and Recreation Program, PA DCED Grants; NGBG- Northwest Greenways Block Grant Program; ARC- Appalachian Regional Commission; PFB- People for ; Bikes Community Grants; REI- REI Community Outreach Grants; CF- Crowd Funding Options; STP- Surface Transportation Program; Federal; TIGER- Transportation Investment Generating Economic Recovery (TIGER) grant program; US Dept of Transportation; CMAQ- Congestion Mitigation and Air Quality Improvement (CMAQ) Program; US Dept of Transportation



Table 3: Implementation Matrix

Priority Ranking	Project Name	Brief Description (refer to Priority Project Descriptions)	Estimated Budget for Construction	Refer to Pages # for Project Details, Maps and Cross- Sections	Permits Required	Potential Funding Sources	Contacts	Next Steps	
9	13TH STREET AND 13TH STREET BRIDGE (SHARE THE ROAD, SHARED USE PATH AND LANE DIET)	13th Street and 13th Street Bridge- Lane diet and add 'Share the Road Signs,' sidewalk repairs and sidewalk widening	\$140,000	"Priority Area #9: 13th Street - Connection to the Third Ward" on page 85	PennDOT HOP	TAP, MTF, TIGER, STP, CMAQ	PennDOT	Meet with District 1-0 PennDOT for bridge modifications	Investigate MPH change at bridge
		Alternative A- Lane Reduction from 4 lanes to 3 lanes and NB-SB Bike Lanes	\$40,000						Traffic study
		Alternative B- Lane Diet (keep 4 lanes) and SB Bike Lane, SLM NB	\$30,000						Traffic study
10	ELK STREET EXTENSION BETWEEN 3RD STREET AND 8TH STREET (SHARE THE ROAD)	Mark and Sign Elk Street as 'Share the Road'. Since this roadway is posted at 25 MPH and has lower traffic volumes; a shared lane design is recommended	\$25,000	"Priority Area #10: Elk Street Extension (8th St - 3rd St)" on page 93	City Streets Approval	TAP, MTF, DCNR		City Review and Approval Process	Coordinate with DPW for maintenance and utility repairs- great opportunity to mark lanes and SLM's
11	Wayfinding Signs at Samuel Justus Trail		\$10,000		Trail Association and ETPA coordination	DCNR, GTRP, Foundations, NGBG, ARC, CF	GAP Trail and ETPA for Sign Standards	Hire a sign designer and/or experienced trail sign manufacturer	Coordinate destinations, sign ahead and at each turn, design sign layout
12	Misc. Intersection Improvements for Pedestrian & ADA Compliance	26-curb ramp improvements, approximately \$8,600 each	\$223,600		PennDOT HOP and City Streets	TAP, MTF, TIGER, STP	PennDOT	City Rev/Approval; PennDOT	Phase projects based on route priority
13	Misc Wayfinding Signs throughout system		\$30,000		City Streets Approval	DCNR, GTRP, Foundations, NGBG, ARC, CF	GAP Trail and ETPA for Sign Standards	Hire a sign designer and/or experienced trail sign manufacturer	Coordinate destinations, finalize routes for peds and bikes; design sign layout
14	Support Facilities at Bandstand Park	Bike Building/Bike Shelter at Park;	\$80,000-\$120,000		City Streets Approval	DCNR, MTF, PFB, REI, CF, Foundations		Hire a specialty architect familiar with bike and ped needs, trail towns, eco-tourism	
15	Support Facilities at Bandstand Park	Bike Racks and Bike Lockers	\$10,000		City Streets Approval	DCNR, MTF, PFB, REI, CF, Foundations			

TAP- Transportation Alternatives Program, PennDOT; MTF- Multimodal Transportation Fund, PA Commonwealth Finance Authority; DCNR- C2P2 PA DCNR Trail Grants; GTRP- Greenways, Trails and Recreation Program, PA DCED Grants; NGBG- Northwest Greenways Block Grant Program; ARC- Appalachian Regional Commission; PFB- People for Bikes Community Grants; REI- REI Community Outreach Grants; CF- Crowd Funding Options; STP- Surface Transportation Program; Federal; TIGER- Transportation Investment Generating Economic Recovery (TIGER) grant program; US Dept of Transportation; CMAQ- Congestion Mitigation and Air Quality Improvement (CMAQ) Program; US Dept of Transportation





## Design Budget

Pre-construction items, or 'soft costs', must be considered for each design and engineering project. Below are percentages that can be used for grant applications and budget planning purposes; these are a percent of the total construction estimate.

Construction Contingencies	10%-15%
Maintenance and Protection of Traffic	3%-5%
Permits	2%-4%; this cost will increase if impacts are at a water body or an environmentally sensitive area.
Field Survey, Design and Engineering	15%-20%
Construction Inspection	12%-15%

## Maintenance and Costs

Estimates for life expectancy and costs of pavement markings are provided below: The estimates are for markings in the roadway. If the marking is out of the vehicle wheel path, it will have a longer life expectancy. Obviously, a higher volume road with greater trips and turning movements over the markings will lead to a lower life expectancy.

- Hot Thermo- Sharrow (Shared Lane Marking) – 5 - 6 years \$250 EA
- Hot Thermo- Bike Lane Graphic with Arrow - 6 - 7 years \$350 EA
- Hot Thermo- Ped Crossing, using 'ladder bars' – 5 - 6 years \$12 LF
- Painted-Green Lane markings through intersections – 1 - 3 years \$3- \$5 SY
- Painted- Linear 4" wide yellow or white lane markers – 1 - 3 years \$0.50 LF
- Bike Lane, Shared Lane Marking Signs and Ped Crossing Signs (post mounted Type B sign) \$200-\$250 EA

Note: Epoxy can be used to mark concrete; life expectancy is 3 years in a roadway and 6-7 years in a bike lane.



## Next Steps

1. Commit to implementing the first project(s); include a schedule with project milestone dates.
2. Create the Action Team/BPAC to administer projects, act as a liaison with PennDOT and City DPW and begin coordination with the local healthcare institutions.
3. Agree on how the facility will be maintained. The PA Code requires that local authorities be responsible for installing, maintaining and operating all pavement markings for bicycles, such as the 'sharrows' and bicycle lane symbols.
4. Search out funding sources and begin a funding campaign for infrastructure projects, as well as for community activities, cycling and walking programs and education sessions.
5. Hire an experienced consulting firm to complete construction documents for bidding, permitting and constructing the Phase-1 Project. The details are what will make the project safe, accessible and convenient. Conduct the following services:
  - » Field Survey of chosen alignment
  - » Apply for permits through the Bicycle Occupancy Process (BOP) and submit documents to PennDOT for review of work within the State Route ROW; submit design plans for review.
  - » PNDI Report
  - » NPDES Permit Application
  - » Construction Documents for Bidding
6. Bid and build the first project(s) to connect trail to town and the internal routes through the City.
7. Plan and organize community events featuring the trail to encourage bicycling, walking, jogging and getting outside and socializing with neighbors and visitors. These events can serve as fundraisers and may include community bicycling rides, fundraisers, walk-a-thons, 5K races, exercise programs and educational sessions.
8. Plan for additional enhancements to fund and build the bike building and enhancements to bus stops and transfer stations; coordinate with the County, transit authority and landowners.





# Appendices

**Meeting and Event Summaries**

**Funding Source Descriptions**

**Chart - Funding Source List**

**Bicycle - Fact Sheet AARP**

**Bike Programs from Franklin**

**Oil Region Cycling Classic, June 2017 Tour**

**Trail Town (Progress Fund) Franklin Report**

**Pedestrian Curb Ramp Existing Conditions**

**Accident Facts for 12th and Liberty**



## City of Franklin Bicycle-Pedestrian Connection Master Plan

### Kick Off Meeting Summary

July 20, 2016 at 9 am, City Hall

**Attendance:** list of attendees is attached

- Bob Genter, RLA, ASLA . Mackin Engineering Company
- Amy Wiles, AICP . Mackin Engineering Company

### Introduction / Project Overview

- Tracy Jamieson, City Manager, provided an overview of the City's Bronze status as a Bicycle Friendly Community and desire to become a Silver. A bike and pedestrian plan is needed.
- Bob Genter provided a brief overview of Mackin and history in trail planning and development. Projects include work on the Great Allegheny Passage (one of the first feasibility studies to design of cycle track in Homestead); trail town master plans (local example = Titusville); and connectivity / multi-modal community studies (recent examples = Indiana).
  - o Bob discussed a revised hierarchy to better plan and design community streets- Pedestrians, Bicycles, Cars and Trucks- in that order. This will help focus our perspective and better design streets for people. Our community streets should be accessible, safe, and comfortable places for people to walk, bicycle and drive along. Traffic calming is a key component to bicycle safety and design.
  - o Schedule: The project will take approximately eight months to complete.
  - o Meetings: 5 committee meetings and 2 public meetings will be held.

### Stakeholders

- Mackin will be interviewing stakeholders as part of the project. Stakeholders should represent many facets of the community, including but not limited to the following types of organizations:
  - o Hospital / healthcare
  - o County and Local Transit
  - o Businesses and Services
  - o Outdoor recreation groups (i.e. cycling, walking, running, paddling)
  - o Community / service groups (i.e. Rotary)
  - o Disabled and senior citizen groups and organizations
- ***Committee members should come to the next meeting with names and contact information for potential stakeholders.***

### Committee Input

- Great trails in/near Franklin; public restrooms are lacking (portable toilets located in the Riverfront Park).

- There are many certified trail town businesses in town but could market more / better.
- Elementary School has 140 walkers; approximately 2-20 kids bike to school daily. Currently using the sidewalk.
- Public transit is available, but the only real stop is at the Buffalo Street Gazebo . otherwise, all others are unmarked. Need to talk to Transit as a stakeholder.
- The Northwest Commission will be starting a ride-share feasibility study in September for the region.
- PA Bicycle Routes is identifying major hubs along the marked state bike routes.
- Franklin has a Complete Streets Policy but not sure everyone understands what this means. Traffic speeds are too high for some streets (the posted speed limit is higher than what is considered to be safe; traffic calming is needed).
- There is no wayfinding signage in town to direct visitors to/from parks, trails, businesses, etc. There is a kiosk with a map at the Samuel Justus Trailhead (Salt Box) but no directional signing.
- Franklin has two grocery stores (one in the downtown and one in the Third Ward); these should be hubs.
- Need to utilize Main Street (Liberty) to bring people into town from the trail; Elk Street could be used as an exit back to the trail.
- 12<sup>th</sup>/Liberty is a dangerous intersection for cyclists and pedestrians and cars. Lane markings are unclear at this bend in the road (US322 and Route8).
- Heavy truck traffic is an issue on the 322 bridge; would require a study to do a lane diet (PennDOT). Public Works stated that 322 is often used as a detour when there are issues on I-80.
- Need to obtain maintenance and resurfacing plans from PennDOT and City for upcoming years . can incorporate recommendations.
  - o **Lyndsie DeVito** to gather DOT road maintenance list for Franklin Bike-Ped areas impacted.
- The streetscape on Liberty is aging and the brick strip could be reviewed for use as part of bikeway. Research and identify options (such as angled parking, bike lane, etc.) . could be costly because of existing infrastructure (utilities, street trees, etc.).
- Events:
  - o Green County, Ohio holds a 4-day bike event on their various trails . could do something similar here.
  - o Oil City is hosting a Bike and Brew.
  - o Apple Fest (October) is too busy to incorporate this project . would get lost.
 Other events include Taste of Franklin (Aug 6/7) and Rockin Riverfest (Aug 20).

### **“Tour de Franklin”**

- Mackin and the City / Committee will host a public bicycle ride as part of the Rockin Riverfest on Saturday, August 20<sup>th</sup>. . the ride will begin at the Salt Box on the Samuel Justus Trail and ride through the downtown and back. It is expected to take approximately 30 minutes and will end with a discussion about what issues/opportunities people saw during the ride.
- ***The City will coordinate with the Rotary to set a start time and will promote this event.***



### Next Meeting

- The next committee meeting will be held August 4 at 9 am.
- The purpose will be to finalize the Tour de Franklin+details (including identifying the route) and beginning to discuss concepts and priorities for the project.
- ***Tracy will confirm a date/time/location and send out to the Committee.***

### Project Contact Information

Project Consultant: Mackin Engineering Company

Project Manager: Bob Genter, RLA, ASLA, Director - Land Development Services  
[rwg@mackinengineering.com](mailto:rwg@mackinengineering.com) / 412.788.0472

**City of Franklin Bicycle-Pedestrian Connection Master Plan**  
**Conceptual Plan Review Meeting Summary**  
August 4, 2016 at 9 am, City Hall

**Attendance:** list of attendees is attached

- Robert Genter, RLA, ASLA . Mackin Engineering Company
- Mathew Lokay, RLA, ASLA . Mackin Engineering Company

**Meeting Overview**

- Bob Genter provided a brief overview of the past meeting and began with a discussion of the conceptual bicycle route plan to connect the trail into town.
- Bob discussed again the revised hierarchy of design for our street corridors- pedestrians and bicycles first, then for cars and trucks. This hierarchy promotes safety and the creation of comfortable street corridors for people, where cars actual travel the posted speed.
- Bike Routes-A and Routes-B were discussed. It was concluded that each route is to be used as part of the final bike-ped connection solution.
- Destination points were determined to be reached by the routes suggested below- including the extension of the route into the Third Ward at Grant Street and the extension of Elk Street to 3<sup>rd</sup> Street at the boat launch and playground.
- Bike Route-A extends from the Riverfront Park trail into 9<sup>th</sup> Street as a Bike Lane and crosses Liberty at the traffic signal toward Buffalo St. Bike Route-A traverses Buffalo as a Shared Lane Marking (SLM) to 14<sup>th</sup> Street, crosses Liberty at the traffic signal, and follows Elk Street as a SLM route all the way back to 9<sup>th</sup> and Riverfront Park.
  - o The route was edited by the Doug Baker (Mayor), removing 15<sup>th</sup> St and replacing with it with 14<sup>th</sup> St as the connection between Buffalo and Elk.
- Bike Route-B extends the riverfront park trail toward Washington Crossing (RT8) Bridge, climbs the slope and enters the signalized intersection of Washington Crossing and Elk Street.
  - o Route-B extends to Liberty as a separated bike lane into town and then follows the Safe Routes project along the north side of Liberty as a above the curb one-way (WB) bike path.
  - o Route-B recommends a lane reduction on Liberty between 13<sup>th</sup> St and S.Park St, from 4 lanes to 3 lanes with a central turn lane. This will provide space for an EB and WB bike lane, and keep all of the parking. Liberty is restricted to 2 and 3 lanes on each end of town. A traffic study is recommended to confirm the lane reduction is possible and the level of service can be maintained.

- Lyndsie DeVito (PennDOT District 1-0) discussed the potential for a roundabout (RA) at 12<sup>th</sup> and Liberty as a traffic calming and safety device. She will follow up the District for the RA size, and impact.
  - Route-B is an economical solution to provide for a separated bicycle route into town. The exit route would use the Elk Street as a SLM.
- Tracy Jamieson commented about the recent accident at Howard and 13<sup>th</sup> Street in the Third Ward; and emphasized that the project should include the 13<sup>th</sup> Street Corridor from Grant Street in the Third Ward to Liberty St. in Franklin.
- Sue Smith (NW Commission) noted that a study will be conducted for a park-n-ride facility to be built somewhere in the region; this can be coordinated with the transit service stops/facilities for the bike-ped master plan.
- Erin Wiley Moyers (PA DCNR) suggested the route extend toward 3<sup>rd</sup> Street past the memorial park/fortress and include the river launch; and Jeff Brunton (OARS) suggested the playground, just past 3<sup>rd</sup> St., would be a destination to connect with a SLM, shared path or bike lane.
  - The conceptual solution may include a shared use path (8' wide between the curb and including the current sidewalk space) on the north side, and repair the existing sidewalk along the south side for pedestrians only.
  - Creating a separated shared use path, above the curb would provide for people who don't feel safe bicycling in the road with a way to get to the river launch and the playground.
- Bob Genter discussed the use of a bike box, ahead of the stop bar, to provide for left turn movements by bicycles. This will be further detailed in future meetings.
- Maintenance was discussed. Usually the trail association or municipality maintains the bikeway and trail system. Typically, PennDOT doesn't maintain bikeways. They will rebuild or remark them if disturbed during a PennDOT road project.

### **“Tour de Franklin”**

- The City/Project Committee/Mackin will host a public bicycle ride as part of the Rockin Riverfest on Saturday, August 20<sup>th</sup>. The ride will begin at the Salt Box on the Samuel Justus Trail at 11:30 am and ride through the downtown along Buffalo Street, 14<sup>th</sup> Street and Elk Street, back to the Riverfront Park. It will include discussion for the improvements along the route and end with a discussion about what issues/opportunities people saw during the ride. Bob Genter and Ron Snyder will participate in the Tour.

### **Next Meeting**

- The next committee meeting will be held October. Tracy will confirm a date/time/location and send out to the Committee.



### Task Items:

- Stakeholder Phone Interviews by Mackin
  - Mackin will send a list of potential Stakeholders and general topics for discussion for final approval to Sheila Boughner for confirmation prior to conducting phone interviews.
- Confirm ROW-along Liberty, at the Washington Crossing/Elk intersection, along 9<sup>th</sup> Street and along 8<sup>th</sup> Street.
- Detail the Bicycle Routes A and Routes B, and provide budgets
- **District 1-0** to provide feedback for 12<sup>th</sup> St/Liberty potential RA size and impacts
- Can **PennDOT** and the **City of Franklin provide maintenance and resurfacing plans** from for upcoming years on state and local roads? This data can be incorporate into the project report.

### Project Contact Information

Project Consultant: Mackin Engineering Company

Project Manager: Robert W. Genter, RLA, ASLA, Director - Land Development Services  
[rwg@mackinengineering.com](mailto:rwg@mackinengineering.com) / 412.788.0472

**City of Franklin Bicycle-Pedestrian Connection Master Plan**  
**Tour De Franklin Ride Meeting Minutes**  
August 20, 2016 at 11:30 am, Samuel Justus Trailhead

**“Tour de Franklin”**

- The tour was a public bicycle ride advertised as part of the ~~Rockin Riverfest~~ The tour began at the Salt Box on the Samuel Justus Trail at 11:30 am and cycled through the downtown of the City of Franklin.
- The route crossed the 8<sup>th</sup> Street Bridge and entered the Riverfront Park. The tour made several stops along the way. The route continued on 9<sup>th</sup> Street to Buffalo Street; and looped from Buffalo to 14<sup>th</sup> Street and Elk Street.
- The tour stopped at 12<sup>th</sup> Street to review the Liberty improvements and then continued, via Elk Street, to Riverfront Park.



**Meeting Overview**

- The Tour included approximately 25 persons including Doug Baker, Ron Snyder and Sheila Boughner. Bob Genter of Mackin described the purpose of the tour and the route, as well as what is a bike lane, a shared lane marking/~~sharrow~~ and bike route.
- Bob also explained that participants are to travel single file and follow the ~~rules~~ of the road The tour was led by Ron and Bob was the ~~sweep~~
- The tour made several stops along the route to regroup and for Bob to explain what is being considered for each bikeway improvement.



The following items were expressed by Bob and by participants:

1. Sign the Samuel Justus Trail with ahead and turn signs for Services, Shops, Food, Destinations. Similar to the highway road signs on interstates that alert travelers for upcoming attractions and destinations. Continue the signs into Franklin as bike route wayfinding signs.
2. Repair sidewalks including surfaces and curb ramps; and clear brush at the trail and bridge- as part of the master plan priority projects.



3. Move the 25 mph sign from the west side to the east side of the 8<sup>th</sup> Street Bridge (Rt322) to slow traffic entering the city; and provide a safer travel for cyclists using the road bridge.
4. Create a separate a path for bikes and for walkers through Riverfront Park to avoid conflicts.
5. Install a bike lane from Riverfront Park along 9th Street extending to Buffalo Street. This will provide a designated lane for people to feel more comfortable to enter the city streets.
6. Find new parking spaces to accommodate those ~~taken~~for bike lanes.
7. Mark Buffalo and Elk with Shared Lane Markings

8. Start a bike repair program where kids can repair their bikes and people can drop off old bikes; potential location would be a current auto repair, bike shop, or trade school.
9. Begin a ~~Bike~~ Rodeo Program in Parks- children can bring their bikes to ride and police can lead a discussion about bicycle safety and bike awareness for automobile drivers. The mayor said a bike rodeo was done in the past.
10. Emphasize bicycle safety and bicycle riding rules- begin at schools continue with Drivers Training Courses for senior and for new drivers. The school district has a walk safety program.
11. Create an ~~above~~ the curb one-way bicycle lane along Liberty to guide cyclists into to central part of town- 12<sup>th</sup> Street. Liberty has a 100' wide ROW. The one-way bike lane would extend from Washington Crossing to 12<sup>th</sup> Street. Bicycles would use Elk to get back to the park and the trail.
12. Band Stand Park is a possible location for a ~~bike~~ building. The shelter would house bike infrastructure including wayfinding maps-signs, public toilets, bike lockers and bike racks and bike repair station. Visitors could lock up their bike and travel supplies and tour Franklin on foot or continue by bike.



### **Project Contact Information**

Project Consultant: Mackin Engineering Company

Project Manager: Robert W. Genter, RLA, ASLA, Director - Land Development Services

[rwg@mackinengineering.com](mailto:rwg@mackinengineering.com) / 412.788.0472



## **Franklin Bicycle-Pedestrian Connection Master Plan**

### **Draft Plan Review- Meeting Summary**

**October 12, 2016 at 9 am, 191 Howard Street**

**Attendance:** list of attendees is attached

- Robert Genter, RLA, ASLA – Mackin Engineering Company
- Mathew Lokay, RLA, ASLA – Mackin Engineering Company

#### **Meeting Overview**

- Bob Genter provided a brief overview of the past meetings, bicycle tour, interviews, maps, and alternative plans completed to date, and Applefest project banner display.
- The Project schedule was presented and reviewed. The next meeting will be a public open house to display the result of the Draft Plan. The date is November 9, 2016.
- The results of the past meetings, current documents, project mapping and bicycle master plans are available on the project FTP site. These items can be downloaded to the municipal web site to future promote the project.
- The meeting focused on a review of the draft plan improvements for bicycle infrastructure, as described during a 69 slide 'power point' presentation.
  - o The bicycle plans begin with a connection from trail to town, includes destination points within the city, parks, business district, and provides options for bicycle travel.
  - o The plan also considers:
    - transit stops
    - wayfinding from the trail town
    - bicycle support facilities, such as a bike building with shelter, racks, lockers, vendor booth/docent station, kiosk/wayfinding
- The route extends from the city center into the Third Ward at Grant Street and from 8<sup>th</sup> street south to 3<sup>rd</sup> Street (playground and river launch site).
- A general project budget was discussed.
- Priority projects were recommended - refer to attached for results.
- The next meeting will be the first public meeting. It is to be advertised by Franklin and will be held November 9, 2016.
  - o This meeting will be an open house event to present and display the Draft Master Plan.
  - o The City of Franklin to finalize the location and time; and advise committee and Mackin.

#### **Project Contact**

Project Consultant: Mackin Engineering Company

Project Manager: Robert W. Genter, RLA, ASLA, Director - Land Development Services

[rwg@mackinengineering.com](mailto:rwg@mackinengineering.com) / 412.788.0472

## City of Franklin Bike-Ped Master Plan

### November 18, 2016 Phone Conference with Tracy Jamieson, City Manager, and Sheila Boughner, Community Development

#### RESULTS OF CONFERENCE

1. Police Chief is concerned that bicyclists and walkers will walk-out into traffic at the 8<sup>th</sup> St. Crossing.
  - a. The report should highlight that a PennDOT review and approval is required for all SR installations; including the flashing warning device crossing of 8<sup>th</sup> St.
  - b. This master plan must be followed by preliminary and final engineering plans, construction documents for bidding, and permit approval prior to construction phase.
2. Riverview Park plan is approved by committee
3. 9<sup>th</sup> and Riverview Driveway-
  - a. increase sight distance for Elk St. motorists and driveway users by marking and posting 'No Parking to Corner'; discussion included speed humps/tables and bumps-RWG said-rebuild roadway for table-costly, drainage issues, snow plowing issues, and bumps are noisy for residents)- ahead transverse markings may work along with the crossing markings.
  - b. mark crossing and sign crossing; also, add ahead signs to Elk St
  - c. next step, if new marked crossing doesn't work, add three-way stop condition (driveway, 9<sup>th</sup>, Elk)
4. Liberty- to 8<sup>th</sup> St. to 3<sup>rd</sup> St.-
  - a. Shared lane markings ('sharrows')
  - b. Or- bike lane into town
  - c. Or- cycle track above curb into town
  - d. And- cycle track above curb out of town (concern is that visitors will ride in and out of town using same one-way cycle track)
5. Typical Above the Curb Cycle Track and Walking Route -
  - a. Current plan is for a 5' (one-way) bike lane (made of asphalt), and a 5' sidewalk (made of concrete) with 2' buffer space between; total width is between 12'. The route can 'split' to avoid major trees and utility impacts.
6. Bus Stop locations-
  - a. Add Shop-n-Save along Buffalo (currently, there is a stop here) Plan will recommend better signing and the development of a pad, shelter, lighting, bike racks, and wayfinding signs/rider information.
  - b. Add Bus Stop in 3<sup>rd</sup> Ward at 'Save A Lot' grocery/'Family Dollar'; corner of Grant St. and Pine St.
7. Add a Route- 8<sup>th</sup> St. to Liberty-
  - a. Rebuild wider sidewalks between Liberty and Riverfront Park- both north and south side of 8<sup>th</sup>.) this will provide ADA access for walkers and bikes that want to use the walk.
  - b. Add SLM's to 8<sup>th</sup> and a Bike Box at 8<sup>th</sup> at Elk left turn

- c. Add SLM's to 8<sup>th</sup> from Elk to Liberty (*8<sup>th</sup>, Buffalo to Liberty is 1 way toward River-westbound*)
  - d. A 'contra-flow lane on 8<sup>th</sup>, between Liberty and Buffalo, could be installed along the north side to provide a signalized crossing of Liberty and a connection to Buffalo
  - e. 7<sup>th</sup> is not signalized as a crossing at Liberty- not recommended for route crossing
- 8. Roundabout to be part of recommendations for plan- discuss benefits for function and location
  - 9. Create a Benefits and Impacts Statement for the report
  - 10. Create a Next Steps statement for report- once the master plan is approved, the next step is traffic study, a field survey of phase-1 & 2 projects, utility clearance, environmental clearance, ROW plan, preliminary and final engineering plans, HOP approvals, construction documents for bidding- then the construction phase.

#### ACTION PLAN

- 11. Mackin to make above edits to Draft Report and Draft Plans
- 12. Mackin to make overlay for wayfinding signs- between trail and town
- 13. Tracy to send meeting dates to Mackin
  - a. Preliminary Public meeting- Preliminary plan review (probably in January)
  - b. Final Public meeting- a tentative meeting date for Final plan review (probably in February or March)



## MEMORANDUM OF MEETING

On Tuesday, December 20, 2016 a meeting was held at the offices of PennDOT Engineering District 1-0 for the purposes of discussing a potential transportation project within the City of Franklin, Venango County. Participating in the discussion were representatives from the City of Franklin, Venango County Planning Department, Northwest RPO and PennDOT 1-0. An attendance sheet listing the individuals and their contact information is attached.

### PennDOT Connects

An overview of the new PennDOT Connects policy was provided. This initiative is to engage municipal officials, regional planning partners and local stakeholders early in the planning of transportation projects and continue this collaboration throughout design development.

The goal of this early planning and continual collaboration is to have the project better serve the needs of the community and identify components and features to best meet those needs. Items such as but not limited to: safety, transit, pedestrian access, bicycle accommodations, storm water management, utilities, emergency services, right-of-way, public opinion, community events, future maintenance, etc. are to be considered.

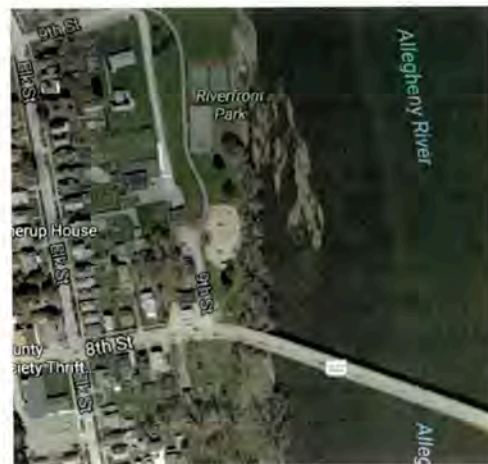
### Franklin Bicycle-Pedestrian Study

The city manager provided a brief overview of the Franklin Bicycle-Pedestrian Study being conducted by Mackin Engineering Company.

The study was begun in the summer of 2016 and is evaluating safer routes to convey bicyclists from the Allegheny River Trail and Samuel Justus Trail along the east side of the Allegheny River into and through the City of Franklin.

The study has not yet concluded and several routes under consideration as well as shared access facilities.

The city inquired about speed studies along SR 0322 on the segment crossing the Allegheny River (8<sup>th</sup> Street Bridge) and on the segment crossing French Creek (13<sup>th</sup> Street Bridge) to see if posted speed limits could be reduced to 25mph. Some coordination would need to be done with Cranberry Twp. as the 8<sup>th</sup> Street Bridge is mostly within that municipality. The city must make a written request to PennDOT for these speed studies in order to be conducted.



## MEMORANDUM OF MEETING

The city inquired about a new mid-block crossing on 8<sup>th</sup> Street just after crossing the 8<sup>th</sup> Street Bridge to allow bicyclists and pedestrians to enter Riverfront Park more readily. PennDOT generally discourages these types of crossings and indicated an analysis would need to be performed to determine if such a crossing would be safe and not impede travel efficiency.

The study suggested a shared facility be created across the 8<sup>th</sup> Street Bridge. The bridge section will need to be evaluated to see if adequate width is available to create this corridor.

The study also suggested a road diet be done on that section of Liberty Street between 12<sup>th</sup> Street and 13<sup>th</sup> Street. The roadway section is currently four lanes of traffic with a parking lane on each side. A road diet would provide two travel lanes, a center, left-turn lane, two parking lanes and a bicycle corridor between the parking and travel lanes on each side. A traffic analysis would be required to determine if this road diet and new section are feasible.

Recommendations made by the study could be implemented in a phased approach.

Cost estimates for various study recommendations would be developed and included in a final report.

### 12<sup>th</sup> Street / Liberty Street Intersection

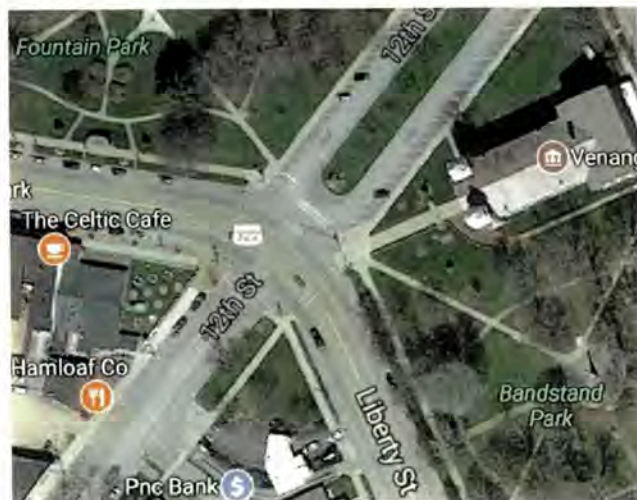
The bicycle-pedestrian study did identify deficiencies with the intersection: expansive pavement, undesirable geometry, etc.

The intersection is on a National Highway System (NHS) route and is an incident management route for a section of I-80.

An accident cluster does exist at the intersection.

Attendees were in agreement that improvements to the intersection would serve to enhance safety for both motorists and pedestrians and facilitate traffic flow and efficiency.

The city also saw opportunities to add amenities to the intersection as it offers a gateway into its downtown business district and is adjacent to a city park and the county courthouse site.





## MEMORANDUM OF MEETING

The city is open to the concept of a roundabout at this intersection. If the project were to be programmed and included on the Northwest RPO Transportation Improvement Plan (TIP) a roundabout would be one of several alternatives to be evaluated. These alternatives would then be presented to the public prior to announcement of a preferred alternative.

The intersection is a site that receives much pedestrian traffic as it is a central hub between businesses (i.e., banks, restaurants, retail stores), the county courthouse, a city park and on-street parking facilities. One leg of 12<sup>th</sup> Street approaching the intersection is also reserved for a Farmer's Market that occurs on Wednesday's and Saturday's throughout the year.

### Project Programming and Funding

The first step in initiating a new project request would be to complete and submit a Linking planning and NEPA (LPN) Level-1 form. This form could be completed by the Venango County Dept. of Planning (in conjunction with the city) and submitted at any time.

Following the initial screening and review of the LPN-Level 1 form, the next step would be to complete a LPN-Level 2 form with more project detail. Accompanying this form would also be a Project Initiation Form (PIF) that documents some of the considerations advocated by PennDOT Connects. A sample copy of a PIF was provide to the city.

Once the LPN, Level 1 &2 forms along with the PIF are completed and submitted a presentation would be made before the Northwest RPO Transportation Advisory Committee (TAC). The next Northwest RPO TAC meeting is to occur in March 2017.

If voted on favorably by the Northwest RPO-TAC then the Long Range Transportation Plan (LRTP) would need to be amended to add the project. Following that amendment the project could then be added to the next Northwest RPO Transportation Improvement Plan (TIP) update. This next TIP update is to occur in the fall of 2017.

As the project is on a NHS route and is known location of an accident cluster it would meet criteria for several types of federal funding (e.g., NHPP, HSIP). Determining which funding is most applicable and available would be determined during a TIP update.

The Franklin Bicycle-Pedestrian Study is likely to recommend various projects that can be implemented individually over a period of time. The city would have opportunities to apply for funding from various sources (not necessarily thorough the TIP) to advance these projects.

### Future Maintenance Responsibilities

Federal and state funding for transportation projects will cover the construction cost of most features included in the project. Future maintenance though is a responsibility that is distributed to various parties.



## **MEMORANDUM OF MEETING**







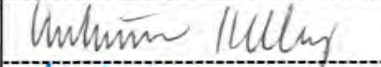


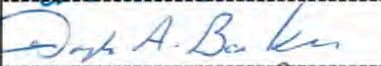
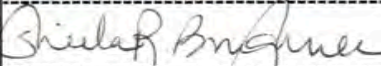
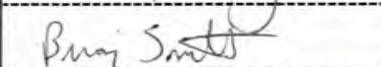

PennDOT generally maintains the pavement surface from face-of-curb to face-of-curb within incorporated cities and boroughs. This includes ice/snow removal, pavement patching and resurfacing.

Cities and boroughs are expected to maintain those facilities beyond the face-of-curb including: sidewalks, highway lighting, landscaping, traffic signals, etc. Standard maintenance agreements have been prepared and must be executed with the responsible party prior to project advertisement for these features to be included.

Initial Planning Meeting /  
Project Identification

12th Street @ Liberty Street  
City of Franklin - Venango Co.

December 20, 2016

PRINTED NAME	SIGNATURE	REPRESENTING	TELEPHONE NO.	E-MAIL ADDRESS
Michael L. McMullen		PennDOT 1-0 (Design)	814-678-7077	<a href="mailto:mmcmullen@pa.gov">mmcmullen@pa.gov</a>
Brian S. Yedinak		PennDOT 1-0 (ADE-Design)	814-678-7130	<a href="mailto:byedinak@pa.gov">byedinak@pa.gov</a>
Brian A. McNulty		PennDOT 1-0 (Planning)	814-678-7035	<a href="mailto:brmcnulty@pa.gov">brmcnulty@pa.gov</a>
Tracy Jamieson		City of Franklin (Manager)	814-437-1485	<a href="mailto:tjamieson@franklinpa.gov">tjamieson@franklinpa.gov</a>
Susan Smith		Northwest RPO	814-677-4800	<a href="mailto:sues@northwestpa.org">sues@northwestpa.org</a>
Jason Ruggiero		Venango Co. Planning	814-432-9682	<a href="mailto:jruggiero@co.venango.pa.us">jruggiero@co.venango.pa.us</a>
<del>Thomas J. McClelland</del>		<del>PennDOT 1-0 (Design)</del>	<del>814-678-7081</del>	<del><a href="mailto:rmcclellan@pa.gov">rmcclellan@pa.gov</a></del>
Autumn Kelley		PennDOT 1-0 (Environmental)	814-678-7393	<a href="mailto:aukelley@pa.gov">aukelley@pa.gov</a>
Emily Donaldson		Venango Co. Planning	814-432-9681	<a href="mailto:edonaldson@co.venango.pa.us">edonaldson@co.venango.pa.us</a>
Kim Mourer		Venango Co. Planning	814-432-9689	<a href="mailto:kmourer@co.venango.pa.us">kmourer@co.venango.pa.us</a>
Douglas A. Baker		City of Franklin (Mayor)	814-437-1670	<a href="mailto:dbaker@franklinpa.gov">dbaker@franklinpa.gov</a>
Sheila Boughner		City of Franklin (CDD)	814-437-7841	<a href="mailto:sboughner@franklinpa.gov">sboughner@franklinpa.gov</a>
Brian Smith		PennDOT 1-0 (Traffic)	814-678-7081	<a href="mailto:briansmit@pa.gov">briansmit@pa.gov</a>
<del>SOE SMITH</del>		<del>NWC</del>		

**Franklin Bicycle-Pedestrian Connection Master Plan  
Public Meeting #1  
Draft Plan Presentation  
January 19, 2017 at 5 pm, City Hall**

**Welcome:**

- Tracy Jamieson, City Manager – City of Franklin
- Robert Genter, RLA, ASLA – Mackin Engineering Company
- Mathew Lokay, RLA, ASLA – Mackin Engineering Company

**Tasks Completed to Date:**

1. July 20, 2016- Committee Kick-Off
2. August 4, 2016- Committee Conceptual Route Review- established the project area, points of interested/destinations, and conceptual bicycle routes A&B
3. Sent- Video Clip and Bike Lanes Mean Business Article
4. August 20, 2016- Tour-de-Franklin Bicycle Ride; Tour of City and Route Discussion, as part of the 'Rockin Riverfest'
5. Aug and Sept- Route Development and Budget Creation
6. September- Stakeholder Interviews
7. PennDOT supplied electronic web link for project coordination (MPMS IQ System)
8. City supplied list of past Bike-Ped Programs
9. Mapping and graphics provided to City to post
10. Trail Town Phone Conference (special interest group)
11. October 9, 2016- Tour during Applefest and Project Banner Displayed
12. October 12, 2015 Draft Plan presented to Steering Committee
13. November 18, 2015 Draft Edit Phone Conference
14. December 20, 2015, PennDOT meeting to review proposed routes



**The Plan:**

- The project area includes the connection from the Allegheny River at the 8th Street Bridge, into the City along Liberty Street; and utilizes Buffalo and Elk Streets. The alignment also extends to the Third Ward at Grant Street.
- The bicycle-pedestrian connection plan begins with mapping destination points- places of interest, services, restaurants, shops, parks, etc. 'place where people want to go'
- A priority is to create safe and convenient connections for people to walk and bicycle between trail and town
- The plan proposes connections for people to walk and people to ride a bicycle within the city; between homes, schools, stores, parks, the trail, bus stops, etc.
- The plan considers options for travel- including separated cycle track, bike lanes, shared lane markings- 'sharrow'; and pedestrian sidewalk improvements for crossings, intersection redesign, new sidewalks, repairs to existing routes.
- The plan also considers:
  - o Programs for bicycle safety and programs to engage bicycle users
  - o Programs for pedestrian improvement and ped safety
  - o Wayfinding- from trail to town
  - o Route coordination with PennDOT
  - o Bicycle support facilities- bike building with shelter,





- racks, lockers, vendor booth/docent station, kiosk/wayfinding
- Integration and upgrades of bus stops- wayfinding and bicycle infrastructure, and parking for easier transfer

### Next Steps:

- Review and Edit Plans per public and steering committee comments
- Finalize alignments, project budgets and project priorities
- Finalize programs to support, educate, and engage bicycle and pedestrian users
- Begin outreach to funding sources- federal, state, local, private foundations, corporate/business
- Public Meeting #2- Final Plan Presentation
- Report Completion

### Project Contact

Mackin Engineering Company

Project Manager: Robert W. Genter, RLA, ASLA, Director - Land Development Services

[rwg@mackinengineering.com](mailto:rwg@mackinengineering.com) / 41 2.788.0472



## Funding Source Descriptions for Bicycle and Pedestrian Infrastructure Projects

- The Pennsylvania State Transportation Advisory Committee (TAC) May 2016. The TAC was established in 1970 by Act 120 of the State Legislature, which also created the Pennsylvania Department of Transportation (PennDOT)
- Liquid Fuels Fund – PennDOT Office of Chief Counsel has issued an opinion that bike lanes and markings are allowable Liquid Fuels Fund expenditures.
- PennDOT Agility Program – The Agility Program may be useful in addressing maintenance costs of bicycle and pedestrian facilities with municipalities.
- Funding Partnerships – Partnerships with organizations such as schools, universities, economic development organizations, health care systems, major employers, and community groups have been successfully used to complete bicycle and pedestrian projects.
- Pennsylvania Infrastructure Bank – The Pennsylvania Infrastructure Bank may be a source of loan funds for bicycle and pedestrian infrastructure.

Act 89 Multimodal Transportation Fund – This new source of grant funding has provided two rounds of grants to date.

### **☐ Multimodal Transportation Fund (MTF) of DCED**

**Purpose:** MTF provides grants to encourage economic development and ensure that a safe and reliable system of transportation is available to the residents of this commonwealth. The program is intended to provide financial assistance to improve transportation assets that enhance communities, pedestrian safety, and transit revitalization. The program will be jointly administered by DCED and PennDOT, under the direction of the Commonwealth Financing Authority. Grants are available for projects with a total cost of \$100,000 or more; grants shall not exceed \$3,000,000 for any project. A local match of at least 30% of the on-federal share of the project costs is required.

**Eligibility:** Local government or County; Councils of Governments; Businesses & non-profits; Economic Development Organization; Public Transportation Agency (including but not limited to an airport authority, public airport, port authority, or similar public entity); Ports: rail and freight

**Deadline:** The application deadline for the program is July 31, 2016 for consideration at the November 2016 CFA board meeting. The completed hard copies of the application must be postmarked no later than July 31, 2016

Website: <http://community.newpa.com/programs/multimodal-transportation-fund/>

#### **Greenways, Trails, and Recreation Program (GTRP)**

**Purpose:** Act 13 of 2012 established the Marcellus Legacy Fund and allocates funds to the Commonwealth Financing Authority for planning, acquisition, development, rehabilitation and repair of greenways, recreational trails, open space, parks and beautification projects. Grants shall not exceed \$250,000 for any project; a 15% match of the total project cost is required.

**Eligibility:** Municipalities, Counties, Councils of Governments, Authorized Organizations, Institution of Higher Education, Watershed Organization, For-Profit Businesses

**Deadline:** The application deadline for the program is June 30, 2016 for consideration at the September 2016 CFA board meeting.

**Contact:** PA Department of Community and Economic Development  
Office of Innovation and Investment – CFA Programs Division  
Greenways, Trails and Recreation Program 400 North Street, 4th Floor  
Commonwealth Keystone Building Harrisburg, PA 17120-0225  
Telephone: (717) 787-6245 Fax (717) 772-3581 E-mail: [ra-dcedsitedvpt@pa.gov](mailto:ra-dcedsitedvpt@pa.gov)

Website: <http://community.newpa.com/programs/greenways-trails-and-recreation-program-gtrp/>

#### **Bicycle information for Planning, Design and Promotions can be found at below sources:**

- » Advocacy Advance (passionate advocates for bicycling and walking): [www.advocacyadvance.org](http://www.advocacyadvance.org)
- » Southwest Planning Commission (SPC) Bicycle Maps
- » League of American Bicyclists - Bicycle Friendly Community Program, [www.bikeleague.org](http://www.bikeleague.org)
- » Bikes Belong.org and People for Bikes.org



peopleforbikes.org

- » Bike Commuting 101, Bike Pittsburgh web page at [www.bikepgh.org](http://www.bikepgh.org);

Bike Pittsburgh - 188, 43rd Street, Suite 1, Pittsburgh, PA 15201

**Federal Programs – Many federal funding programs may be used to fund bicycle and pedestrian facilities. FHWA has produced a matrix to describe program eligibility.**



# Pedestrian and Bicycle Funding Opportunities

## U.S. Department of Transportation Transit, Highway, and Safety Funds

Revised August 12, 2016

This table indicates potential eligibility for pedestrian and bicycle projects under U.S. Department of Transportation surface transportation funding programs. Additional restrictions may apply. See notes and basic program requirements below, and see program guidance for detailed requirements. Project sponsors should fully integrate nonmotorized accommodation into surface transportation projects. Section 1404 of the Fixing America's Surface Transportation (FAST) Act modified 23 U.S.C. 109 to require federally-funded projects on the National Highway System to consider access for other modes of transportation, and provides greater design flexibility to do so.

Key: \$ = Funds may be used for this activity (restrictions may apply). \$* = See program-specific notes for restrictions. ~\$ = Eligible, but not competitive unless part of a larger project.															
Activity or Project Type	Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds														
	<a href="#">TIGER</a>	<a href="#">TIFIA</a>	<a href="#">FTA</a>	<a href="#">ATI</a>	<a href="#">CMAQ</a>	<a href="#">HSIP</a>	<a href="#">NHPP</a>	<a href="#">STBG</a>	<a href="#">TA</a>	<a href="#">RTP</a>	<a href="#">SRTS</a>	<a href="#">PLAN</a>	<a href="#">NHTSA 402</a>	<a href="#">NHTSA 405</a>	<a href="#">FLTP</a>
Access enhancements to public transportation (includes benches, bus pads)	\$	\$	\$	\$	\$		\$	\$	\$						\$
ADA/504 Self Evaluation / Transition Plan								\$	\$	\$		\$			\$
Bicycle plans			\$					\$	\$		\$	\$			\$
Bicycle helmets (project or training related)								\$	\$SRTS		\$		\$*		
Bicycle helmets (safety promotion)								\$	\$SRTS		\$				
Bicycle lanes on road	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Bicycle parking	~\$	~\$	\$	\$	\$		\$	\$	\$	\$	\$				\$
Bike racks on transit	\$	\$	\$	\$	\$			\$	\$						\$
Bicycle share (capital and equipment; not operations)	\$	\$	\$	\$	\$		\$	\$	\$						\$
Bicycle storage or service centers at transit hubs	~\$	~\$	\$	\$	\$			\$	\$						\$
Bridges / overcrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Bus shelters and benches	\$	\$	\$	\$	\$		\$	\$	\$						\$
Coordinator positions (State or local)					\$ 1 per State			\$	\$SRTS		\$				
Crosswalks (new or retrofit)	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Curb cuts and ramps	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Counting equipment			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Data collection and monitoring for pedestrians and/or bicyclists			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Historic preservation (pedestrian and bicycle and transit facilities)	\$	\$	\$	\$				\$	\$						\$
Landscaping, streetscaping (pedestrian and/or bicycle route; transit access); related amenities (benches, water fountains); generally as part of a larger project	~\$	~\$	\$	\$			\$	\$	\$						\$
Lighting (pedestrian and bicyclist scale associated with pedestrian/bicyclist project)	\$	\$	\$	\$		\$	\$	\$	\$	\$	\$				\$
Maps (for pedestrians and/or bicyclists)			\$	\$	\$			\$	\$		\$	\$*			
Paved shoulders for pedestrian and/or bicyclist use	\$	\$			\$*	\$	\$	\$	\$		\$				\$



Key: \$ = Funds may be used for this activity (restrictions may apply). \$* = See program-specific notes for restrictions. ~\$ = Eligible, but not competitive unless part of a larger project.															
Activity or Project Type	Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds														
	<a href="#">TIGER</a>	<a href="#">TIFIA</a>	<a href="#">FTA</a>	<a href="#">ATI</a>	<a href="#">CMAQ</a>	<a href="#">HSIP</a>	<a href="#">NHPP</a>	<a href="#">STBG</a>	<a href="#">TA</a>	<a href="#">RTP</a>	<a href="#">SRTS</a>	<a href="#">PLAN</a>	<a href="#">NHTSA 402</a>	<a href="#">NHTSA 405</a>	<a href="#">FLTTP</a>
Pedestrian plans			\$					\$	\$		\$	\$			\$
Recreational trails	~\$	~\$						\$	\$	\$					\$
Road Diets (pedestrian and bicycle portions)	\$	\$				\$	\$	\$	\$						\$
Road Safety Assessment for pedestrians and bicyclists						\$		\$	\$			\$			\$
Safety education and awareness activities and programs to inform pedestrians, bicyclists, and motorists on ped/bike safety								\$SRTS	\$SRTS		\$	\$*	\$*	\$*	
Safety education positions								\$SRTS	\$SRTS		\$		\$*		
Safety enforcement (including police patrols)								\$SRTS	\$SRTS		\$		\$*	\$*	
Safety program technical assessment (for peds/bicyclists)								\$SRTS	\$SRTS		\$	\$*	\$		
Separated bicycle lanes	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Shared use paths / transportation trails	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Sidewalks (new or retrofit)	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$				\$
Signs / signals / signal improvements	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signed pedestrian or bicycle routes	\$	\$	\$	\$	\$		\$	\$	\$		\$				\$
Spot improvement programs	\$	\$	\$			\$	\$	\$	\$	\$	\$				\$
Stormwater impacts related to pedestrian and bicycle projects	\$	\$	\$	\$		\$	\$	\$	\$	\$	\$				\$
Traffic calming	\$	\$	\$			\$	\$	\$	\$		\$				\$
Trail bridges	\$	\$			\$*	\$	\$	\$	\$	\$	\$				\$
Trail construction and maintenance equipment								\$RTP	\$RTP	\$					
Trail/highway intersections	\$	\$			\$*	\$	\$	\$	\$	\$	\$				\$
Trailside and trailhead facilities (includes restrooms and water, but not general park amenities; see guidance)	~\$*	~\$*						\$*	\$*	\$*					\$
Training					\$	\$		\$	\$	\$	\$	\$*	\$*		
Training for law enforcement on ped/bicyclist safety laws								\$SRTS	\$SRTS		\$			\$*	
Tunnels / undercrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$

#### Abbreviations

ADA/504: Americans with Disabilities Act of 1990 / Section 504 of the Rehabilitation Act of 1973

[TIGER](#): Transportation Investment Generating Economic Recovery Discretionary Grant program

[TIFIA](#): Transportation Infrastructure Finance and Innovation Act (loans)

[FTA](#): Federal Transit Administration Capital Funds

[ATI](#): Associated Transit Improvement (1% set-aside of FTA)

[CMAQ](#): Congestion Mitigation and Air Quality Improvement Program

[HSIP](#): Highway Safety Improvement Program

[NHPP](#): National Highway Performance Program

[STBG](#): Surface Transportation Block Grant Program

[TA](#): Transportation Alternatives Set-Aside (formerly Transportation Alternatives Program)

[RTP](#): Recreational Trails Program

[SRTS](#): Safe Routes to School Program / Activities

[PLAN](#): Statewide Planning and Research (SPR) or Metropolitan Planning funds

[NHTSA 402](#): State and Community Highway Safety Grant Program

[NHTSA 405](#): National Priority Safety Programs (Nonmotorized safety)

[FLTTP](#): Federal Lands and Tribal Transportation Programs (Federal Lands Access Program, Federal Lands Transportation Program, Tribal Transportation Program, Nationally Significant Federal Lands and Tribal Projects)

#### Program-specific notes

Federal-aid funding programs have specific requirements that projects must meet, and eligibility must be determined on a case-by-case basis. For example:



- TIGER: Subject to annual appropriations.
- TIFIA: Program offers assistance only in the form of secured loans, loan guarantees, or standby lines of credit, but can be combined with other grant sources, subject to total Federal assistance limitations.
- FTA/ATI: Project funded with FTA transit funds must provide access to transit. See [Bikes and Transit](#) and the FTA Final Policy Statement on the [Eligibility of Pedestrian and Bicycle Improvements under Federal Transit Law](#).
  - Bicycle infrastructure plans and projects funded with FTA funds must be within a 3 mile radius of a transit stop or station, or if further than 3 miles, must be within the distance that people could be expected to safely and conveniently bike to use the particular stop or station.
  - Pedestrian infrastructure plans and projects funded with FTA funds must be within a ½ mile radius of a transit stop or station, or if further than ½ mile, must be within the distance that people could be expected to safely and conveniently walk to use the particular stop or station.
  - FTA funds cannot be used to purchase bicycles for bike share systems.
  - FTA encourages grantees to use FHWA funds as a primary source for public right-of-way projects.
- CMAQ projects must demonstrate emissions reduction and benefit air quality. See the CMAQ guidance at [www.fhwa.dot.gov/environment/air\\_quality/cmaq/](http://www.fhwa.dot.gov/environment/air_quality/cmaq/) for a list of projects that may be eligible for CMAQ funds. Several activities may be eligible for CMAQ funds as part of a bicycle and pedestrian-related project, but not as a highway project. CMAQ funds may be used for shared use paths, but may not be used for trails that are primarily for recreational use.
- HSIP projects must be consistent with a State's [Strategic Highway Safety Plan](#) and either (1) correct or improve a hazardous road location or feature, or (2) address a highway safety problem.
- NHPP projects must benefit National Highway System (NHS) corridors.
- STBG and TA Set-Aside: Activities marked "\$SRTS" means eligible only as an SRTS project benefiting schools for kindergarten through 8<sup>th</sup> grade. Bicycle transportation nonconstruction projects related to safe bicycle use are eligible under STBG, but not under TA (23 U.S.C. 217(a)).
- RTP must benefit recreational trails, but for any recreational trail use. RTP projects are eligible under TA and STBG, but States may require a transportation purpose.
- SRTS: FY 2012 was the last year for SRTS funds, but SRTS funds are available until expended.
- Planning funds must be used for planning purposes, for example:
  - Maps: System maps and GIS;
  - Safety education and awareness: for transportation safety planning;
  - Safety program technical assessment: for transportation safety planning;
  - Training: bicycle and pedestrian system planning training.
- Federal Lands and Tribal Transportation Programs (FLTTP) projects must provide access to or within Federal or tribal lands:
  - Federal Lands Access Program (FLAP): Open to State and local entities for projects that provide access to or within Federal or tribal lands.
  - Federal Lands Transportation Program: For Federal agencies for projects that provide access within Federal lands.
  - Tribal Transportation Program: available for federally-recognized tribal governments for projects within tribal boundaries and public roads that access tribal lands.
- NHTSA 402 project activity must be included in the State's Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>
- NHTSA 405 funds are subject to State eligibility, application, and award. Project activity must be included in the State's Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>

#### **Cross-cutting notes**

- FHWA Bicycle and Pedestrian Guidance: [http://www.fhwa.dot.gov/environment/bicycle\\_pedestrian/](http://www.fhwa.dot.gov/environment/bicycle_pedestrian/)
- **Applicability of 23 U.S.C. 217(i) for Bicycle Projects:** 23 U.S.C. 217(i) requires that bicycle facilities "be principally for transportation, rather than recreation, purposes". However, sections 133(b)(6) and 133(h) list "recreational trails projects" as eligible activities under STBG. Therefore, the requirement in 23 U.S.C. 217(i) does not apply to recreational trails projects (including for bicycle use) using STBG funds. Section 217(i) continues to apply to bicycle facilities other than trail-related projects, and section 217(i) continues to apply to bicycle facilities using other Federal-aid Highway Program funds (NHPP, HSIP, CMAQ). The transportation requirement under section 217(i) is applicable only to bicycle projects; it does not apply to any other trail use or transportation mode.
- There may be occasional DOT or agency incentive grants for specific research or technical assistance purposes.
- Aspects of many DOT initiatives may be eligible as individual projects. For example, activities above may benefit Ladders of Opportunity; safe, comfortable, interconnected networks; environmental justice; equity; etc.



# Bicycling | A LIVABILITY FACT SHEET

Half of all trips taken in the United States are three miles or less, yet most Americans drive — even to the closest destinations. Only 3 percent of commuting trips in the U.S. are by bicycle, compared to up to 60 percent in The Netherlands.

Still, it's not unreasonable to believe we can improve our numbers. The popularity of bicycling has been on the rise. The number of bike trips doubled between 1990 and 2009, and many communities and the federal government are embracing the bicycle as a transportation solution for a healthy and viable future.<sup>1</sup>

Surveys show that 60 percent of Americans would ride a bicycle if they felt safe doing so, and eight out of 10 agree that bicycling is a healthy, positive activity.

Although issues related to bicycling continue to be debated, experience shows that bicycle-friendly features increase safety for all road users, including motor vehicles.<sup>2</sup>

In 2010, New York City removed a traffic lane and painted a two-way bicycle path with a three-foot parking lane buffer

alongside Brooklyn's Prospect Park. Weekday bicycling traffic tripled, speeding by all vehicles dropped from 74 to 20 percent, crashes for all road users dropped 16 percent and injuries went down 21 percent, all without a change in corridor travel time.<sup>3</sup> Throughout New York City, deaths and serious crashes are down 40 percent where there are bike lanes.<sup>4</sup>

Bicycling also provides economic benefits: Two-thirds of merchants surveyed on San Francisco's Valencia Street say that bike lanes have improved business. In North Carolina's Outer Banks, bicycle tourism has already generated \$60 million in annual economic activity on its \$6.7 million bicycle infrastructure investment. In 2009, people using bicycles spent \$261 million on goods and services in Minnesota, supporting more than 5,000 jobs and generating \$35 million in taxes.<sup>5</sup>

Building bike infrastructure creates an average of 11.4 jobs for every \$1 million spent. Road-only projects create 7.8 jobs per \$1 million.<sup>6</sup> The average American household spends more than \$8,000 a year on its cars; the cost to maintain a bicycle is about \$300 a year.<sup>7</sup>

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**Building bike infrastructure creates an average of 11.4 jobs for every \$1 million spent.  
Road-only projects create 7.8 jobs per \$1 million.**



This path in New Smyrna Beach, Fla., is part of a Volusia County plan to link schools, parks and businesses through interconnected paths. Fifteen miles were completed by 2012 with overwhelming public support. (Image: bikeflorida.net.)

# Myth-Busting!

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## ■ “Bicyclists don’t follow rules.”

While there are bicyclists who do break the law, a large Federal Highway Administration study found that motorists failed to yield the right of way in 43 percent of crashes; bicyclists were at fault 36 percent of the time.<sup>8</sup> Since the 1982 passage of Idaho’s “stop as yield” law, which allows cyclists to treat stop signs as yield signs, there has been “no discernible increase in injuries or fatalities,” according to the Idaho Department of Transportation.<sup>9</sup>

## ■ “Bicyclists don’t pay their fair share.”

All road users — cars, trucks, bicycles, pedestrians, buses, light rail — are subsidized to some extent by society at large. Funding for U.S. roadways comes partly from vehicle taxes, fuel taxes and tolls, which together account for up to 60 percent of direct costs. General taxes and fees pay the remaining 40 percent. The federal gas tax of 18.4 cents per gallon has not been raised since 1992.

Cars, buses and trucks impose much higher maintenance and capital costs on roads than bicycles do, and they benefit from subsidies that are not directly paid by motorists.<sup>10</sup> In 2009, the Seattle Department of Transportation paid only 4 percent of its road expenses with the gas tax while non-motor vehicle funds paid for the rest.<sup>11</sup>

Motor vehicle crash injuries cost society \$99 billion in 2010 due to medical expenses and lost productivity.<sup>12</sup> Pedestrians and bicyclists bear a larger share of costs than they impose.<sup>13</sup>

## ■ “Bicycling is for fit middle-class white guys.”

Six in 10 young bicycle owners are women, eight out of 10 American women have a positive view of bicycling and two out of three believe their community would

be a better place to live if biking were safer and more comfortable. Between 2001 and 2009, the fastest growth in bicycle use in the U.S., from 16 to 23 percent, occurred among self-identified Hispanics, African-Americans and Asian-Americans, 86 percent of whom have a positive view of bicyclists.<sup>14</sup>

## ■ “Bicycling is too dangerous.”

Bicycling does tend to have higher fatality rates per mile than motorized travel, but a typical motorist drives five to 10 times more miles than a typical cyclist.

Bicycling risk can be significantly reduced through improved infrastructure and a greater number of bicycles on the road.<sup>15</sup> Bicycling also imposes a minimal risk to other road users and provides significant health benefits that can offset crash risks.<sup>16</sup>

There were no bicycling fatalities in bicycle-friendly Portland, Ore., in 2013 even though bicycling accounts for at least six percent of all trips. By comparison, 21 people were killed inside motor vehicles that year.<sup>17</sup>

## ■ “Bicyclists slow down cars and create congestion.”

Average traffic speeds in Manhattan increased nearly seven percent since the installation of bicycle lanes south of 60th Street in 2008.<sup>18</sup> Bicycles take up way less road space than motor vehicles and cyclists tend to avoid congested roads that don’t have bike lanes.<sup>19</sup>

## ■ “Bicycle lanes hurt business.”

After the installation of protected bike lanes on Manhattan’s 8th and 9th avenues in 2007, retail sales increased 49 percent in those areas compared to 3 percent in the rest of the borough.<sup>19</sup>

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# How To Get It Right



Bicycle parking promotes bicycle riding. Bike racks can be placed on or off the street. Fun Fact: One vehicle parking space can accommodate 12 bicycles.



Grade-separated, protected bike lanes (like this raised cycle track in Missoula, Montana) provide separate, defined spaces for riders and drivers.

**To encourage bicycling and bicycle-friendly streets and communities, try the following:**

## ■ Embrace a public process and build support

Develop an education and awareness campaign prior to implementation, and reach out broadly to community members, elected officials and municipal leaders. Government officials may need to see public support before acting. Toward that end, advocates can share this fact sheet, talk to neighbors, build community support and then meet with decision makers, the media, experts and others to discuss the benefits of bicycling. Agency staff can engage residents by hosting workshops to build acceptance and understanding.

## ■ Start with a pilot project

Do a simple, low-cost project, such as striping a bike lane in an area with high bicycling potential and an existing right of way. This can help residents become comfortable with bicycling and enable municipal staff to document what works and what doesn't. Promote the pilot as a road improvement project rather than only as a bicycle project.

## ■ Provide adequate bicycle parking

Bicycle racks encourage bicycling. Well-placed racks provide a secure place for parking bikes while shopping, working or playing. Racks can be located inside buildings or bolted into sidewalks or even the street. A single parking space can hold up to 12 bicycles on staple racks (they look like an inverted "U" shape) mounted in a row.

## ■ Create routes and wayfinding signs

Develop a system of routes cyclists can follow to get around town safely. Install highly-visible wayfinding signs that indicate distances, destinations and street names and install signs at all important crossings.

## ■ Establish a bike share

More than 500 communities worldwide, including at least 50 in the U.S., have a short-term bicycle rental or bike share program.<sup>20</sup> (New York City and Washington, D.C., feature popular bike share networks.) People can join a share program for the day or a full year by paying a nominal fee. To participate, a rider checks out a bicycle from a computerized kiosk and then returns the bike at a share program rack near his or her destination.

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# Success Stories

## ■ Palo Alto, California: Bicycle Boulevards

Bicycle boulevards are low-volume, low-speed streets that have been optimized for bicycle travel. Palo Alto has an extensive network of paths, bike lanes and boulevards, including connections to schools throughout town.

Data from the 2010 Census showed 7.1 percent of residents commuted to work by bicycle, an increase from 5.6 percent in 2000. The city continues to provide facilities, services and programs to promote travel by bicycle.

## ■ Indianapolis, Indiana: Cultural Trail

The eight-mile, \$63 million walk-bike trail was completed in May 2013, having been financed by both public and private dollars.

The trail winds through the downtown of this auto-oriented city (home of the Indy 500), connecting a half-dozen emerging cultural districts, a 1.5 mile section of the historic Indianapolis Canal and to White River State Park, a former industrial wasteland that's now filled with museums, lawns and attractions. By April 2014 the trail had added more than \$864 million to the local economy.

## ■ Memphis, Tennessee: Broad Avenue

The Broad Avenue Arts District initiative revitalized a struggling commercial and residential area. The project's popularity exploded when the focus was expanded to include bicycles.

"The lanes slowed down traffic and people started noticing the businesses more," says Pat Brown, co-owner of T Clifton Art Gallery. "Our revenues have grown on average 30 percent per year. Yes, that's for an art-related business

in a tough economy." The district has seen more than 15 new businesses and nearly 30 property renovations. Restaurants report a growth in business due to bicyclists.

## WHY IT WORKS

**Protected Bike Lanes** provide a barrier between motor vehicles and cyclists. (This barrier can be installed and permanent, or as simple as a row of parked cars, planters or plastic posts.) They're good for ...

**Business:** A Portland study found that bike riders will go out of their way to use a street that has good bicycling infrastructure. That's more business exposure.

**Safety:** Drivers don't have to worry about unexpected maneuvers by cyclists and pedestrians don't need to dodge bike riders on sidewalks.

**Lawfulness:** Protected bike lanes in Chicago resulted in a more than 150 percent increase in the number of bike riders obeying traffic lights.

**Everyone:** Bicycles don't pollute, they cause less wear and tear on roadways than cars do, they help people stay healthy!

Source: Adapted from the Tranitized.com infographic "Why Build Protected Bike Lanes?"

## RESOURCES

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2. **Bicycling Means Business: The Economic Benefits of Bicycle Infrastructure.** Flusche, D. League of American Bicyclists, Advocacy Advance (2009, 2012), [http://www.advocacyadvance.org/site\\_images/content/Final\\_Econ\\_Update\(small\).pdf](http://www.advocacyadvance.org/site_images/content/Final_Econ_Update(small).pdf)
3. **Urban Bikeway Design Guide.** National Association of City Transportation Officials (NACTO Sept. 2012), <http://nacto.org/cities-for-cycling/design-guide/>
4. **Pedestrian and Bicycle Information Center.** [www.pedbikeinfo.org/](http://www.pedbikeinfo.org/)
5. **Alliance for Biking & Walking.** Resource Library, <http://www.bikewalkalliance.org/resources/resource-library/>
6. **Safe Routes to School National Partnership.** <http://www.saferoutespartnership.org/resourcecenter/National-Learning-Network-Library-of-Resources>
7. **Association of Pedestrian and Bicycle Professionals.** <http://www.apbp.org/?page=Library>
8. **Fundamentals of Bicycle Boulevard Planning & Design.** Portland State University, Initiative for Bicycle and Pedestrian Innovation (2009)
9. **Bicycling and Walking in the United States: 2014 Benchmarking Report.** <http://www.bikewalkalliance.org/resources/benchmarking>
10. **Protected Bike Lanes Mean Business.** <http://www.bikewalkalliance.org/resources/reports/protected-bike-lanes-mean-business>
11. **National Complete Streets Coalition.** <http://www.smartgrowthamerica.org/complete-streets>



### AARP LIVABLE COMMUNITIES

**Mail:** 601 E Street NW, Washington, DC 20049  
**Email:** [livable@aarp.org](mailto:livable@aarp.org)  
**Online:** [aarp.org/livable](http://aarp.org/livable)



### WALKABLE AND LIVABLE COMMUNITIES INSTITUTE

**Mail:** 2023 E. Sims Way #121, Port Townsend, WA 98368  
**Email:** [community@walklive.org](mailto:community@walklive.org)  
**Online:** [walklive.org](http://walklive.org)

## Bicycle programs

### **Walk/Bike to School Day**

Since 2004, the City of Franklin has sponsored a Walk/Bike to School Day in both the spring and the fall in conjunction with National Walk to School Day. Partners on the event have included the Allegheny Valley Trails Association, the Franklin Area School District, the Venango County Sheriff's Department and the Franklin YMCA. For the event, children who live in the City are encouraged to walk or bike along three routes to school – at Central Elementary School or St. Patrick School. The children are accompanied by adults and law enforcement personnel assists at major intersections. The event has been a success since its inception. At the most recent event last spring, more than 100 children participated, 61 of them on bicycles. (See the enclosed newspaper account of the event.)

### **Bicycle Rodeo**

A bicycle rodeo focusing on bicycle skills and safety was held earlier this summer for children ages 5 to 10. The event was sponsored by the City of Franklin, PennDOT, the Venango County Sheriff's department and the Franklin Area Chamber of Commerce. Helmets were provided to some children who did not have them and two bicycles were given away at the event, thanks to the Venango Chapter of Alliance of Bikers Aimed Toward Education. The event – which drew about 15 participants – was regarded by organizers as a success. It was their first such effort, and they expect more children to participate next year and in coming years as public awareness of the event grows.

Similar bicycle rodeos were previously sponsored by the Franklin Knights of Columbus in cooperation with PennDOT, the Sheriff's department and Safe Kids Venango.

### **Classes**

At one time, several local residents were certified League of American Bicyclists instructors. They taught some classes at some of the local schools but had trouble attracting adults to the Smart Cycling classes.

### **Fire department programs**

In the summers of 2010 and 2011 the Franklin Fire Department teamed up with Children's Hospital of Pittsburgh and Pizza Joe's on a program designed to encourage children to wear helmets when bicycling and to provide helmets to those needing them. If a fire fighter noticed a child not wearing a helmet while bicycling, they stopped the child and discussed the importance of wearing a helmet and gave them a letter to pass on to their parents. The letter provided information about how bike helmets help prevent head injuries and offered a free bicycle helmet for the child. As part of the program, the fire fighters handed out vouchers for small pizzas to those children they encountered who were wearing helmets while bicycling.

### **Police department programs**

The police department has a bicycle patrol and does bicycle registrations for residents.

oilregioncycling.com



**June 16-18, 2017**

♦ **Franklin Area Chamber of Commerce** ♦ **1327 Liberty St.** ♦ **Franklin, PA 16323**

An exciting sponsorship opportunity is on the way this summer with the inaugural Oil Region Cycling Classic, set for June 16 to 18, in Venango County.

The weekend event will draw bicyclists from near and far to ride the region's beautiful trail system along the Allegheny River (2017 PA River of the Year) and Oil Creek and take in some of the area's numerous attractions.

Day 1 will feature the 58-mile Franklin to Emlenton, Day 2 will offer participants a 48-mile ride from Franklin to Titusville, and the third day will offer a choice of pedaling from Franklin to Van or pedaling and paddling from Franklin to Fisherman's Cove.

We are promoting the Oil Region Cycling Classic in Western Pennsylvania and Eastern Ohio as well as portions of West Virginia and Western New York. An added draw will be the Franklin Blues & Barbecue Festival, which will be held in Franklin's downtown parks that same weekend.

To help ensure the success of this weekend, we are soliciting contributions at the following levels: a Grand Sponsor at \$2,000, as well as Gold (\$1,000), Silver (\$500), and Bronze (\$250) and Patron (less than \$200) sponsorships. See attached for additional information regarding sponsorships.

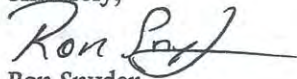
All proceeds from the event will benefit the Allegheny Valley Trail Association's trail maintenance fund.

If your organization would like to participate in this year's event as a sponsor, please contact the Franklin Area Chamber of Commerce at [administrator@franklinareachamber.org](mailto:administrator@franklinareachamber.org) or (814) 432-5823.

Additional information about the event is available via e-mail at [rsnyder@hotmail.com](mailto:rsnyder@hotmail.com) or by calling (814) 758-9434.

Thank you for your time and consideration.

Sincerely,

  
Ron Snyder

Chair of ORCC Organizing Committee

Bill Weller

  
Allegheny Valley Trail Association President





## Creating Opportunities Along the Nation's Trails

425 West Pittsburgh Street | Greensburg, PA 15601 | (724) 216-9160 | [www.trailtowns.org](http://www.trailtowns.org) | [www.progressfund.org](http://www.progressfund.org)

### Franklin Trailhead Design Charrette Wednesday May 18, 2016

A top priority from the Franklin Trail Town Assessments was the need to make improvements to the trailhead so that there is better connectivity between the trail and the business district. To address the site, stakeholders assessed the site to review current conditions and opportunities.

The following report breaks down the trailhead into various segments.

#### Northern End

Current conditions and issues:

- Commercial Building
  - Former bike shop does not anticipate to open in 2016
  - Currently no water or sewage at building
  - Large mural on façade
- Parking
  - Often used for overflow parking
  - Gravel lot
- Road/Pathway to Bridge
  - Shared road way and the only direct connection to downtown Franklin
- General
  - Maintenance of site is questionable or unknown
  - Official ownership of land and parcels needs addressed
  - Concern of safety/speed under the bridge

Recommended solutions:

- Commercial building
  - Determine owner's interest in renting or selling for new commercial ventures
  - Survey interest for potential market vendors
  - Mural is in good shape, may need repainted or updated in coming years
  - Best potential uses for building:
    - Bike shop
    - Café/Refreshments/Snacks/Ice Cream
    - Combination business
- Parking
  - Note: Parking is most likely attached to commercial property owner
  - Dependent on owner approval, parking improvements for designated overflow parking could be considered with either retaining gravel or improving the site with pavement, preferably permeable paving. Line markers and/or curb stops should be included to better organize and establish parking spots.
  - An ADA spot must be included at this site if improvements are made.

- Retain green space between parking area and trail. Additional landscaping with native plants should be consider to help define separation.
- Road/Pathway to Bridge
  - Add sharrow markings
  - If improving parking area, consider repaving as well
  - Additional native planting and landscaping would help create visual connection between trail and bridge
  - Install bollards on trail near bridge and pathway to help calm cyclist speed at this intersection

Further research needed:

- Determine land and property ownership beyond the trail with private owners and PENN-DOT
- Determine costs and feasibility to install utilities to site/building

## **Bridge to Saltbox**

Current conditions and issues:

- General concern of safety/speed under the bridge
- No barriers to prevent vehicular traffic onto trail
- Newly installed art piece
- Former landscaping/garden box surrounds a general trail sign

Recommended solutions:

- Install bollards between the Saltbox and the bridge
- Remove garden box and sign
- See Public Art category

Further research needed:

- Not applicable

## **Saltbox and Information Hub**

Current conditions and issues:

- The Saltbox Building
  - Porch is faded/chipping
  - Signs on porch are not very visible, do not match
  - Limited information is available on the outside/when closed
  - ADA access ramp is not well connected to parking
  - Largely volunteer operated with sporadic hours
- Parking area towards bridge
  - To access, cars primarily drive on trail
  - It seems that locals only know that is available
  - Generally, an underutilized space
- Bulletin Board and Restroom Area
  - Has mostly up to date info with maps
  - No takeaway information
  - No event or current activities information
  - Roof is missing shingles
  - Bathrooms are not ADA accessible on grass
  - There are two signs located next to bulletin board including an interpretive panel

Recommended solutions:

- The Saltbox Building

- Repaint/re-stain the deck
- Replace current two signs with one new projecting sign above doorway to better identify site
- Add information at door/windows on regarding hours/where to get information when not open
- Install rackcard holders on building or at bulletin board
- Pave connection between ADA ramp and current parking area
- Volunteer recruitment:
  - Consider community groups, social clubs, students to volunteer to open and provide information during high traffic times to establish regular days or hours
- Parking area towards bridge
  - Upon further review and discussion, this site is largely underutilized and may benefit the trail and trail user as greenspace or gathering area
  - A parklet could be created by relocating the bathrooms and additional amenities could be moved or added to this site:
    - Bike racks
    - Bike FixIt station
    - Kiosk with a town and trail maps, additional interpretive information
    - Benches and Picnic benches
    - Trash and Recycle bin
  - A larger picnic pavilion could fit in this space if positioned properly
  - Additional landscaping would benefit the site
  - Consider using decorative pavers to feature the site
- Bulletin Board and Restroom Area
  - Bulletin board needs repaired – new shingles, stained, etc
  - Calendar of events, how to volunteer, find more information, etc should be added to bring people into town and get involved
  - This information should be easy to read in an organized manner
  - Brochure holders should be installed at this site onto the bulletin board for takeaway maps and information

Further research needed:

- Review ADA requirements for the restrooms and ramp access to the Saltbox

## **Parking Area**

Current conditions and issues:

- The parking area is large and open - often full on busy days
- There are no defined spaces
- There is no definition between parking spots and trail
- Vehicles more than often drive onto the trail creating a safety issue
- Lighting appears to be available and sufficient

Recommended solutions:

- It is highly recommended to install a physical barrier between the trail and parking
  - This could be small boulders, planters, curb stops, fencing, etc
  - This will create a safer and more welcoming area
- Parking spots should be marked so that there is no confusion on how or where to park whether it is a slow or busy day and deter vehicles from crossing onto the trail



- ADA parking spots should be located near the Saltbox to better access amenities and restrooms
- Permeable paving should be considered whenever possible
- Additional lighting could be considered to instill additional safety

Further research needed:

- Further engineering and design is needed to best determine the site's layout and capacity

## **Southern End/Highway Entry**

Current conditions and issues:

- The roadway entry is a sharp turn and not well defined – vehicles tend to drive on the trail immediately
- The powerlines locations are in the middle of the roadway leading to parking causing vehicles on the trail
  - If cars are park before the powerline, entering vehicles must resort to driving on trail
  - Water tends to gather in this area
- At the trail gateway, there is some signage including some mileage, trail rules and an unused brochure holder
- “Begin” post likely left over from events

Recommended solutions:

- Widen roadway into trail head
  - Mark trail and road crossing with signage and pavement markings
- It may be beneficial to site design and function if powerlines are relocated, if feasible
- The addition of rain gardens and re-sloping the area would help with the water buildup and brighten up the space
- Add a stop sign for cyclists at the entry way from the trail into trailhead for safety for incoming vehicles
- Remove “Begin” sign and relocate brochure holder to bulletin board

Further research needed:

- Further engineering study for the widening of the roadway and water collection issue

## **Riverfront/Greenspace**

Current conditions and issues:

- Picnic tables and benches are worn out and could use some freshening up
  - Also not placed well, some are located closely to slope
- Three flower planters are located at random points
- Unable to see the town on the other side from the trailhead
- Canoe launch path is not well defined and difficult to get in and out of
  - No signage marking the river access
- BBQ grill is not in a great location – on the edge of slope
- Exercise Area
  - Worn out and underutilized
- Pavilion is in good condition, ADA accessible, and heavily used
  - There is demand for an additional one
- There is limited trash receptacles and no recycling available

Recommended solutions:

- Picnic tables and benches need to be refurbished and better positioned in central location and/or throughout the trailhead
  - Consider painting benches and picnic tables in form of public art
- Add additional flower planters (if Garden Club is willing to contribute or maintain) and determine best locations with overall design of site
- Open the view-shed by clearing tree branches (overgrowth or dead trees) in one or two locations (to be identified). This will open up the view to create better visibility of town across the river.
- The canoe launch needs to be regraded for safety and visibility.
  - On the river edge, signage to identify amenities and access point, should be consistent with water trail signage standard in the region.
- BBQ grill should be moved away from slope and create better ADA accessibility.
- Exercise Area should be removed and materials salvaged, if possible.
  - The area could become more parking or additional pavilion site.
- Additional trash receptacle needed and recycling receptacles added. Locations to be considered include near the Saltbox and bulletin board, southern entry, and/or northern entry.

Further research needed:

- Determine if engineering or environmental studies need to be completed for the canoe launch area
- Determine if Young Professional Group will be taking on any activities with the exercise area or interested in assisting with redeveloping the space

## Public Art

Current conditions and issues:

- There is one piece currently installed between the bridge and Saltbox
- There is a mural painted on the side of the vacant commercial building on the northern area

Recommended solutions:

- Current art piece could be incorporated into larger art piece or relocated at trailhead
- Regional interest to create archways in each town, but include unique component to Franklin
- Bridge supports under could be utilized for new mural – this could include some wayfinding
- Art at trailhead could reflect images such as architecture, town scenes, etc to show trail users what is across the bridge
- Reuse wood and materials from exercise park
- Suggested locations:
  - Southern gateway
  - Side of Saltbox building
  - Under the bridge

Further research needed:

- Determine best locations and medium to be installed
- Public input is recommended to build support and consensus

## Franklin Signage Recommendations

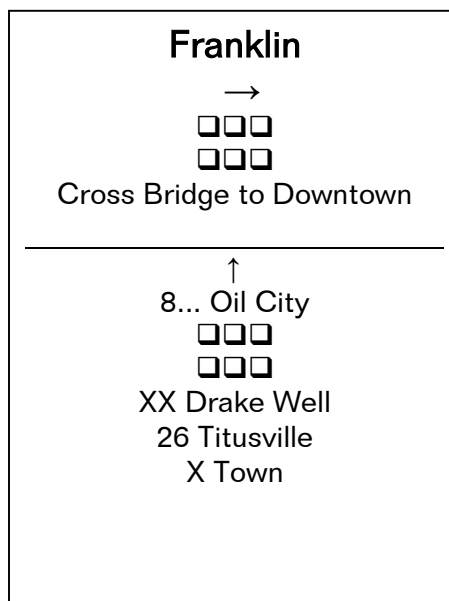
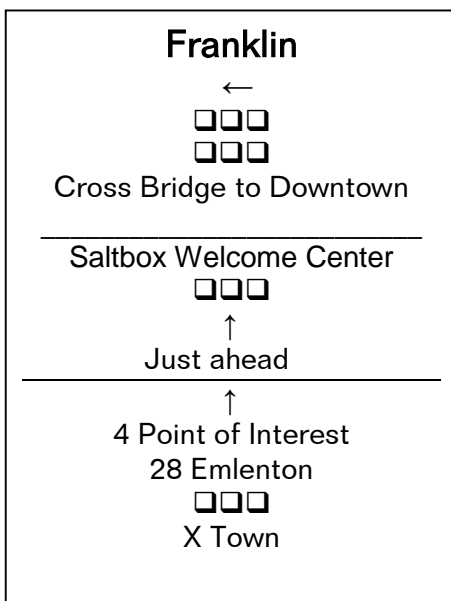
The following are suggested signage designs to better guide visitors along the trail and into town. Other communities including Emlenton and Foxburg have already added Erie to Pittsburgh Trail informational kiosks. Franklin can be a model for other communities looking to add and improve wayfinding signage along the Erie to Pittsburgh Trail. Panels could be updated as needed especially when more trail is connected.

### Parking Access:

- From the workshop, it was suggested the existing TODS sign be repositioned so that it can be better recognized while driving.
- If the Erie to Pittsburgh Trail Alliance adopts new trail-wide guidelines (or guidelines similar to the GAP trail), a new large totem should be placed to be visible for road traffic replacing the existing wooden sign.
- Some tree branch clearing may help open up the visibility of the trail and the access road.

### Northern End “Triangle”:

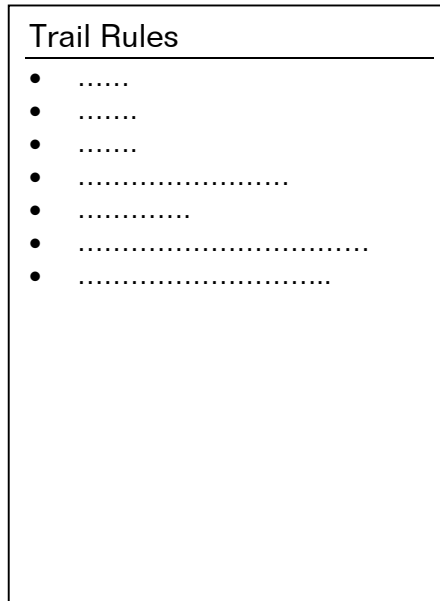
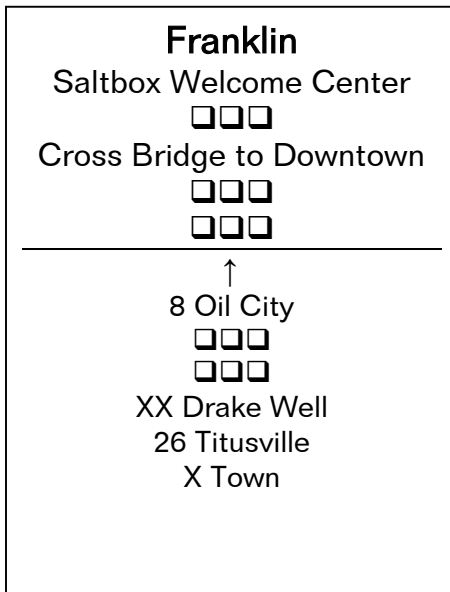
- To call out the connection to the bridge, a two sided totem with icons and mileage should be placed at this site
- An additional “trailblazer” type sign should be placed at the top of the hill with one side marked “To downtown” and other “To trail”





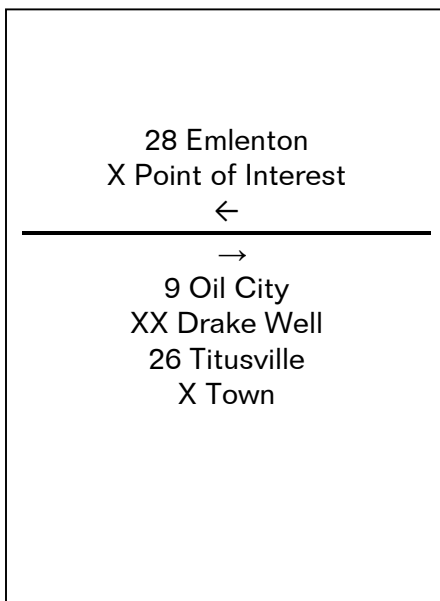
### Southern End:

- New signage would replace the existing signage
- Additional language on the icon panel could call out “Use caution: road crossing ahead”



### Near Parking Area or Gathering Place:

Mileage to nearby points of interest and towns should be marked. An additional trail rules sign could be added in this area or reposted on the bulletin board. A kiosk, similar to those in Emlenton and Foxburg, featuring regional trail map, interpretive, town map/business directory should be consider for future signage option. This would not replace information boards with up to date information.



## Photos from Charrette 5-18-2016



Photo 1



Photo 2

Photo 1: Road/Pathway to Bridge

- Add sharrows, landscaping, “To Trail/To Town” signage

Photo 2: Northern End “Triangle”

- Add wayfinding signage with icons – can be new sign or possibly attached to pole, and add bollard at trail and pathway intersection



Photo 3

Photo 3: Bridge to Saltbox

- Area can be redesigned to include the following:
  - Improved/paved connection to ADA ramp
  - Move restrooms
  - Move/add bike racks
  - Add bike fixit station, benches, picnic tables, kiosk, and other amenities
  - Add landscaping
  - If positioned properly, area could support new pavilion





Photo 4

Photo 4: Saltbox and Information Hub

- Repair/re-stain bulletin board
- Reorganize and add visitor information including
  - Upcoming events and activities
  - Online resources



Photo 5

Photo 5: Parking Area

- Further research is needed to determine engineering and design
- Delineate parking between trail with boulders, planters, etc to increase safety to site
- Line parking spaces to better designate available parking, curb stops can be added



Rockwood Trailhead with boulders separating trail and parking





Photo 6



Photo 7

Photo 6 and 7:

- Widen roadway to parking area
- Replace road signage
- Consider clearing/trimming trees approaching the entry roadway
- Add cyclist stop sign at trail and roadway intersection

Inventory of Existing Pedestrian Curb Ramps and Walks

Intersection #	East-West Road**	North-South Road**	# of Ramps Compliant w/Detectable Warning Surface	# of Ramps need Detectable Warning Surface only	# of Ramps Non-Compliant	Non-Compliant Ramps*	# of Missing Ramps	Missing Ramps	Existing Signal?	Pedestrian Push Buttons	Pedestrian Signal Heads	Notes for Existing Conditions
1	8th Street	Riverfront Park	1	1	0	--	0	--	No	--	--	Widen northern sidewalk from 4' to 10' for 80' to bridge. Extend southern sidewalk 40' to existing curb cut and construct ADA ramp. This may be a midblock crossing of a state route.
2	8th Street	Elk Street	0	7	1	NEW	0	--	Yes	None	8 Man/Hand Non-countdown	
3	9th Street	Elk Street	0	1	5	NWS, NWE, NEW, NES, SEN	2	SWE, SEW	No	--	--	Important intersection next to Riverfront Park
4	9th Street	Liberty Street	0	4	4	NWS, NWE, NEW, SEN	0	--	Yes	4 for Liberty St crossings only. These are too far from road edge.	8 Walk/Don't Walk Non-countdown	Needs stub poles to put push buttons closer to road edge.
5	9th Street	Buffalo Street	0	2	5	NWS, NWE, NEW, SEN, SWN	1	SWE	No	--	--	
6	10th Street	Buffalo Street	0	3	5	NWS,NWE, SEN, SEW, SWE	0	--	No	--	--	School on SE corner
7	10th Street	Elk Street	0	2	4	NWS, NWE, SEN,	2	SWE, SEW	No	--	--	Two ramps in NE corner were under construction
8	Washington Crossing	Elk Street	0	2	4	NW corner, NE corner	0	--	Yes	None	6 Walk/Don't Walk Non-countdown. No	No crossing of southern leg because there is no sidewalk.
9	Washington Crossing	Liberty Street	0	4	2	NES, SEW	0	--	Yes	Yes, with audio	6 Walk/Don't Walk Non-countdown. No	No crossing of northern leg.
10	11th Street	Buffalo Street	8	0	0	--	0	--	No	--	--	
11	11th Street	Liberty Street	2	4	2	NES, SEN	0	--	Yes	4 for Liberty St crossings only.	8 Walk/Don't Walk Non-	
12	11th Street	Elk Street	0	0	8	All 8 ramps	0	--	No	--	--	
13	S Park Street	Elk Street	2	4	2	SEW, SWE	0	--	No	--	--	
14	S Park Street	Liberty Street	2	1	1	SWE	2	NWE, NEW	No	--	--	T intersection
15	12th Street	Buffalo Street	8	0	0	--	0	--	No	--	--	
16	12th Street	Liberty Street	5	0	3	NWS, NWE, SWN	0	--	Yes	None	8 Hand/Man. Non-countdown	Curb extensions on 1.5 corners. Should consider curb extensions on NE and SW corners and NWE corner. Faded textured crosswalks.
17	12th Street	Elk Street	4	1	0	--	1	NEW	No	--	--	Consider curb extensions on western park side. Consider all-way stop control.
18	Elk Street	W Park Street	1	3	2	SWN, SWE	0	--	No	--	--	Corner ramps in NW and SE corners.
19	Liberty Street	W Park Street	2	0	2	NWE, SEN	0	--	No	--	--	T intersection. Faded textured crosswalks. No crossing on western leg. Curb extensions on 3 corners.
20	Liberty Street	13th Street	7	0	0	--	0	--	Yes	4 for pedestrian exclusive phase with bell	8 Hand/Man. Non-countdown	Faded textured crosswalks. Corner ramp in NE corner.
21	Buffalo Street	13th Street	2	5	1	SWN	0	--	Yes	None	8 Hand/Man. Non-countdown	Manhole in SW corner sidewalk ramp.
22	Buffalo Street	14th Street	0	1	5	NWS, NWE, NEW, SWE, SWN	2	NWS, SWN	No	--	--	
23	Liberty Street	14th Street	3	3	2	NWS, NES	0	--	Yes	4 for Liberty St crossings only.	Yes, 8 Hand/Man with countdown.	
24	Elk Street	13th Street	8	0	0	--	0	--	Yes	None	8 Walk/Don't Walk Non-	Slight curb extensions on NE and SE corners.
25	Elk Street	14th Street	5	0	1	SWE	2	NWS, SWN	No	--	--	

60

48

59

12

10

\* First 2 letters are quadrant (NW, NE, SE, SW). Third letter is the direction the ramp is facing. For example, NEW=Ramp is in northeast corner facing west.

\*\* Intersections 1 thru 17 Liberty St is North-South. Intersections 18-25 Liberty St is East-West.

## Accident statistics for the intersection of 12<sup>th</sup> and Liberty streets in Franklin

01/01/2004 to 01/05/2017\*

- Total accidents – 92
- Reportable (Those which resulted in injuries and/or damage so severe the vehicles had to be towed from the scene) – 31
- Non-Reportable – 48
- Unknown – 13
- Vehicles involved – 179
- People Injured – 23
- Accidents involving pedestrians – 7; Pedestrian accidents occurred on 05/09/2004, 03/03/2006, 09/11/2011, 10/26/2011, 02/14/2013, 06/20/2014, 06/11/2015.

\*Based on data provided by the Franklin police department

### **Pedestrian hit by vehicle**

Posted: Saturday, February 16, 2013

The Venango County 911 Center confirmed Friday that a pedestrian was hit by a vehicle Thursday night in Franklin. Officials said the call came about 7:20 p.m. Thursday for a person struck near the intersection of 12th and Liberty streets near the Venango County Courthouse.

Reports on Friday indicated the victim was a woman and that she had been flown to a Pittsburgh hospital for treatment of her injuries. Franklin police had not released any information about the incident early Friday evening. Franklin police and fire departments and Community Ambulance Service responded to the call.