



COMPREHENSIVE PLAN

2024
UPDATE

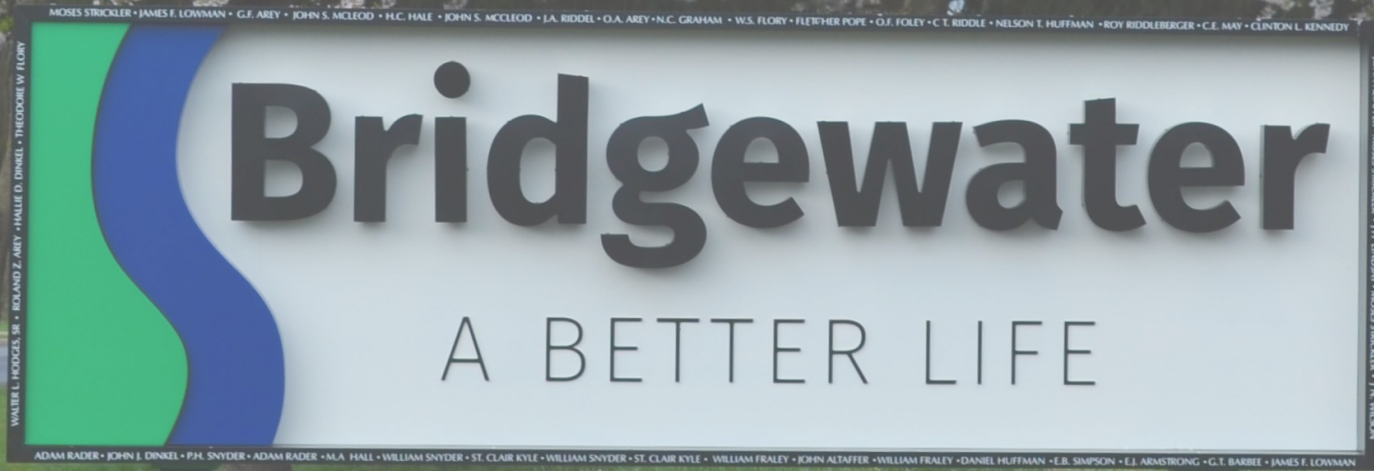


Table of Contents

Module 1: Profile of Bridgewater.....	1	Module 6: Public Services.....	36
Module 2: Natural Environment.....	3	Module 7: Land Use.....	45
Module 3: Population and Housing.....	7	Module 8: Featured Capital Improvements.....	48
Module 4: Employment and Commerce.....	11	Appendices	
Module 5: Transportation.....	15		

MODULE 1: PROFILE OF BRIDGEWATER

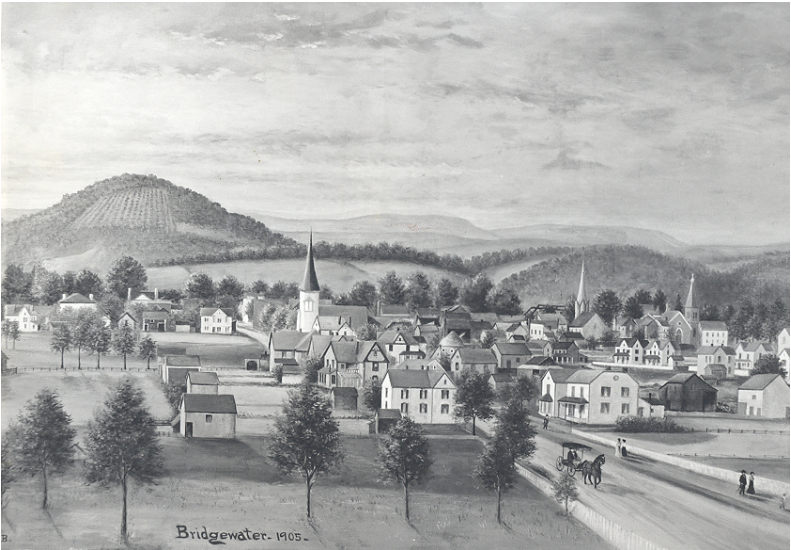
TOWN HISTORY

The first settlers in the area now known as Bridgewater arrived in the 1740's and were of Scots Irish descent. They were soon followed by German settlers. The area became known as McGill's Ford around 1759 because John and William McGill owned approximately 350 acres on either side of the North River. Several early families built mills along the North River and the settlement came to be known as Bridgeport since it was a port for flatboats to ship products down the river.

Around 1800, John and Jacob Dinkle arrived with their families and started a sawmill, gristmill, cording mill, and stave mill. Jacob Dinkle's tavern, built in 1815 on South Main Street, and John Dinkle's two-story brick house, built around 1820 on Wynant Place, are still standing. The settlement soon became known as Dinkletown. On February 7, 1835, the settlement was chartered as the Town of Bridgewater. The name was derived from its proximity to the North River and the bridge that was built there in 1820. The original land area within the town was approximately twenty acres.

Bridgewater grew as a location of trade during the 1800's due to its proximity to major transportation routes. The Warm Springs-Harrisonburg Turnpike, which passed through Bridgewater and North River, was navigable by flatboats as far upstream as the town. By 1850, Bridgewater had two gristmills, two cabinet shops, several general stores, a tavern, and a hotel. Bridgewater College was chartered in 1880.

The town also continued to grow in land area. After an election held by local citizens, the area known as "Old Town" annexed the area known as "New Town" in 1854. A new Town charter was granted in 1884 which extended the boundaries to include an additional 100 acres on the southeast side of town. The new charter also provided that a mayor and six councilmen be elected to govern town affairs.



Annexation initiatives took place in 1895 adding approximately 300 acres on the southeast, northeast, and southwest sides of town. Further annexations took place during the 1980's and 1990's adding land to the east and north.

TODAY



Fireworks over the Bridgewater Community Center during Eve of Independence celebrations.

The Town of Bridgewater presently encompasses approximately 2,477.5 square miles (about 1,586 acres). The Town continues to serve as an area for trade and commerce with government and professional offices, banks, restaurants, and a variety of retail establishments. The Town also is an employment center with several major industries located within its borders. Bridgewater College, a private educational institution, and Bridgewater Retirement Community are also located within the Town.

The Town of Bridgewater is located along the northern edge of the North River near the southern border of Rockingham County. The City of Harrisonburg, the county seat, is located approximately five miles to the northeast. Other nearby urban areas include the City of Staunton (25 miles southwest), the City of Charlottesville (60 miles southeast), and the City of Winchester (75 miles northeast). The Washington D.C. and Richmond metropolitan areas are within a two-and one-half-hour drive.

Six Town Council members and the Mayor form the governing body of the Town of Bridgewater. The Mayor and all Council members are elected at large. The Town Manager oversees the day-to-day management of town operations and coordinates long-range planning and budgeting activities.

A seven-member Planning Commission appointed by Town Council oversees planning activities and reviews requests for rezoning and amendments to Town ordinances pertaining to zoning and land development activities. A court-appointed Board of Zoning Appeals considers requests for variances from Zoning Ordinance requirements. A seven-member Industrial Development Authority, also appointed by Town Council, issues bonds and supports economic development in Bridgewater.

MODULE 2: NATURAL ENVIRONMENT

BACKGROUND

Physical and natural features have helped to determine the pattern of development that has taken place in Bridgewater. Sensitive features of the natural environment are identified and incorporated into community planning activities in order to provide a pleasant and safe environment for residents. Growth and development should be sensitive to natural drainage systems, steep slopes, and soil characteristics to avoid leaving problems for future generations to correct.

DRAINAGE

The majority of Town land lies within the Dry River-North River drainage basin. A small northeastern segment of the Town drains into Cooks Creek, which empties into the North River southeast of Bridgewater. The North River flows southeast until joining with the Middle and South Rivers to form the South Fork of the Shenandoah River.

The location of the Town along the edge of Dry River and North River results in periodic flooding to portions of the Town during periods of heavy rain. A major flood in 1949 resulted in the loss of three lives and in substantial damage to property within the Town. An earthen levee was built by the Army Corps of Engineers in 1951 in the northwestern section of the Town to offer flood protection. Major floods also occurred in 1985 and 1996. Due to the existence of the levee, the resulting damage of these floods was minimal with the exception of properties located along the bank of North River.



A portion of the levee on the northwestern portion of Town.

Floodplain maps prepared by the Federal Emergency Management Agency (FEMA) indicate 100-year floodplains of 750 to 1,500 feet in width along the Dry River and 40 to 1,500 feet along the North River. The levee along the Dry River protects adjacent residential areas from 100-year flood events. The North River 100-year floodplain along the southern border of Town extends into areas of residential and commercial development.

In 2014, FEMA reevaluated the 100-year floodplain. Approximately 200

acres of Town land lie within the 100-year floodplain, which is approximately seven percent of Town land.

Areas that lie within the 100-year floodplain are not suitable for development. These areas generally are suited for recreational purposes, agriculture, or limited use as parking areas.



Cooks Creek Arboretum, in the floodplain, is now an 8-acre park maintained by the Town.

TOPOGRAPHY

The Town lies along the banks of the North and Dry Rivers within a small valley formed by low ridges along its eastern, northern, and western edges. The Dry River, and its associated floodplain, create a small gap among these ridges at the northwest corner of Town. Elevations above sea level range from a low of 1,160 feet along the North River and a high of 1,300 feet in the area of Breezewood Terrace. The majority of the Town lies at an elevation of approximately 1,190 feet.

Most of the land within the Town is characterized as level to gently rolling with slopes of less than 15%. Slopes greater than 15% are located along the banks of the North River and on the western edge of the prominent ridge line associated with the eastern and northern Town limits.

Approximately 38.5 acres of land within the Town limits has slopes of 15 - 25%. Approximately 16 acres of Town land has slopes that exceed 25%.

Areas with slopes between 15-25% generally pose limitations to development but can be considered suitable for development if proper site design and engineering practices are followed. Slopes of 25% or greater pose serious constraints on development and are generally considered unsuitable for building purposes. Grading for roads, utilities, and other land clearing activities on steep slopes (25% or greater) can lead to accelerated rates of soil erosion, possible slope failure, and increased volumes and rates of stormwater runoff.

SOIL CHARACTERISTICS

The United States Soil Conservation (SCS) has compiled information on soil characteristics for the Bridgewater area.

The soils within the Town of Bridgewater pose very few development constraints. Approximately 85% of the Town is classified by the SCS as having Class I or II soils. These soils are well-drained and are generally suitable for development and placement of structures. The remaining soils within the Town are characterized by poor drainage or excessive wetness, shallow depth to bedrock, and are prone to water and wind erosion. These soils are generally associated with floodplains and steep slopes.

Some areas of Town contain shrink-swell soils. When this condition is identified, special construction requirements need to be considered.

VEGETATION

The Town of Bridgewater has planted more than 2,000 trees since 1990. A portion of this expense was funded by the Virginia Department of Forestry and the U.S. Small Business Administration. Mature trees and woodlands are found along the banks of North and Dry Rivers. Small woodlands also exist near Mount Crawford Avenue and College Woods Drive. Most trees and vegetation in Bridgewater are associated with single-family homes and public development.

GOAL: PRESERVE AND ENHANCE THE NATURAL RESOURCES OF BRIDGEWATER AND SURROUNDING AREAS TO THE GREATEST EXTENT PRACTICABLE.

OBJECTIVE: Identify and preserve sensitive or fragile environmental features.

- The Town should continue enforcing soil and erosion regulations as established by the Department of Conservation and Recreation (DCR). Bonding is currently required of all developers.

OBJECTIVE: Encourage innovative site planning techniques to protect sensitive environmental areas.

- Future development should be encouraged to cluster buildings and dedicate permanent open space to the Town.
- The Town should consider a "traditional neighborhood design" ordinance for this reason, among others.

OBJECTIVE: Maintain a healthy and attractive balance between the built and natural environment.

- The Town should continue its practice of planting trees and encouraging developers to do so through the Green Law. A mature tree canopy will not only make for a more attractive Town, but it will also reduce energy costs and buffer extreme summer temperatures.
- Continue to work with our legal staff to develop landscape standards.
- Town ordinances should expand landscaping requirements to include planting of trees for all multi-family residential, commercial, and industrial development.
- The Town should continue its policy of planting trees along new streets.
- Develop a comprehensive beautification and enhancement program for its central business district and vehicular entrances.

OBJECTIVE: Preserve and enhance the quality of the North River.

- Continue our policy of weekly street sweeping to prevent the discharge of sand, gravel, grit, and petroleum products into the North River.
- Limit withdrawals from the North River during times of drought.

MODULE 3: POPULATION & HOUSING

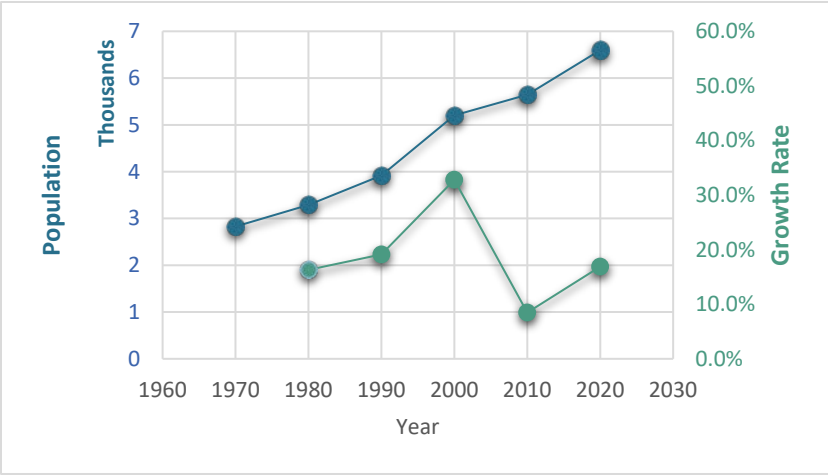
PLANNING HISTORY

A community's health and vitality is determined by the characteristics of its existing population and the ability to attract new residents. Availability, cost, and condition of the housing stock are also major factors in helping to determine the composition of the community's population. Bridgewater's housing stock is a prime asset in ensuring the future vitality of the community.

POPULATION

Bridgewater experienced a steadily increasing rate of population growth over five decades. From 1970 – 2020 the population more than doubled. Bridgewater is fulfilling its role as a growth center as envisioned in both the Town and County Comprehensive Plans. The following chart illustrates the growth that has occurred over this time period.

BRIDGEWATER POPULATION GROWTH						
Year:	1970	1980	1990	2000	2010	2020
Population:	2,828	3,289	3,918	5,203	5,644	6,597
Growth Rate:		16.3%	19.1%	32.8%	8.5%	16.9%



In 1985 and 1997, the Town responded to a limited supply of developable land by completing agreements with the County to annex additional land.

Economic trends for expansion are positive. Four of Bridgewater's top five employers have long been located here, and ten of the top 35 employers in Rockingham County have Bridgewater locations (see page 12). People will continue to be drawn to the Bridgewater area because of the economic opportunities. Additionally, with an uptick in remote jobs since the COVID pandemic, the Town has seen an increased number of new

residents who choose to live in Bridgewater while working for corporations located in larger cities. Bridgewater is also a desirable place to live due to moderate weather, physical attractiveness of the community, the existence of Turner Ashby High School within our borders, and a low crime rate. Lastly, the ice rink, golf course, mini-golf, Sipe Center, parks, recycling center and other amenities and services provided by the Town are making Bridgewater a more and more popular living destination.

HOUSING



Town parks contribute to making Bridgewater a popular living destination.

Our largest decade of growth occurred in the 1990's. During that time 485 new housing units were constructed. Housing in Bridgewater can be broken down into three main categories: single family detached, single family attached (such as townhouses and duplexes), and multi-family (such as apartments).

The housing stock in Bridgewater is relatively young with more than 75% of the housing units being built after 1960. In the past decade, more than 275 new housing units have been constructed and initial plans are underway for 120 new townhomes at Bridgewater Fields and 139 townhomes at The Glen at Cooks Creek, a proposed development near Turner Ashby High School.

During the boom in the housing market experienced in the early 2020's, Town staff noticed an increase in the development of long vacant properties in Bridgewater's residential districts. Most of these properties were dimensionally non-conforming. In general, the Town's approach is that a property owner should be allowed to develop his property in keeping with the use regulations for each zoning district. In these non-conforming cases, Bridgewater's Zoning Code allows for the Board of Zoning Appeals to

establish new area regulations and setbacks for the individual property, thus bringing it into conformance and offering a higher potential for development.

One unique aspect of the Town is the Bridgewater Retirement Community (BRC). Apartment and duplex residences are available to retirees who do not need the regular nursing care of supervised residential care environments. Nursing care facilities are available at Bridgewater Home to these residents if needed in the future. BRC has seen recent expansion with the 2020 completion of The Grove which offers 58 new independent-living apartments for BRC residents and a 2021 expansion of their masterplan adopting 6 additional properties into their Planned Unit Development. In the fall of 2023, BRC completed a network of scenic and serene walking trails that are available to retirement community and town residents alike. Bridgewater Retirement Community has proven itself to be a desirable place to live.



Bridgewater Retirement Community expanded their masterplan in 2020.

In summary, the many types of residences available in Bridgewater provides a variety of housing choices for Town residents. Apartments and other rental units provide housing opportunities for young people just entering the job market and beginning careers. It is also appealing to older citizens seeking reduced housing maintenance costs and smaller living spaces. Townhouses, duplexes, and four-plexes can also provide affordable housing opportunities for families or individuals.

GOAL: ACCOMMODATE BENEFICIAL GROWTH THROUGH A VARIETY OF HOUSING MODELS.

RATIONALE: Growth for the sake of growth is of little concern to us. We understand, however, that as we continue to improve the quality of life in Town, more people will want to live here. Thus, we anticipate rising development pressure, and we should guard against that pressure leading to incompatible developments beyond our boundaries. Our goal, therefore, is to accept new developments inside or outside of our current

boundaries, but only to the extent that such developments can be said to maintain or improve the quality of life for Bridgewater residents.

OBJECTIVE: Continue to develop an appeal to residents of all ages.

- Town officials should ensure that Bridgewater remains an attractive, vibrant, comfortable, and safe place to live through the provision of public services and maintenance activities.
- Town officials should keep current ordinances in place that prevent residential growth in industrial areas and restrict it to special use provisions in business areas.
- Town officials should anticipate considerable development pressure and put in place the ordinances and agreements necessary to accommodate this growth.

OBJECTIVE: Encourage a variety of housing models, so long as each provides a good quality of life for the people within and enhances the wider community.

- Town officials should draft and adopt Traditional Neighborhood Development ordinance.
- Town officials should ensure that the Zoning Code does not hinder the development or preservation of manufactured homes except in cases where they may present safety concerns, such as flooding or other natural hazards.

OBJECTIVE: Identify adjacent land areas appropriate for new residential growth to continue providing adequate choices and availability for new residents.

- The Town should identify sites within the annexation area which are best suited for residential development, considering topographic conditions, traffic circulation, and the extension of municipal services.

OBJECTIVE: Reduce the overall number of structures within our regulatory floodplain.

- Consider purchasing for demolition competitively priced properties.
- Revise floodplain ordinance to closely mirror FEMA's model ordinance.



The Town has long followed a policy of buying floodplain land as it becomes available and developing it into public parkland which is less vulnerable to costly damage when flooding occurs.

MODULE 4: EMPLOYMENT & COMMERCE

BACKGROUND

A diversity of commercial, industrial, and quasi-public employers provides a stable tax base for the Town and a broad choice of employment opportunities. Tax revenues from these employers assist in providing public services and amenities to Town residents.

To provide a meaningful economic analysis, it is necessary to look at the economy of the combined Rockingham/Harrisonburg/Bridgewater community, since the economies of the three jurisdictions are highly intertwined. Many of the area's employment centers are directly related to agribusiness which flourishes in the County. Likewise, County residents benefit from the convenience of having commercial and service centers centrally located in the City and Town.

Because of its diversity and its large agricultural base, the area economy is strong and generally less affected by national economic fluctuations than more urbanized areas. Area growth has been aided by easy access to the interstate highway system, proximity to major metropolitan markets, and the availability of water resources.

COMMERCE

The ten largest employers in Rockingham County represent the industries of education, health care, manufacturing and distribution, public administration, and hospitality/recreation. Ten of the top 35 employers in Rockingham County have locations in Bridgewater including: Marshalls (1274), Perdue Farms (519), Bridgewater Retirement Community (467), Bridgewater College (399), and Shickel Corporation (112). These institutional entities are responsible for a significant portion of our revenues. Harrisonburg functions as the regional retail center in Rockingham County, drawing from a broad market area which includes the Town of Bridgewater. Town residents and Bridgewater College students form the primary customer base for retail and



service establishments in the Town. Most of these establishments are located along Main Street.

Recent population growth has created a community of over 6,500 residents (including 1,385 residential college students). This increased size should provide the impetus for commercial development with more diverse shopping opportunities within the Town borders. A major food market, pharmacy, clothing stores, and increased services should all be supportable in a community of 6,500 people.

Tourism is another potential revenue source which has been increasing with the addition of the Generations Park Ice Rink, the Sandy Bottom Mini-Golf Course (as well as the Par-3 course), Sipe Center, Riverwalk, and the expected opening of Macado's in 2025. Future projects such as the proposed Convocation Hall at Harrison Park promise to increase the number of outsiders making their way to Bridgewater.

Top Employers in Rockingham County

#		#	
1	Rockingham County School Board	19	Bridgewater Home, Inc.
2	Sentara Healthcare	20	Sunnyside Presbyterian Home
3	Wal Mart	21	VPGC LLC
4	Cargill Meat Solutions	22	Shenandoah Growers Inc
5	Merck Sharp & Dohme Corp	23	Interchange Group Inc
6	Marshall's	24	Food Lion
7	Great Eastern Resort Management	25	Science Applications International Corporation
8	Pilgrims Pride Corp	26	Great Eastern Resort Corporation
9	LSC communications Book LLC	27	Shenandoah Valley Electric
10	County of Rockingham	28	Lantz Construction Company
11	Danone North America Public Benefit	29	Avant Healthcare Professionals
12	Millercoors LLC	30	Massanutten Ski Lodge
13	QSi LLC	31	Rockingham Co-op Farm Bureau Inc.
14	Perdue Products	32	Partners Excavating Company Corporation
15	Dynamic Aviation Group	33	McDonalds
16	Sysco Food Services	34	Farmer's and Merchants Bank
17	Riddleberg Brothers, Inc	35	Shickel Corporation
18	Bridgewater College	Locations in Bridgewater are highlighted.	

Source: Virginia Employment Commission, Economic Information & Analytics, Quarterly Census of Employment and Wages (QCEW), 2nd Quarter (April, May, June) 2023.

GOAL: MAINTAIN AND EXPAND THE STRONG EMPLOYMENT BASE OF BRIDGEWATER.

OBJECTIVE: Retain and strengthen present establishments within the Town.

- Work with local industry representatives to ensure that adequate water and sewer capacity is available for future expansions.
- Continue to improve the transportation network in the industrial area to allow simultaneously easier access for industries and a safer, calmer experience for pedestrians and other motorists.

OBJECTIVE: Seek opportunities to expand and build upon the present commercial base.

- Continue to promote the idea of Bridgewater being a good place to locate or expand a business.

OBJECTIVE: Promote and encourage tourism activities.



- Continue to improve the Town's amenities that draw outsiders, such as Generations Park Ice Rink, Sandy Bottom Par-3 and Mini-Golf, Sipe Center, Riverwalk, and the proposed Convocation Hall at Harrison Park.
- Document the Town's historic resources, and other resources that might attract visitors, to create an awareness of these assets. Improve and preserve the appearance of historic structures within the Town. Encourage their preservation and adaptive use.
- Continue to hold Charter Day activities, Eve of Independence, Summer's End Celebration, and other special events celebrating the community to promote civic pride and attract visitors from the county and surrounding region.
- Permit bed and breakfast establishments, Airbnb's, and small inns in areas appropriate for such uses.

GOAL: PROVIDE A RANGE OF GOODS AND SERVICES TO MEET RESIDENT NEEDS AND MAINTAIN THE ECONOMIC VIABILITY OF THE EXISTING DOWNTOWN COMMERCIAL AREA.

OBJECTIVE: Provide for a mixture of retail stores, business and government offices, restaurants, personal and professional services, financial services, and other related and appropriate uses within the Town.

- Ensure that Town land-use regulations permit a variety of commercial and business uses to promote diversity and flexibility in business location decisions.
- Town officials should work with local business owners to help retain existing businesses and promote their expansion.
- Continue to encourage and facilitate (when possible) development and beautification of properties along Main Street to protect the existing character of the area. This area, particularly from the North River to North River Road, represents the traditional commercial activity center of the Town and has undergone a higher density and intensity of development than other areas.
- Ensure that the northern Main Street commercial area continues to be a site for commercial and retail development.

OBJECTIVE: Promote Bridgewater as a shopping destination.

- Continue to improve the appearance of Main Street and seek the cooperation of local businesses in this effort.
- Continue to seek out businesses that would complement and expand the present mix of goods and services available to Town residents.
- Continue to provide an inventory of commercial properties available for lease or sale to facilitate business expansion or recruitment.

MODULE 5: TRANSPORTATION

INTRODUCTION

The Town of Bridgewater Comprehensive Plan provides guidance for the performance and development of the Town's transportation system. The transportation recommendations in this module meet current needs and identify future improvements.

This module addresses the following topics:

- Inventory and Analysis
- Land Use and Planning Assumptions
- Transportation System Needs Assessment
- Current Transportation Projects and Recommendations
- Future Transportation Goals and Strategies

PURPOSE

The local and regional transportation network is a vital component of any community. Commerce and industry must have access to highways, railways, and air transport to conduct business. Vehicular traffic itself provides customers for local businesses and the U.S. Surface Transportation Assistance Act of 1982 ("STAA") guides certain truck passage along Dinkel Avenue and part of Main Street.



Residents safely walking along Riverwalk under Main Street.

That said, the transportation network must also serve the needs of townspeople. It must allow the ready flow of traffic *within* the town, and not just *through* the town, as well as safe, convenient, and pleasant travel by foot or on bicycle.

The transportation network and land-use regulation must not work at cross purposes. Land regulation must not create the need for unnecessary transportation, and the transportation system must not needlessly interfere with beneficial land uses.

INVENTORY AND ANALYSIS

This section will review the following categories:

- Existing Roads
- Bicycle and Pedestrian Facilities
- Truck Routes
- Public Transportation
- Air Travel

EXISTING ROADS

Bridgewater uses local, state, and federal funding for its transportation projects. Streets are considered and paved as needed, based on use and pavement deterioration. Appendix 5-1 catalogs the functional classifications of Town streets and the Annual Average Daily Traffic statistics for these streets. Also, see Appendix 5-2 for Bridgewater paving records and Appendix 5-3 for VDOT's Urban Maintenance Inventory.

VDOT's Six Year Improvement Program (SYIP) outlines planned spending for transportation projects currently proposed for construction or study. There are five projects currently noted in the Virginia Transportation Six-Year Program (FY 2019-2024) for the Town of Bridgewater. See Table 5-1 below for the most recent Six-Year Program.

Table 5-1 Virginia Transportation Six-Year Program (FY2023 Revised)

UPC	Description	Route	District	Road System	Estimate	Previous	FY23	FY24-28	Balance
					(Values in Thousands of Dollars)				
113896	GEN-OAK CONNECTOR - BRIDGEWATER	0700	Staunton	Enhancement	\$299	\$255	\$44	\$0	\$0
116859	RIVERWALK PHASE 2	EN20	Staunton	Enhancement	\$327	\$271	\$0	\$0	\$55
121208	RIVERWALK PHASE III ENHANCEMENT	EN22	Staunton	Enhancement	\$480	\$0	\$353	\$128	\$0
121202	#SGR23LP BRIDGEWATER NORTH MAIN ST. PRIMARY EXT. PAVE.	0257	Staunton	Primary	\$206	\$0	\$206	\$0	\$0
80488	RTE 42 - IMPROVE RTE. 42 (MAIN STREET) IN BRIDGEWATER	0042	Staunton	Urban	\$1,986	\$2,120	\$0	\$0	(\$134)
113894	MAIN STREET PEDESTRIAN SAFETY IMPROVEMENT - BRIDGEWATER	0042	Staunton	Urban	\$241	\$194	\$46	\$0	\$0

While most of these project titles above are self-explanatory, one may seem vague. Project 80488 - Route 42 is the new stoplight, crosswalk signals and turning lane installed at the Dylan-Old River-Main Street ("DORM") completed in 2022.

MAIN STREET

Main Street is an arterial, north-south highway, stretching 1.9 miles from the town's southern boundary to its northern boundary. Ten streets enter onto Main Street from the west, and 14 streets enter from the east. The streets entering from the west line up with streets entering from the east in only four locations.

There are six signalized intersections on Main Street. The traffic signal at Dylan Circle-Old River Road-Main Street ("DORM"), along with new pedestrian crosswalk signals and a southbound turning lane onto Old River Road, was completed in 2022.

On its most-traveled section, from Mt. Crawford Avenue to the northern boundary, Main Street carries about 22,000 vehicles per day.

DINKEL AVENUE

Dinkel Avenue is an arterial, east-west highway. It stretches 1.73 miles, from the Town's eastern boundary to Main Street. On its most-traveled section, from Mt. Crawford Avenue to the eastern boundary, Dinkel Avenue carries about 9,700 vehicles per day. Dinkel



Asphalt stamping crosswalks on Dinkel Avenue in 2021.

Avenue serves as the Town's principal means of access to Interstate 81, though recent improvements to Airport Road, just south of town, make Dinkel a viable alternative for southbound journeys.

Over the past decade, the Town made extensive improvements to the Bridgewater College corridor of Dinkel Avenue to improve pedestrian safety. These improvements created clearly defined crosswalks and narrowed the roadway to slow traffic through campus. The Town also installed flashing radar speed signs and additional lighting to improve corridor safety. The speed limit was reduced to 25 mph in 2016 along this stretch of Dinkel Avenue.

OTHER ROADS

Aside from Main Street and Dinkel Avenue, four other streets lead out of town: North River Road to the west, Dry River Road to the northwest, Cannery Woods Lane to the north, and Oakwood Drive to the east. Additionally, Mt. Crawford Avenue runs out of town before bending back to Dinkel Avenue just inside the town's eastern boundary.

In addition to the two arterial streets, Bridgewater maintains about 20 miles of "collector" streets, in the terminology of Va. Code § 15.2-2223. The term is misleading, however, because "collecting" vehicles for the arterials is only a small part of their function. They serve to define and connect neighborhoods. They allow for pedestrian and bicycle traffic. They are the primary means through which utility services are distributed throughout town. In Bridgewater, as in any vibrant community, the neighborhood streets are akin to blood vessels in the human body.

BICYCLE AND PEDESTRIAN FACILITIES

RIVERWALK

Riverwalk, Phase I was completed in 2021 and was funded through a VDOT Highway Safety Improvement Program (HSIP) grant. Because it crosses *under* Main Street, Riverwalk offers the town's simplest, safest Main Street crossing. Riverwalk is designed to be a beautiful recreational amenity while also providing functional transportation. The Town will improve the functionality of Riverwalk by beginning Phase II in 2023. Phase II is funded through a VDOT HSIP grant and will extend the path through Bridgeview Park onto West Bank Street.



Phase I of Riverwalk was completed in 2021. Phase II is slated to begin in 2023. Design work is underway for Phase III

The Town received a Transportation Alternatives Grant for Phase III, which will extend the path to the east along East Riverside Drive. Engineering and design work will begin in 2023, with construction anticipated in 2025.

GEN-OAK CONNECTOR

The Gen-Oak Connector is a VDOT HSIP-funded project that involves the construction of a sidewalk and shared-use path to connect Generations Park on North Liberty Street with Oakwood Park on Mt. Crawford Avenue. Gen-Oak will fill gaps in our existing network of sidewalks and also install two crosswalks on Mt. Crawford Avenue (one at North Liberty Street and another at Naomi Lane near the entrance to Oakdale Park.)

Construction on the Gen-Oak Connector is scheduled to be completed in 2023. It will connect Bridgewater's busiest park, Oakdale, with Main Street's Generation's Park, the Town's outdoor summer concert venue and winter ice rink.

DRY RIVER ROAD

In 2021, the Dry River Road project was completed. Improvements to Dry River Road were proposed in the early 2000s, and was mostly funded by VDOT through Revenue Sharing. The Dry River Road Project installed a new sidewalk along the western side of Dry River

Road and bicycle lanes on both sides of Dry River Road. The project span was between Dry River Road's intersection with Cannery Woods Drive and Old River Road. The project also made stormwater improvements in this corridor.

OAKWOOD DRIVE

Renovations to Oakwood Drive, completed in 2017, provided a sidewalk along this heavily traveled road in Bridgewater's northeast. By completing the Oakwood Drive Project, we now have a sidewalk along the entire stretch of Oakwood Drive from Main Street to the Town's eastern border near Weeping Willow Lane.

TRUCK ROUTES

Bridgewater has a notable industrial district including a Perdue Plant and Marshalls Distribution Center. These and other industries, as well as a large Cargill Factory in neighboring Dayton, contribute a fair amount of freight traffic to Bridgewater's transportation system, especially on Main Street, which averages 880 trucks daily, and Dinkel Avenue, which averages 582 trucks daily.

PUBLIC TRANSPORTATION

Bridgewater is currently served by BRITE Bus Service on its Blue Ridge Community College (BRCC) Shuttle. Specifically, Bridgewater is part of the BRITE BRCC North Route, which operates five days per week. BRCC North buses are only available southbound in Bridgewater. Riders seeking to travel to Harrisonburg must travel south to BRCC and then are routed north to Harrisonburg before returning to Bridgewater. Three pick-up points are offered throughout town along Dinkel Avenue and Main Street. Data from October 2022 shows ridership around 24 users per week.

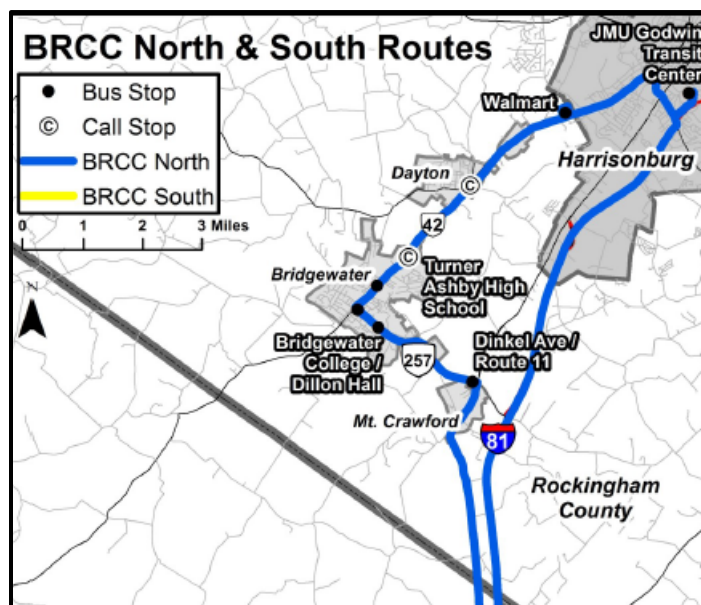


Figure 5-1. BRITE Bus North. The BRITE bus is an important element of our transportation network. Bridgewater has three stops on BRCC North.

Bridgewater is supportive of public transportation and believes it is an important part of our transportation planning. In 2020, the Town received a grant from the Central

Shenandoah Planning District Commission (CSPDC) for two bus stop shelters. These were installed at the bus stop locations near Bridgewater College and the 500 block of North Main Street.

AIR TRAVEL

Bridgewater's closest regional airport is the Shenandoah Valley Regional Airport, located approximately eleven miles southeast of town in Weyers Cave. A smaller, privately-owned airport, Bridgewater Air Park, is located on the town's southeastern border.

LAND USE AND PLANNING ASSUMPTIONS

Development trends and anticipated growth areas must be taken into consideration when planning transportation decisions and investing in infrastructure. The following factors will be considered in this section:

- Historic Traffic Volumes
- Population and Demographics
- Commuter Characteristics
- Land Use

HISTORIC TRAFFIC VOLUMES

As stated previously, Bridgewater's most heavily trafficked areas are Main Street and Dinkel Avenue. Main Street traffic counts have fluctuated significantly over the past several years as shown in Table 5-2 below. Meanwhile, Dinkel Avenue traffic counts have decreased in recent years, although 2020 and 2021 data may be skewed due to the COVID-19 pandemic.

Table 5-2 Data obtained from VDOT AADT Records

HISTORICAL TRAFFIC VOLUMES (Heavy Traffic Areas) TOWN OF BRIDGEWATER							
	2002	2010	2017	2018	2019	2020*	2021
Main Street	17,000	15,000	15,000	14,000	14,000	12,000	22,000
Dinkel Avenue	8,700	10,000	11,000	10,000	10,000	9,500	9,700

*variation from normal traffic in years prior due to COVID-19 according to VDOT traffic info

POPULATION AND DEMOGRAPHICS

Bridgewater population and demographics are discussed in detail in Module 2. Table 5-3 shows the town's population growth since the 1990 U.S. Census. Bridgewater's continued growth, and the growth of outlying communities, impacts vehicle traffic volumes and safety within the town.

Table 5-3 Population Growth Data (U.S. Census Bureau)

Population Growth Bridgewater, Virginia (U.S. Decennial Census)			
Year	Population	Growth	Growth Rate
1990	3,918		
2000	5,203	1,285	32.80%
2010	5,644	441	8.48%
2020	6,596	952	16.87%
*2030	7,104	508	7.70%

*Projected, based on 7.7% growth expected in Virginia between 2020 and 2030 (U.S. Census Bureau)

COMMUTER CHARACTERISTICS

Planners study commuter behavior to better understand the pattern, length, and type of work-related trips taking place in the area. Community characteristics are taken into account when determining future travel demand, traffic congestion management, and local economic activity.

According to the Census Bureau, in 2020, 70.2% of Bridgewater residents drove alone to work. 11.3% walked, and 6.98% worked at home. The average commute time for Bridgewater residents is 15.6 minutes. The average Bridgewater household owns two cars. Bridgewater residents often commute to the City of Harrisonburg, Rockingham County, or Augusta County for shopping and entertainment.

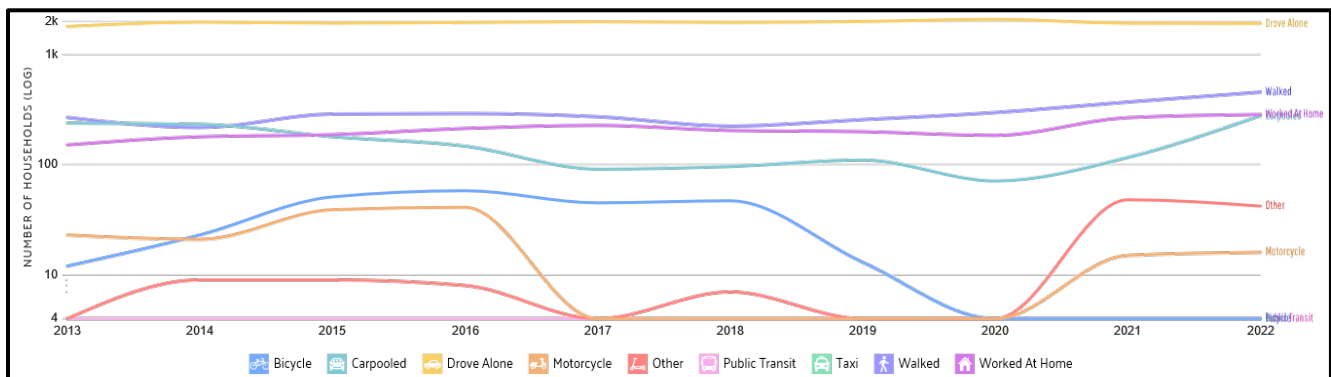


Figure 5-2. Comparison of modes of transportation to and from work, 2013-2020. (Source Data USA)

LAND USE

Appendix 5-4 depicts Bridgewater's land use and annexation area. As of 2023, the Town currently has no plans to annex property, and much of the buildable land in Bridgewater has already been developed. However, several anticipated developments noted on the map below will affect traffic volume and patterns in the community. These developments are:

- **EXPANSION OF BRIDGEWATER FIELDS**

Bridgewater Fields is the private development of townhomes in the eastern part of Bridgewater along Dinkel Avenue. Construction of Bridgewater Fields began in 2020. Currently, certificates of occupancy have been issued for 28 townhomes, and building has been approved for 22 more. When completed, this subdivision will consist of 160 townhomes. These properties are zoned R-3.

- **THE GLEN AT COOKS CREEK**

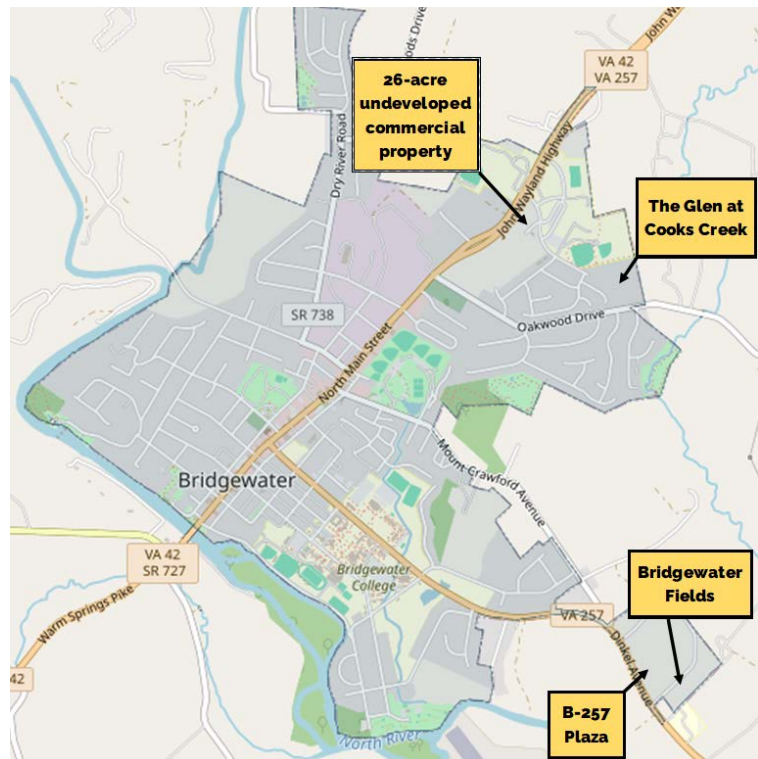
The Glen at Cooks Creek is another private development of townhomes, located in the northern part of Bridgewater near Turner Ashby High School and adjacent to Oakwood Drive. In 2022, developers began the process to develop this subdivision. It will consist of 139 upscale townhomes with access off Turner Ashby Drive. These properties are zoned R-3.

- **B-257 PLAZA**

The B-257 Plaza is a privately-development retail development located on Dinkel Avenue on the eastern side of Bridgewater. The property, totaling about ten acres, is adjacent to Bridgewater Fields. A portion of this property is currently under development, with this phase containing up to five commercial spaces. This property is zoned B-2.

- **COMMERCIAL PROPERTY ADJACENT TO TURNER ASHBY HIGH SCHOOL**

26-acres of land adjacent to Turner Ashby High School is privately-owned and available for commercial use. While this property has



Anticipated future development.

been vacant for many years, the owner has been actively pursuing sale of the property. It is zoned B-2.

NEEDS ASSESSMENT

This section will discuss specific needs in the Bridgewater transportation system related to the following areas:

- Main Street
- Dinkel Avenue
- Oakwood Drive
- Mount Crawford Avenue
- Turner Ashby Drive (The Glen at Cook's Creek)
- Volunteer Drive (Bridgewater Fields)
- Health Center Drive (B-257 Plaza)
- Commercial site adjacent to Turner Ashby High School

MAIN STREET

Main Street remains relatively efficient at carrying vehicles through town, but this efficiency creates difficulty for vehicles:

- 1) turning onto Main Street,
- 2) turning off Main Street, and
- 3) crossing Main Street.

This difficulty crossing Main Street is even more acute for bicyclists and pedestrians. The efficient flow of cars and trucks along Main Street can create a virtual wall separating the east and west sides of town. This hassle crossing Main Street creates severe challenges for students (or teachers) wishing to bike or walk to John Wayland Elementary School or Turner Ashby High School on North Main Street. The traffic volume and composition can also cause significant apprehension for pedestrians who simply want to walk along Main Street.

Additionally, the significant number of non-signalized intersections along a 0.24 mile stretch of Main Street likely contributes to the larger number of traffic incidents reported in this area. The segment is noted in VDOT Potential for Safety Improvement as a 2016-2020 top potential safety improvement segment with a total of 14 crashes reported (see Figure 5-5). The Town proposes to address this concern with the Mount Washington Connector to be discussed later in this plan.

At times, even the north-south efficiency of Main Street is challenged. Traffic delays are not atypical at times when public schools convene or dismiss students. Similar delays

occur when large employers change shifts or when large trucks must maneuver from Main Street into industrial entrances.

Likewise, utility work in or along Main Street invariably creates delays, and major utility work could degrade Main Street for a long period of time.

NEED: Traffic flow on and onto Main Street must be considered to safely and efficiently deal with vehicular, pedestrian, and bicycle users.

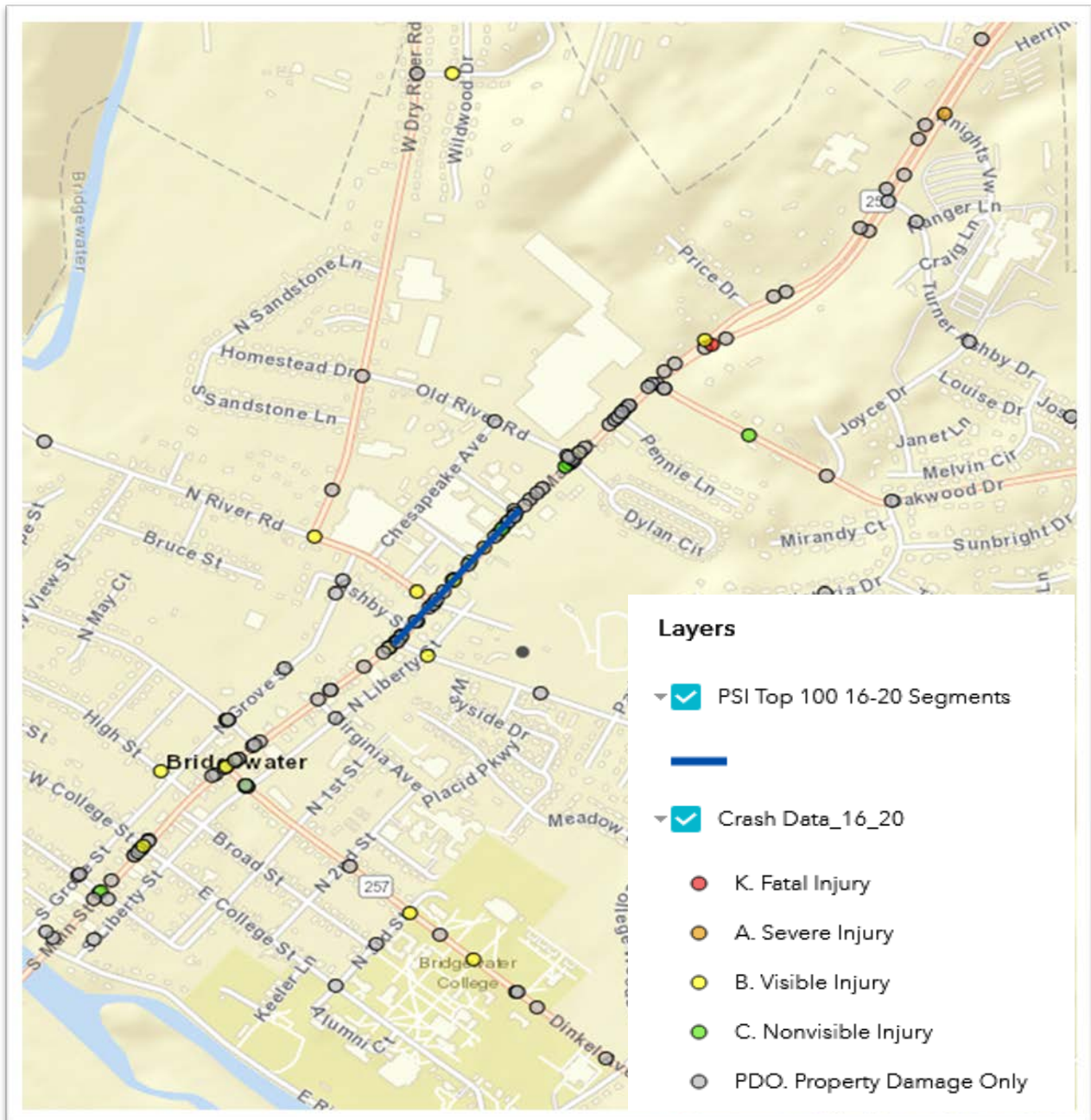


Figure 5-3. VDOT Potential Safety Improvement Segments and Intersections (2016-2020).

DINKEL AVENUE

In much the same way as Main Street, traffic along Dinkel Avenue divides Bridgewater College, creating hazards for students walking through campus. Improvements installed in the past decade, including the narrowing of Dinkel Avenue, additional lighting, and radar speed signs, have helped to create improved driver awareness of pedestrians. The Town and Bridgewater College have also worked to educate students about pedestrian safety.

NEED: Careful consideration must continue to be given to whether the five crosswalks in the college campus provide the most beneficial balance of safety and efficiency along this stretch of Dinkel Avenue.

OAKWOOD DRIVE

According to VDOT traffic data, the portion of Oakwood Drive that lies within town limits carries around 6,100 vehicles per day, with 4% of those vehicles being large trucks. Oakwood Drive serves a dual purpose:

- connecting the northern part of town to Route 11.
- serving as a central neighborhood street for various developments built in the late 20th and early 21st centuries.

These two purposes are not entirely consistent, with traffic volumes and vehicular speeds too often disrupting neighborhood harmony.

NEED: Continue to explore avenues to manage both residential and business uses along this roadway.

MT. CRAWFORD AVENUE

Mt. Crawford Avenue appears to serve as a substitute for Dinkel Avenue for westbound traffic approaching Main Street. Perhaps because much of the street has not yet been annexed, it falls below the citizens' expectations for a major "collector" street. It is extremely narrow with several blind curves.

NEED: Consider best approach to manage traffic safety.

TURNER ASHBY DRIVE

The proposed construction of the Glen at Cooks Creek residential development also includes the completion of Turner Ashby Drive. Upon completion, Turner Ashby Drive will connect with Oakwood Drive.

NEED: Improve traffic flow and connectivity with the completion of Turner Ashby Drive.

VOLUNTEER DRIVE

Construction is underway on the Bridgewater Fields subdivision, located just north of Dinkel Avenue on the east side of Bridgewater. The developer for Bridgewater Fields is constructing portions of Volunteer Drive as the different townhome phases are completed. Ultimately, Volunteer Drive will be completed, creating a connector from Don Litten Parkway to Dinkel Avenue.

NEED: Completing Volunteer Drive to create a connector road between Don Litten Parkway and Dinkel Avenue.



Figure 5-4 Construction on Bridgewater Fields began in 2020. Upon completion, the subdivision will contain 160 townhomes.

HEALTH CENTER DRIVE

To provide a safe distance between roads connecting to Dinkel Avenue, the developer of B257 Plaza was required to move and widen Health Center Drive, a private road between the property in question and the Bridgewater Sentara Health Center, to serve as a shared access road to the medical facility as well as the new businesses. This work will be completed in 2023.

NEED: As the eastern side of this commercial property develops, access will also be required off Volunteer Drive. Depending on commercial use of the remaining property, it may be necessary to widen Dinkel Avenue in this area and add a stoplight.

COMMERCIAL SITE NEXT TO TURNER ASHBY HIGH SCHOOL

Ideally, this property can be accessed through a four-way stop light along a dual carriage portion of Route 42.

NEED: Once the commercial use of this property has been determined, consideration must be given to addressing specific needs and requirements associated with the projected traffic generated.

CURRENT TRANSPORTATION PROJECTS AND RECOMMENDATIONS

Based on existing and future transportation needs, this section will present the following recommended projects, which are also depicted on Appendix 5-5:

- Mount Washington Connector
- North Main Street Crosswalk
- Main Street Pedestrian Improvement Project
- Traffic Signal Coordination
- Riverwalk
- Turner Ashby Drive Extension
- Interconnectivity

MOUNT WASHINGTON CONNECTOR



Conceptual Drawing of proposed Mt. Washington Connector

The proposed Mount Washington Connector, as shown below, will provide another critical east-west connection across Main Street, funneling traffic from the problematic North River Road intersection to a new intersection at Mt. Crawford Avenue. Depending on the final design, the connector may also tie into Ashby Street.

RECOMMENDATION: Continue opportunistically acquiring land for the Mt. Washington Connector. Begin preliminary design work for the connector and look for funding sources.

VDOT Cost Estimate: Estimated \$2.1 million (VDOT SPLCE Tool).

NORTH MAIN STREET CROSSWALK

This proposed crosswalk and pedestrian refuge will provide a safer way for pedestrians and bicyclists to cross Main Street between Turner Ashby High School and John Wayland Elementary School. The Town has designed the crosswalk and procured a contractor to construct the pedestrian refuge and sidewalk improvements, along with purchasing pedestrian crosswalk signals.

RECOMMENDATION: Complete this crosswalk in 2023 as planned, with pedestrian refuge and pedestrian crosswalk signals.

Cost: Approximately \$150,000 as part of the Town's FY2023 budget.

MAIN STREET PEDESTRIAN IMPROVEMENT PROJECT

The Main Street Pedestrian Improvement Project is a VDOT-funded HSIP project that will create or upgrade pedestrian crosswalk signals at four intersections along Main Street:

- College Street (upgrade)
- Dinkel Avenue (upgrade)
- Mt. Crawford Avenue (upgrade)
- Oakwood Drive (new installation)

RECOMMENDATION: Complete the installation or improvement of these four crosswalks in 2023.

Cost: Richardson-Wayland Electrical Company LLC was awarded the bid for the Main Street Pedestrian Improvement Project. The amount bid for construction is \$162,513.70

TRAFFIC SIGNAL COORDINATION

With the recent installation of a traffic signal at the Dylan-Old River-Main Street intersection ("DORM"), there is a growing need to coordinate all the signals on Main Street. Coordination of the lights may create efficiencies which allow for better traffic flow *and* pedestrian safety. The Town is planning to upgrade all five existing traffic control cabinets by 2024, using an Econolite traffic control system. The first of these upgrades will be completed in 2023 and the remainder upgraded in 2024. (The new intersection at DORM was installed with the newer Econolite traffic cabinet.)

Upon completion of these traffic cabinet upgrades, the Town will utilize new technology to better coordinate traffic flow along Main Street.

RECOMMENDATIONS:

- 1) Upgrade traffic control cabinets at the intersections of Turner Ashby Drive, Oakwood Drive, and Mt. Crawford Avenue in 2023.
- 2) Upgrade traffic control cabinets at the intersections of Dinkel Avenue and College Street by 2024.
- 3) Coordinate all of the six traffic signals for better traffic flow and management by 2024.

Cost: The initial purchase of traffic control cabinets and equipment in 2023 is \$150,000. An additional \$100,000 is likely in 2024.

RIVERWALK

The masterplan for Riverwalk will connect Wildwood Park on the west end of town with Sandy Bottom Park along Bridgewater's eastern border, creating a pedestrian and bicycle connector approximately two miles in length (see Appendix 5-6). As stated previously in this module, Phase I was completed in 2021. Currently, funding and easements have been obtained for Riverwalk Phase II. Engineering and design work is underway for Phase III.

RECOMMENDATION: Begin Phase 2 of Riverwalk in 2023. (Also, see Goals section)

Cost: Construction of Phase I was bid at \$275,714; Phase II will bid in mid-2023. Phase III will likely bid in 2025. Phase I was fully funded through an HSIP grant; Phase II (\$352,290, SYIP) and III (\$480,211, SYIP) are each Transportation Alternatives grants, with 80% VDOT funding secured.

TURNER ASHBY DRIVE EXTENSION

The developer of The Glen at Cooks Creek will extend Turner Ashby Drive to connect it to Oakwood Drive. According to the 2022-issued special use permit for this planned unit development, the extension will be completed by February 2026 or by such time as the 60th dwelling is sold, whichever is earlier.



Figure 5-6. The proposed Glen at Cooks Creek.

RECOMMENDATION: ENSURE THAT THIS PROJECT IS COMPLETED BY THE DEVELOPER AS NOTED ABOVE.

Cost: No cost to Town

INTERCONNECTIVITY



Figure 5-7. The only entrance and exit for Dylan Circle is off Main Street, causing traffic congestion.

Improving interconnectivity away from Main Street eases short trips and calms Main Street. Section 5-18.1 of Bridgewater's code authorizes the Town to require that new subdivisions provide connection points for vehicular traffic to adjacent properties.

A prime example is shown to the right in Figure 5-10. When Dylan Circle was developed, the Town did not request that an easement be preserved for travel to Oakdale Park. Later when we investigated an exit through the park to relieve

the Dylan-Main Street intersection, dwellings stood in the way.¹

RECOMMENDATION: Enforce Section 5-18.1

Cost: No cost to Town

FUTURE TRANSPORTATION OBJECTIVES AND STRATEGIES

The issues presented in this section may be equally as important as the recommendations in the previous section, but either they must be realized opportunistically, or the project details need to be fleshed out a bit more before implementation. All the same, the following goals and strategies are of primary importance when considering the future of Bridgewater's transportation system:

- Sidewalk Improvements
- Main Street Flow
- Riverwalk (Future Phases)
- Expansion of Public Transportation Offerings
- Bicycle Travel
- Mt. Crawford Avenue Improvements
- Coordination with Significant Users

SIDEWALK IMPROVEMENTS

Where there is sufficient land to do so, the Town widens and improves sidewalks in conjunction with municipal projects. One such area that will likely be improved will be in the area of the Mt. Washington Connector, which conceptually will include additional and expanded sidewalks, bike lanes, and a BRITE bus stop.

In 2022, the Town received State of Good Repairs (SGR) funding to repave portions of Main Street between Green Street and Oakwood Drive. As part of this funding, the Town will likely make ADA improvements to sidewalks at the intersections of Green Street, Virginia Avenue, Fraley Lane, Quality Street, and Pennie Lane. See Appendix 5-7

STRATEGY:

- 1) Continue this program. As projects allow, expand Main Street sidewalks from five feet to at least eight feet in width. Remove or relocate obstacles

¹ The Dylan Circle example also explains why the Town should be reluctant to vacate the "paper alleys" which are scattered throughout town. Though we might not be able to see transportation utility in such an alley today, it might be evident tomorrow.

to pedestrians in Main Street sidewalks. In some locations, utility poles have been placed within already narrow sidewalks.

- 2) Investigate other means of walking or biking to John Wayland, such as a path from the west.

MAIN STREET FLOW

Make Main Street safer and less hectic for vehicular and pedestrian traffic. A 2017 Main Street transportation study identified some potential areas of improvement. The study noted that traffic signal coordination could improve travel time on Main Street. The Town is upgrading traffic cabinets in 2023 and 2024 and seeks to synchronize intersections in 2024.

Reducing the number of entrances onto Main Street was also noted as a way to improve traffic flow. The DORM intersection improvements in 2022 closed six entrances on or near Main Street, and the proposed Mt. Washington Connector will close additional entrances around the Mt. Crawford Avenue intersection.

For many years, the Town has considered the construction of a bypass road connecting Dinkel Avenue to North Main Street. The goal has been to feed traffic away from Main Street, particularly to create more efficient truck access to industrial sites. While we do not believe there is an immediate need for the bypass construction, we cannot predict what future Town Councils will face, particularly if land on the east side of Town is annexed. As such, the Don Litten Parkway remains in the Town's long-term plan, and in other regional planning documents like the Harrisonburg-Rockingham Metropolitan Planning Organization's (HRMPO) 2045 Long Range Transportation Plan (LRTP).

The Town is also committed to reducing the number of overhead utilities on Main Street and will incorporate the undergrounding of said utilities whenever practicable. Other strategies presented in the 2017 Main Street study includes adjustments at existing intersections like Dinkel Avenue, where the turning radius is very narrow.

STRATEGY:

- 1) Lessen the number of entrances onto Main Street, where doing so would not unlawfully impinge upon property rights.
- 2) Continue exploring methods to relocate or bury underground utility lines since extensive utility repairs could create havoc on Main Street.
- 3) Develop plans for the ultimate replacement of Main Street's aging water and sewer lines, so that such replacement can be made as efficiently as possible.

RIVERWALK (FUTURE PHASES)

Riverwalk development must occur opportunistically because we have pledged not to condemn property rights for the path. The master plan for Riverwalk will connect Wildwood Park on the west side of Bridgewater with Sandy Bottom Park on the east side, spanning approximately two miles. Phase I is complete, Phase II will be constructed in 2023, and Phase III will likely be constructed in 2025. The Town will continue to seek opportunities to expand Riverwalk into neighborhoods that are agreeable to expansion and where the Town can connect and tie into existing pedestrian networks. Riverwalk has been listed in the HRMPO 2045 LRTP.

STRATEGY:

- 1) Continue to pursue easements and funding for future phases of Riverwalk.
- 2) Explore possibilities for spanning North River to incorporate portions of the North River islands into this walkway.

EXPANSION OF PUBLIC TRANSPORTATION OFFERINGS

Explore avenues to increase BRITE Bus ridership. The Town is committed to public transportation and looks to support the expansion of service and routes.

STRATEGY:

- 1) Continue to develop facilities such as the shelters recently installed on North Main Street and Dinkel Avenue; *and*
- 2) Coordinate with BRITE Bus to expand their existing one-way service into bi-directional travel, so that a trip to Harrisonburg does not require a southward journey to Weyers Cave.

BICYCLE TRAVEL

In recent years the Town has endeavored to increase bicycle travel options in the community. The Dry River Road project, completed in 2021, is a prime example. The addition of new bike lanes and shared-use paths will be considered in future transportation projects.

STRATEGY: Consider where bicycle lanes may be installed and otherwise create a better network of bikeable routes.

MT. CRAWFORD AVENUE IMPROVEMENTS

Improving Mt. Crawford Avenue to address its capacity and safety is a goal, but the concept presents engineering, financial, *and* political challenges. Much of the substandard portion of the street lies outside of the town's boundaries. Assuming resolution of the inherent engineering and financial issues, the Council could annex the road and upgrade it. Annexing the road, however, would create pockets of unannexed land surrounded by the town. The Council has long expressed



Figure 5-8 One possible realignment option for Mt. Crawford Avenue would connect it to Volunteer Drive.

a reluctance to annex territory against the wishes of the landowner. The idea is that Bridgewater citizenship is a privilege which should not—and need not—be foisted upon anyone. If the “town-locked” landowners oppose annexation, these principles will be tested.

Moreover, any proper improvement of the street would involve realigning the eastern end as it joins Dinkel Avenue. Figure 5-8 illustrates one possibility, in which the intersection would be replaced by rerouting Mt. Crawford Avenue to Volunteer Drive. Such realignment would involve annexing actively farmed property and severing fields. Historically, the Council has been reluctant to take such actions. Political issues may well trump the transportation issues and there may be no move toward construction.

STRATEGY: Coordinate with HRMPO on a transportation study for Dinkel Avenue, which will also look at the Dinkel-Mt. Crawford Avenue intersection. This will assist in determining the best way forward.

COORDINATION WITH SIGNIFICANT USERS

The Town will communicate with Bridgewater industries and businesses to better understand traffic issues and incorporate those thoughts into our short- and long-term transportation plans.

STRATEGY:

- 1) Seek accommodations with industrial and retail users generating significant truck traffic for such traffic to park, load, and unload on side streets to the extent possible.
- 2) Explore with the Rockingham County School Board how bus stoppages on Main Street might be minimized.

MODULE 6: PUBLIC SERVICES

BACKGROUND

Public services and facilities help determine residents' quality of life. Public services add to residents' convenience and safety while helping make Bridgewater a desirable place to live and work. The provision of various services, especially public water and waste water systems, is a key distinction between rural and urban environments. These services promote efficient use of available land and help attract commerce and industry.

PUBLIC UTILITIES



Bridgewater's Water Treatment Plant

The Town of Bridgewater owns and operates a water treatment and distribution system located on Green Street near its intersection with Breezewood Terrace. At one time, all our drinking water was taken from the North River, treated and distributed to Bridgewater households. However, in 1996, the Town completed the drilling of a well rated at 1.5 million gallons per day (mgd). In 1998, this well was brought online. Today this water is mixed with that from the North River, treated at the water treatment facility and distributed to more than 2,000 Bridgewater

households. The current rated capacity for treatment and distribution is 1.5 mgd with an average daily demand of 0.679 mgd in 2023. The system's storage capacity is 1.53 million gallons.

In recent years, Town staff has made efforts to reduce or eliminate septic systems within the Town limits. Since February 2019, seven homes were disconnected from private septic and connected to Town sewer. Currently, 25 properties are still on private septic. Further, in 2023, water meters were upgraded from manual and touch read to radio read, significantly simplifying the data collection process, and the Town implemented a Supervisory Control and Data Acquisition (SCADA) system at the Water Treatment Plant which allows staff to control, monitor, and analyze all the components of the water treatment process.

During the past four decades, the Town has taken steps to replace undersized and inadequate water lines. More than one-half of the water distribution system has been installed since 1980. A comparison of treated versus metered flow indicates a very low

system loss of less than five percent. System pressure is good throughout the Town. There are still isolated locations within the Town where water lines are undersized or have begun to function inefficiently such as some areas along Main Street. Town officials have identified these problem areas and are making plans to address improvements.

WASTE WATER COLLECTION AND DISPOSAL

The Harrisonburg-Rockingham Regional Sewer Authority (HRRSA) operates the waste water treatment plant providing service to town residents. The rated treatment capacity of this plant is 22 mgd but this capacity is sometimes exceeded during storm events due to inflow and infiltration problems. HRRSA provides wastewater treatment services for the City of Harrisonburg, the Towns of Dayton, Bridgewater, and Mt. Crawford, and nearby areas within Rockingham County.

The Town is responsible for maintenance of the wastewater collection system. This system includes more than 70,000 feet of terra cotta, PVC, and cast-iron pipe as well as four pump stations located near Millview Estates, Bridgewater Fields, Wildwood Park, and the Bridgewater College athletic field on East Riverside Drive.

The sewer mains and manholes throughout Town were recently mapped and added to the Town's new Geographical Information System (GIS). In time, water mains, parcel details, zoning files and other data will also be uploaded. Once completed, this program will be a valuable one-stop source of geographical information for Town residents and staff.

The Town has been replacing old terra cotta waste water lines with PVC pipe as part of a comprehensive inflow/infiltration abatement plan. More than one-half of the wastewater collection system has been installed since 1980. Many of the old brick manholes have been replaced to reduce inflow and infiltration problems.

ELECTRICITY AND NATURAL GAS

Dominion Virginia Power provides electric service to most residential, commercial, and industrial customers in Bridgewater and the surrounding vicinity. A few customers are served by Shenandoah Valley Electric Cooperative. Natural gas service is available in some sections of town through Columbia Gas. Charging stations for Electric Vehicles are available at Generations Park and on the Bridgewater College Campus.

SOLID WASTE COLLECTION AND RECYCLING

The Town provides weekly curbside collection of solid waste to its residents through Waste Management for a monthly fee. Refuse is deposited in the County Landfill.

The Sandy Bottom Recycle Center was opened in 2020 and is available 4 days each week for residents to drop off #1, #2, and #5 plastics, aluminum and steel cans, paper, cardboard, plastic bags, ink cartridges, hard drives and cell phones, glass, and household batteries. It also provides secure bins for professional paper shredding.



Waste Management provides collection service for solid waste in Bridgewater.

INTERNET AND CABLE TELEVISION

Comcast and Glo Fiber provide internet and television service in the Town of Bridgewater, and Verizon provides DSL Internet service.

HEALTH FACILITIES

Sentara RMH Medical Center is located approximately seven miles away from Bridgewater. General medical and related services are available in Bridgewater from physicians, dentists, and other medical personnel. Bridgewater Health Center, on Bridgewater's eastern border, provides Sentara Physical Therapy, Lab, and Imaging services, and Carilion Family Medicine. On Main Street, Hastings Medical Clinic offers primary and urgent care for all ages. Award-winning Bridgewater Retirement Community, an elder care facility, is also located in the Town.

EDUCATIONAL FACILITIES

The Rockingham County School Board operates Turner Ashby High School and John Wayland Elementary School in Bridgewater. Wilbur S. Pence Middle School is located in Dayton. Blue Ridge Christian School, a private religious-based school, provides preschool and elementary school education in the old Bridgewater Elementary School building on Dinkel Avenue. Additionally, Connections Early Learning Center provides licensed full-day care and education for infants, toddlers, and preschoolers as well as programming and academic support for elementary-aged children before and after school and during the summer break. Bridgewater College, founded in 1880, is a private co-educational

institution providing a graduate and undergraduate program. James Madison University and Eastern Mennonite University, located in Harrisonburg, and Blue Ridge Community College in Weyers Cave, also provide post-secondary education.

LIBRARY SERVICES

North River Library provides the main library service to the Town. Bridgewater College's library is also available to the broader community, as is the Massanutten Regional Library in Harrisonburg.

POLICE AND EMERGENCY SERVICES

Bridgewater has ten full-time police officers that provide 24-hour coverage of the Town. These officers have use of eight police vehicles for patrol duties. Police offices are located on the first floor of the Bridgewater Community Center at 201 Green Street.

Volunteer fire and rescue stations are located within the municipal limits and serve both town residents and nearby Rockingham and Augusta County. The Bridgewater Volunteer Fire Department is located at 304 North Main Street. The Bridgewater Rescue Squad is located at 10 Volunteer Drive.



Bridgewater Police Department

PUBLIC RECREATION

Bridgewater owns and maintains thirteen public parks covering 100 acres. More than half of this recreational space was acquired since 1984.

Oakdale Community Park (35 acres) is the Town's largest park located at 134 Mt. Crawford Avenue. Approximately nine acres are leased to the Bridgewater Community Little League which maintains six ball fields and operates a clubhouse and concession stand. The park contains three picnic shelters and two playgrounds, an outdoor basketball court, and a foursquare court. In 2023, the Oakdale Pond was completed. This beautiful water feature helps alleviate stormwater drainage issues. The pond was stocked with small fish in early 2024 and fishing is expected to be available by mid-2025. In addition to the large number of Little League practices, games and tournaments held in the park, many Town-

sponsored events are held here each year, including a Touch-A-Truck and Police Rodeo in the spring, Summer Camp for elementary students, the ever-popular Summer's End Celebration on Labor Day, and Halloween at Oakdale Park.

Harrison Park (15 acres) is located at the corner of Green and Grove Streets. The park is home to the Doug Will Tennis Center, which houses two indoor hard surface tennis and pickleball courts, along with an outdoor basketball court and playground area. Each July, the Bridgewater Volunteer Fire Department holds its fundraising Lawn Party on public land adjacent to Harrison Park.

Generations Park (0.72 acres) features ice skating during the winter. Ice skating season begins on Thanksgiving Day and is open daily through the beginning of March. Admission and skate rental is free for all Town residents who present a B-Rec pass. A nominal fee is charged for non-residents. Skating lessons are provided by Generations Park Ice Academy, and in 2023, the Town started providing weekly Shufflecurl games at the rink. The annual Christmas Tree Lighting is held here each November. Throughout the remainder of the year, Generations Park provides pickleball courts and hosts evening concerts, including Bridgewater's Eve of Independence, which draws large crowds with a free concert from a popular local band, food trucks, and spectacular fireworks.



An afternoon at Mini-Golf at Sandy Bottom Park.

Sandy Bottom Park (18.15 acres) is home to Bridgewater's par three, nine-hole golf course, and an 18-hole miniature golf course. The park facilities also include a practice green, restrooms, a new picnic shelter, a four-square court and sand volleyball court, several picnic tables along the river, two gazebos, and ample parking. Two Town-sponsored events are hosted annually at this park: the Labor Day 5K and the Mini-Golf Leaderboard Tournament. Sandy Bottom Park is

also home to the Bridgewater recycling center addressed earlier in this module.

Wildwood Park (10 acres) is a ten-acre site adjacent to the North River on West Bank Street. A dam in the river provides a pool for fishing. This park contains picnic tables, playground equipment, restrooms, and a picnic shelter. The park provides both wooded areas and grassed open spaces for recreation and exploration. Bridgewater's Easter Egg Hunt is held here every spring. Additionally, John Wayland Elementary School 2nd grade classes visit the park toward the end of the school year to plant flowers and learn more

about the park's ecosystem and how to properly care for our natural environment and the watershed.

Wynant Park (0.83 acres) is a small neighborhood park adjacent to East College Street containing trees and grassed areas as well as playground equipment.

Edgebriar Park (0.24 acres) is located on the banks of the North River along East Riverside Drive. The park has a large well-landscaped water garden and fishing pier that is handicap accessible. Edgebriar is also a great starting point for those wishing to enjoy the Roscoe Burgess Riverwalk Trail.

Cooks Creek Arboretum (8 acres) consists of eight acres of peaceful green space located along the banks of Cooks Creek. It contains several picnic tables as well as a walking path.

The Dinkel-Harris Gateway at Warm Springs Turnpike Park (0.13 acres) is a small park which commemorates the history of Route 42, formerly known as the Harrisonburg-Warm Springs Turnpike.

Bridgeview Park (2.58 acres) includes exercise stations, a walking path, and playground equipment. It is located on West Bank Street. The Roscoe Burgess Riverwalk Trail passes through this park.

Seven Bridges Park (2.23 acres) is located next to the bridge crossing North River. It contains a gazebo, a plaque telling some of the history of Bridgewater during the Civil War, and a boat ramp for those who would like to paddle the river. Seven Bridges Park is along the Roscoe Burgess Riverwalk Trail between Edgebriar and Bridgeview Parks.

Whitelow Park (4.73 acres) along Bridgewater's northwestern border features a walking trail, small pavilion, playground equipment, a swing set, and a gaga ball pit.

Roscoe Burgess Riverwalk is a walking trail along the North River. As of summer 2024, the trail connects Edgebriar, Seven Bridges, and Bridgeview Parks. Proposed future phases will connect all Bridgewater parks along the banks of the North River, from the Sandy Bottom Golf Course to Wildwood Park, with a possible footbridge for those who wish to explore the currently inaccessible islands in the middle of the river.



Citizens enjoying Riverwalk.

Veterans Park (0.11 acres) is Bridgewater's newest park. It provides a sprinkling of green space in Bridgewater's Midtown area and a mountain of respect to our veterans who have served in one of the six branches of the U.S. military. Bridgewater residents and guests may purchase plaques honoring their loved ones who served our country. The plaques are then placed on the memorial wall that serves as a focal point of the park. All proceeds from plaque sales go to the Bridgewater VFW. Next to the wall, a landscaped area features flowers, and several trees including a Dawn Redwood. This tree, once thought to be extinct, symbolizes that those who are lost are never forgotten.

Sipe Center is a multi-purpose theater on the corner of College Street and Main Street which opened in 2019 providing Bridgewater residents and guests with a regular schedule of movies and live performances. Several times per year, residents and guests enjoy the ever-popular Mario Kart Tournaments on the big screen, and a Santa at Sipe event is held here each December. Sipe Center is also available for private rentals and serves as the venue for Bridgewater's monthly Council meeting as well as other administrative gatherings.

Both Rockingham County and Bridgewater College provide recreational programs that are available to the public during certain hours. The Ashby Recreation Association operates a private swimming pool. Bridgewater Community Little League provides baseball, softball, tee-ball, and challenger divisions at Oakdale Park. Shenandoah Valley United offers soccer programming in Rockingham County for youth players from the Central Shenandoah Valley.

PUBLIC BUILDINGS



Bridgewater Community Center in spring.

The Bridgewater Community Center, located at 201 Green Street, houses offices for the Police Department, Town Treasurer, Public Works, and Administration offices. The building also contains Arey Assembly Hall, a large community meeting room which offers space for community events as well as wedding receptions, family reunions, etc. In 2023, renovations to the assembly hall allowed for the creation of 201-B, a small office space perfect for those who work from home or need a

private location to meet with a client. The office, complete with a large desk, printer, and internet access, is free to B-Rec holders. The Town owns property on Volunteer Drive

that serves as the headquarters for the Bridgewater maintenance department and provides a shop and municipal garage for vehicles and equipment.

The Town also leases the old Town Hall building at 109 South Main Street to the Bridgewater Historical Society.

GOAL: ENSURE THAT ALL RESIDENTS CONTINUE TO RECEIVE TOP-QUALITY POLICE, FIRE, RESCUE, AND WASTE COLLECTION AND DISPOSAL SERVICES.

- Offer technical and financial assistance to volunteer fire and rescue squads as needed.
- Continue to provide 24-hour police service and maintain minimum requirements for officers.
- Continue providing collection services through Waste Management and recycling at the Sandy Bottom Recycle Center.

GOAL: PRESERVE AND ENHANCE THE NATURAL RESOURCES OF BRIDGEWATER TO BENEFIT BRIDGEWATER RESIDENTS.

OBJECTIVE: Ensure that public buildings and amenities provide adequate and attractive space to support Town departments, services, and staffing needs.

- Improve and beautify the Town shop area.
- As part of the Tree City USA program (to which Bridgewater was named in 2023):
 - Plant 1.5 trees for every tree removed.
 - Budget adequately for the care of public trees.
 - Celebrate Arbor Day each year.
- Make repairs and improvements to the old Town Hall building.
- Provide network connection between all Town facilities.
- Continue to explore opportunities for undergrounding of utilities along Main Street.

OBJECTIVE: Provide adequate community facilities and services for all neighborhoods.

- Continue to monitor demand for water services to ensure that sufficient capacity and pressure is available to meet resident needs.
- Continue to replace and upgrade deficient water lines.
- Ensure sufficient capacity is reserved with the Harrisonburg Rockingham Regional Sewer Authority.

- Encourage and consider assisting with the installation of electric charging stations as opportunities allow.

OBJECTIVE: Expand recreational activities.

- Seek out funding resources available to the recreation program through grants and other opportunities.
- Continue to connect Town parks to each other through pedestrian and bicycle trails, like the Roscoe Burgess Riverwalk Trail and Gen-Oak Connector
- Continue to reserve land within the floodplain for open space.
- Continue to provide events and spaces that build a strong sense of community in a beautiful environment.
- Explore construction of a Convocation Hall at Harrison Park.

OBJECTIVE: Incorporate public services and community facilities into the Featured Capital Improvements.

- Identify and prioritize areas of need related to existing and proposed community facilities and infrastructure for funding through the Capital Improvements Plan.

OBJECTIVE: Match program costs with revenues.

- Monitor costs of public services and ensure that fees and other assessments are adequate to provide for present and future needs.

MODULE 7: LAND USE

BACKGROUND

Land use patterns in a community contribute significantly toward defining the character of the community and the quality of life enjoyed by the residents.

Historically, Bridgewater has had an insufficient residential base to support a large commercial center. Most large commercial enterprises have tended to locate in Harrisonburg, which has long served as the urban center for Rockingham County. Recent residential growth, much of it experienced in the 1990's, has increased the population to more than 6,500 residents. This population should be large enough to support an increased commercial sector.



Aerial drone photography of Bridgewater by Clark Ops, with permission.

There is also limited land available for industrial and employment growth within the existing Town boundary. Two locations have been identified within the annexation perimeter, which appear suitable based upon topography and transportation access. The first is the area surrounding the proposed intersection of existing Route 257 and Don Litten Parkway. The second area is further north where an extension of Oakwood Drive (Route 704) is projected to intersect with the Route 257 bypass adjacent to Cooks Creek. Both of these locations offer easy access to the major road network and the topography is suitable for the construction of large buildings. All the land in the annexation area, with the exception of a few scattered residential lots, is now in active agricultural use. The land within the annexation area which immediately surrounds the proposed location of the Route 257 bypass constitutes a designated growth area which has been found to meet the intent of the Code of Virginia, section 15.2-2223.1.

FUTURE LAND USE

The Town's Annexation perimeter (see Appendix 7-1) was established to define the land adjacent to Town which may be annexed into the Town at some point in the future. Under the Town's Zoning Ordinance, land annexed into Town is initially zoned agricultural, but may be assigned a higher density use if deemed appropriate by Council.

The Town's Future Land Use Map shows what districts could be created from yet-to-be-annexed land outside of the Town limits. These designations are proposed and may change as the Town's population and economy change in the coming years.

GOAL: MAINTAIN AND IMPROVE QUALITY OF LIFE FOR ALL BRIDGEWATER RESIDENTS THROUGH RESPONSIBLE LAND USE POLICIES.

OBJECTIVE: Encourage efficient and orderly land development patterns which integrate residential, commercial, industrial, institutional and public use in the Town.

- Promote Comprehensive Plan through the Zoning Code.
- Promote Traditional Neighborhood Developments in existing and future developments.
- Encourage Land Development patterns which are consistent with Transportation plan, community facilities, and planned infrastructure improvements.
- Promote a diversity of uses which balance the tax base with the need for services.
- Reduce conflicts between incompatible adjacent uses.

OBJECTIVE: Maintain and enhance visual appeal and existing Town character.

- Encourage compatible building and site design for new development.
- Encourage creative urban design through flexible design standards and incentives.
- Improve key areas of Town-maintained space such as major entrances to Town, central business districts, and other high-traffic areas.

OBJECTIVE: Preserve and enhance historic and cultural resources.



One of two new signs placed at Town entrances.

- Identify important historic locations and features.
- Continue to maintain archives of historic documents and photos.
- Promote restoration of historic structures through incentives and by directing property owners to potential restoration resources.
- Organize and encourage events and activities which highlight Town history, such as Bridgewater's annual Charter Day celebration.
- Cooperate with and support the Bridgewater Historical Society and similar organizations.

OBJECTIVE: Coordinate land use decisions in the annexation area with County officials.

- Identify adjacent County land areas within annexation perimeter which are suitable for residential or commercial growth and seek to coordinate planning with County officials.
- Seek compatibility regarding infrastructure planning in nearby areas adjacent to Town.
- Oppose and inhibit nearby developments which draw on Bridgewater facilities and service but bring no value to Town citizens.

MODULE 8: FEATURED CAPITAL IMPROVEMENTS

OVERVIEW

This list of capital projects helps guide the Town Council towards the goal of meeting community needs. Based on surveys of citizens, board members, and staff, the following projects have been identified.

RIVERWALK

Riverwalk is a walking path along Bridgewater's southern boundary. When completed, it could connect Sandy Bottom Par-3 and Mini-Golf on our southeastern border with Wildwood Park on our southwestern border. Phase two, which extends the path through Bridgeview Park to West Bank Street, was completed in the spring of 2024. The path will continue eastward along East Riverside Drive in future phases three and four, possibly extending to the islands in the river.



MIDTOWN REDEVELOPMENT

The Midtown Connector will provide a badly needed east-west link across Main Street and funnel traffic to enter Main at the Mt. Crawford Avenue stoplight. Additionally, the Town has taken measures to revitalize this area. Veterans Park opened at the end of Ashby Street in 2023, a new landscaped area was installed between 427 and 429 North Main Street in 2024, and the Town has offered attractive uniform signage to new businesses in the area. Exploring ways to increase green space, add beauty, and restore the character of this area will continue to be a part of the Midtown Redevelopment project.

MAIN STREET REDEVELOPMENT

Improving the commercial and visual appeal of our Main Street has been a long-time initiative in Bridgewater. Our Neighborhood Districts solve some of the logistical complications related to zoning requirements for current and future business owners. Our two public facilities, Generations Park and Sipe Center, are and will continue to be

important hubs of activity. Continuing to invest in measures like these will prove to be the best way to improve Main Street to a premier cultural hotspot.

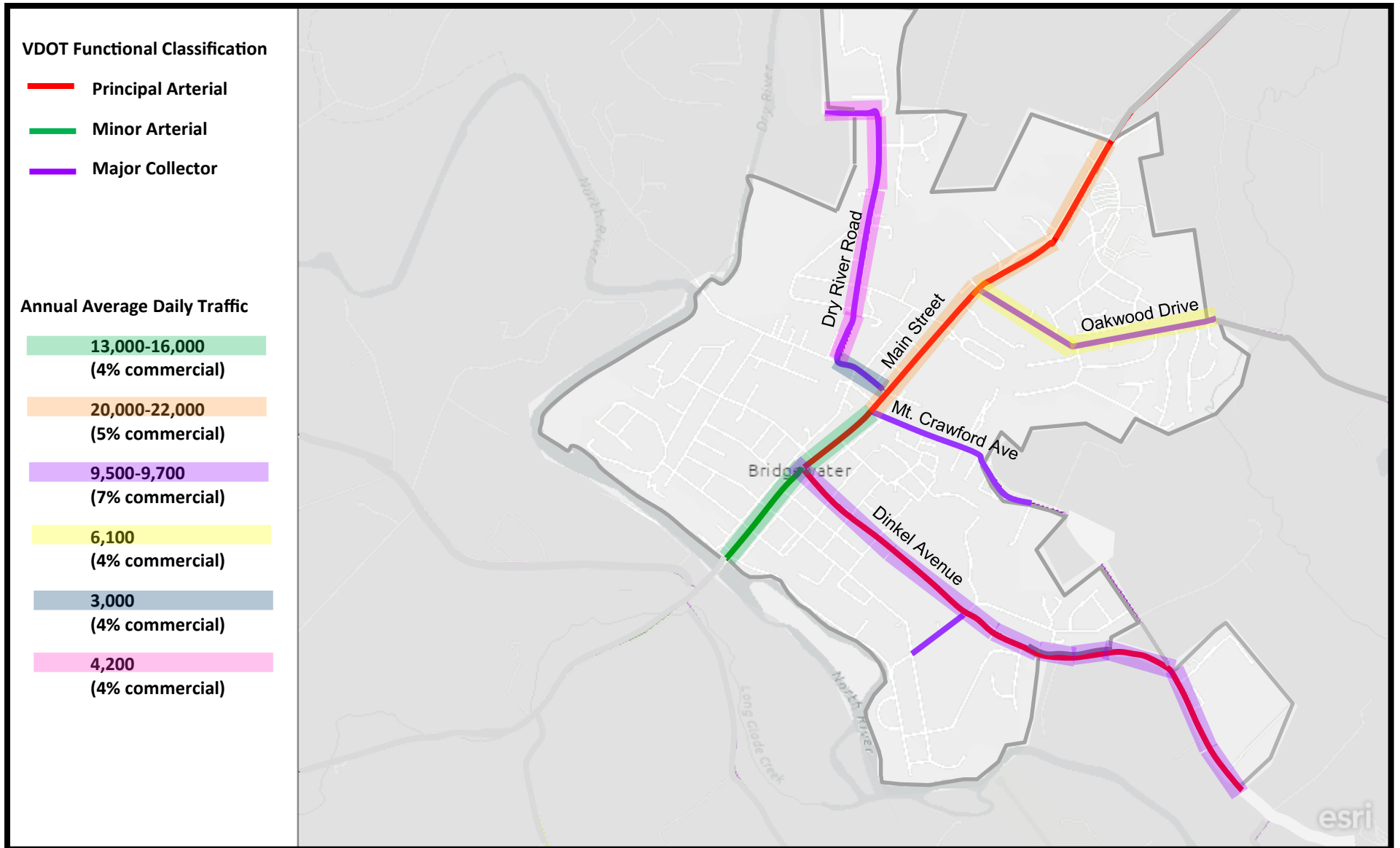
DON LITTEN PARKWAY

While there are no immediate plans to construct the parkway, connecting Route 257 with Route 42 (Dinkel Avenue and Main Street) could reduce the amount of industrial traffic on Main Street.

CONVOCATION HALL

A future Convocation Hall adjacent to the Doug Will Tennis Center at Harrison Park will provide an indoor venue for meetings, trade shows, and festivals in Bridgewater. When it's not being used for scheduled events, the indoor basketball courts and walking path will be popular among residents and guests.

VDOT Functional Classifications and Annual Average Daily Traffic (2021)



Map of VDOT's Functional Classification GIS data. Updated as of 2022.

0.4mi

BRIDGEWATER STREETS PAVING RECORD

YEAR	STREET	FROM	To	TYPE	NOTES
Years before 2016 are not complete lists					
2012	West College Street		Entire Street		
2012	Mirandy Court		Entire Street		
2015	Green Street	Grove	West View		
2015	McGuffin Avenue		Entire Street		
2015	Dinkel Avenue	Clg. View	Hickory		
2015	Main Street	Virginia Ave.	Quality		
2016	Breezewood Terrace		Entire Street		
2016	East College Street	Liberty	3rd Street		
2016	East Riverside Drive	Main	College View		
2016	Tiffany		Entire Street		
2016	High Street	McGuffin	Grove		
2016	Joyce Drive	Turner Ashby	Louise Dr.		
2016	Louise Drive		Entire Street		
2016	North Main Street	Corp. Limits	Crossover south of stoplight at Turner Ashby Drive		
2016	North River Road	Chesapeake	Bridge		
2016	Nover Drive		Just Circle of St.		
2016	Melvin Circle		Entire Street		
2016	Pope Street	Green Street	West Bank		
2016	West Bank	Pope	Dice		
2016	West Avenue		Entire Street		
2016	West Riverside	Grove	End		
2017	Oakwood Drive	Dentist Office	Town Limits		
2017	Fraley Lane		Entire Street		
2017	North Main	Green Street	Virginia Avenue		
2017	North Main North Bound	Knights View	Town Limits		
2017	West Bank Street	Main Street	Dice		
2017	Ashby Street		Entire Street		
2017	Quality Street		Entire Street		
2017	Dry River Road	North River Road	S. Sandstone Ln.		
2017	Liberty Street				
2017	Mountain View Drive	Hickory Lane	just south of retaining wall		
2017	East College Street	College View	Third Street		
2017	Sycamore Court		Entire Street		
2017	Price Drive		Entire Street		
2017	Sandy Bottom	East Riverside Dr.	E. College St.		
2018	Dinkel Avenue	Hickory Lane	Mt. Crawford Ave.	Overlay	
2018		RMH Sentara	Town Limits		
2018	Don Litten Parkway	Dinkel	Volunteer Dr.	Overlay	
2018	Dogwood Drive		Entire Street	Overlay	
2018	Hickory Lane		Entire Street	Overlay	
2018	Holly Hill Drive		Entire Street	Overlay	
2018	Joseph Court		Entire Street	Overlay	
2018	Sunbright Drive		Entire Street	Overlay	
2018	Bridgeport Drive		Portions	Overlay	
2018	Hillcrest Drive		Entire Street	Overlay	
2018	Green Street	Barbee St.	Hillcrest Drive	Full Mill	
2019	Main South Bound Right Lane	Near TA Stoplight	Oakwood Drive		
2019	Main North Bound R&L	Knights View Drive	Oakwood Drive		

2019	Dinkel Avenue	Short Section in front of Bridgewater Healthcare			
2019	Bruce Street	Entire Street		Overlay	
2019	Homestead	Entire Street			
2019	North West View Street	North River Road	Green Street	Full Mill	
2020	Pennie Layne	Entire Street		Overlay	
2020	Richard Court	Entire Street		Full Mill	
2020	Weeping Willow	Entire Street		Overlay	
2020	Oakwood Drive	Main Street	Dentist Office	Full Mill	
2020	Grove Street	Green Street	High Street	Full Mill	
2020	First Street	Dinkel Avenue	Virginia Avenue	Full Mill	
2020	Amber Lane	Entire Street		Overlay	
2020	Milestone Court	Entire Street		Overlay	
2020	Mountain View Drive	E. College St.	Beginning of Retain Wall	Full Mill	
2021	College View Drive	Dinkel	Leavel Ct.	Overlay	
2021	Main Street	North River Bridge	Green St.	Full Mill	
2021	E. College Street	505 East College Street		N/A	
2021	Recycle Center	Parking Lot		N/A	
2021	West View Street	Green	Bank Street	Full Mill	
2021	Dinkel Avenue	Third Street	College View Drive	Full Mill	Median & Crosswalks restamped
2021	Wildwood Drive	Cannery Woods Dr	North Cul-de-sac	Full Mill	
2021	Dry River Road	Homestead Dr	Cannery Woods	Full Mill	Part of sidewalk improvements project
2021	Dry River Road	Cannery Woods Dr	Millview Drive	Full Mill	
2021	Cannery Woods	Wildwood Dr	Dry River Road	Full Mill	
2021	Dry River Road	Homestead Dr	S. Sandstone Ln	Full Mill	
2021	Dry River Road	Homestead Dr	Cannery Woods	Full Mill	
2022	North Main Street	Intersection of Old River Road & Dylan Circle			
2022	Second Street	Virginia Avenue	Dinkel Avenue	Full Mill	
2022	Third Street	East College St.	Dinkel Avenue	Full Mill	
2022	Wynant Place	Alley on west side of Wynant Park		Overlay	
2022	Old River Road	Dry River Road	Chesapeake	Full Mill	
2022	North Pope Street	Green Street	Barbee Circle	Full Mill	
2022	Dinkel Avenue	Sentara	Russian Church	Full Mill	East bound travel lane only

Date: 7/7/2022
 Time: 8:22:25AM

(176) Town of Bridgewater		Planning District: (06) Central Shenandoah		Construction District: (08) Staunton									
S E Q U E N C E	ROUTE(S)	FACILITY NAME	FROM	TO	F U N C T I O N A L C L A S S	F U N C T I O N A L C L A S S	L E N G T H X.XX MILE	R I V E R W I D T H FT	P A V E M E N T W I D T H FT	T Y P E O P E R	# T L	PEAK HOUR	MOVING LANE MILES
101000		AMBER LA	TIFFANY DR	MILESTONE CT	LOC	LOS	0.09	50	35	2	2	0.18	
310000		ASHBY ST	GROVE ST	MAIN ST	LOC	LOS	0.11	40	20	2	2	0.22	
101000		BANK ST E	WYNANT PL	MAIN ST S	LOC	LOS	0.11		17	2	2	0.22	
201000		BANK ST W	MAIN ST S	GROVE ST S	LOC	LOS	0.03		23	2	2	0.06	
202000		BANK ST W	GROVE ST S	DICE ST	LOC	LOS	0.16		22	2	2	0.32	
203000		BANK ST W	DICE ST	POPE ST S	LOC	LOS	0.15		22	2	2	0.30	
204000		BANK ST W	S POPE ST	WILDWOOD PARK ENTRANCE	LOC	LOS	0.29	50	20	2	2	0.58	
301000		BARBEE CIR	GREEN ST	POPE ST	LOC	LOS	0.19		29	2	2	0.38	
302000		BARBEE CIR	POPE ST	CUL DE SAC	LOC	LOS	0.04		36	2	2	0.08	
1000		BREEZEWOOD TER	GREEN ST	CUL DE SAC	LOC	LOS	0.10	0	20	2	2	0.20	
165000		BRIAN AVE	MT CRAWFORD AVE	0.06 S MT CRAWFORD AVE	LOC	LOS	0.06	50	20	2	2	0.12	
166000		BRIAN AVE	0.06 S MT CRAWFORD AVE	CUL DE SAC	LOC	LOS	0.03	50	20	2	2	0.06	
1000		BRIDGEPORT DR	TIFFANY DR	0.11 N TIFFANY DR	LOC	LOS	0.11	50	34	2	2	0.22	
2000		BRIDGEPORT DR	0.11 N TIFFANY DR	OAKWOOD DR	LOC	LOS	0.22	50	34	2	2	0.44	
400500		BROAD ST	MAIN ST	LIBERTY ST	LOC	LOS	0.03	50	35	2	2	0.06	
401000		BROAD ST	LIBERTY ST N	SECOND ST N	LOC	LOS	0.17		20	2	2	0.34	
402000		BROAD ST	SECOND ST N	THIRD ST N	LOC	LOS	0.11		27	2	2	0.22	
1000		BRUCE ST	VIEW ST W	DEAD END	LOC	LOS	0.22	0	20	2	2	0.44	
51000		CANNERY WOODS DR	DRY RIVER RD	NCL	LOC	LOS	0.17	50	30	2	2	0.34	
101000		CHESAPEAKE AVE	RIVER RD N	DEPOT ST	LOC	LOS	0.17		24	2	2	0.34	
102000		CHESAPEAKE AVE	DEPOT ST	CW RXR	LOC	LOS	0.03		19	2	2	0.06	
103000		CHESAPEAKE AVE	CW RXR	OLD RIVER RD	LOC	LOS	0.05		19	2	2	0.10	
1000		CHESTERS WAY	OAKWOOD DR	CUL DE SAC	LOC	LOS	0.11	50	30	2	2	0.22	
104000		CINDIE LA	MT CRAWFORD AVE	DEAD END	LOC	LOS	0.07	50	20	2	2	0.14	
100500		COLLEGE ST E	RIVERSIDE DR E	MOUNTAIN VIEW DR	LOC	LOS	0.09	50	18	2	2	0.18	
201000		COLLEGE ST E	MOUNTAIN VIEW DR	COLLEGE VIEW DR	LOC	LOS	0.25		20	2	2	0.50	
202000		COLLEGE ST E	COLLEGE VIEW DR	THIRD ST N	LOC	LOS	0.35	0	26	2	2	0.70	
203000		COLLEGE ST E	THIRD ST N	MAIN ST S	LOC	LOS	0.33		25	2	2	0.66	
75250		COLLEGE ST W	MAIN ST N	GROVE ST S	LOC	LOS	0.03	0	32	2	2	0.06	
150500		COLLEGE ST W	GROVE ST S	0.10 W GROVE ST S	LOC	LOS	0.10	0	23	2	2	0.20	

(176) Town of Bridgewater			Planning District: (06) Central Shenandoah			Construction District: (08) Staunton							
S E Q U E N C E	ROUTE(S)	FACILITY NAME	FROM	TO	F U N C T I O N A L C L A S S	F U N C T I O N A L C L A S S	L E N G T H X.XX MILE	R I G H T W I D T H FT	P A V E M E N T W I D T H FT	T Y P E O P E R	# T L	P E A K H O U R	M O V I N G L A N E M I L E S
225750		COLLEGE ST W	0.10 W GROVE ST S	DICE ST	LOC	LOS	0.07	0	25	2	2	0.14	
304000		COLLEGE ST W		DICE ST	LOC	LOS	0.11		25	2	2	0.22	
400000		COLLEGE VIEW DR		RIVERSIDE DR	LOC	LOS	0.11		19	2	2	0.22	
401000		COLLEGE VIEW DR		COLLEGE ST E	COL	COS	0.21		22	2	2	0.42	
401750		COLLEGE VIEW DR		DINKEL AVE	LOC	LOS	0.30	50	30	2	2	0.60	
402500		COLLEGE VIEW DR		LEAVEL CT	LOC	LOS	0.26	50	40	2	2	0.52	
101000		DEPOT ST		MAIN ST N	LOC	LOS	0.05		41	2	2	0.10	
102000		DEPOT ST	0.05 E	MAIN ST N	LOC	LOS	0.06		30	2	2	0.12	
151000	SR 257	DINKEL AVE		MAIN ST	UPA	ARP	0.12	52	44	2	2	0.24	
152000	SR 257	DINKEL AVE		FIRST ST	UPA	ARP	0.80	56	44	2	2	1.60	
153000		DINKEL AVE	0.80 S	FIRST ST	UPA	ARP	1.64 S	56	44	2	2	1.68	
201000		DOGWOOD DR		HICKORY LA	LOC	LOS	0.12		32	2	2	0.24	
1000		DON LITTEN PKWY		DINKEL AVE	LOC	LOS	0.23	140	24	2	2	0.46	
1000		DRY RIVER RD		RIVER RD N	COL	COS	0.44	0	21	2	2	0.88	
2000		DRY RIVER RD	0.44 N	RIVER RD N	COL	COS	0.84 N	30	24	2	2	0.80	
1000		DYLAN CIR		MAIN ST N	LOC	LOS	0.06	55	36	2	2	0.12	
2000		DYLAN CIR		DYLAN CIR	LOC	LOS	0.35	50	30	2	2	0.70	
210000		E RIVERSIDE DR		MAIN ST	LOC	LOS	0.67	30	18	2	2	1.34	
101000		FIRST ST N		COLLEGE ST E	LOC	LOS	0.15		18	2	2	0.30	
102000		FIRST ST N		DINKEL AVE	LOC	LOS	0.15		16	2	2	0.30	
51500		GREEN ST		MAIN ST N	LOC	LOS	0.36	0	30	2	2	0.72	
77250		GREEN ST		VIEW ST W	LOC	LOS	0.12	0	39	2	2	0.24	
103000		GREEN ST		BARBEE CIR	LOC	LOS	0.07		20	2	2	0.14	
104000		GREEN ST		HILLCREST DR	LOC	LOS	0.10		25	2	2	0.20	
105000		GROVE ST		W RIVERSIDE DR	LOC	LOS	0.09	30	20	2	2	0.18	
106000		GROVE ST		BANK ST	LOC	LOS	0.09	30	20	2	2	0.18	
107000		GROVE ST		W COLLEGE ST	LOC	LOS	0.10	30	20	2	2	0.20	
108000		GROVE ST		HIGH ST	LOC	LOS	0.11	30	20	2	2	0.22	
109000		GROVE ST		GREEN ST	LOC	LOS	0.30	30	20	2	2	0.60	
109000		GROVE ST		GREEN ST	LOC	LOS	0.30	30	20	2	2	0.60	
1000		HICKORY LA		DINKEL AVE	LOC	LOS	0.19	0	32	2	2	0.38	
101000		HIGH ST		VIEW ST N	LOC	LOS	0.23		31	2	2	0.46	

(176) Town of Bridgewater		Planning District: (06) Central Shenandoah		Construction District: (08) Staunton										
S E Q U E N C E	ROUTE(S)	FACILITY NAME	FROM	TO	F U N C T I O N A L C L A S S	F U N C T I O N A L C L A S S	L E N G T H X.XX MILE	R I G H T W I D T H FT	P A V E M E N T T H ICK N E S S F E E T	T Y P E O P E R A T I O N	# T R A N S P O R T E D	PEAK HOUR	MOVING LANE MILES	
102000		HIGH ST	0.23 S VIEW ST N	MAIN ST N	LOC	LOS	0.10		22	2	2	0.20		
301000		HILLCREST DR		GREEN ST	LOC	LOS	0.09		38	2	2	0.18		
305000		HILLVIEW ST		HOLLY HILL DR	LOC	LOS	0.09	50	30	2	2	0.18		
1000		HOLLEN MILL CT		MILLVIEW DR	LOC	LOS	0.09	50	32	2	2	0.18		
201000		HOLLY HILL DR		POPE ST S	LOC	LOS	0.31	50	30	2	2	0.62		
402000		HOLLY HILL DR		MILLER DR	LOC	LOS	0.02	50	30	2	2	0.04		
501000		HOMESTEAD DR		DRY RIVER RD	LOC	LOS	0.19		30	2	2	0.38		
1000		JANET LA		PHYLLIS LA	LOC	LOS	0.08	50	30	2	2	0.16		
1201000		JOSEPH CT		MELVIN CIR	LOC	LOS	0.13	50	30	2	2	0.26		
1000		JOYCE DR		TURNER ASHBY DR	LOC	LOS	0.07	50	30	2	2	0.14		
2000		JOYCE DR		LOUISE DR	LOC	LOS	0.17	50	30	2	2	0.34		
1000		KLINE CT		GROVE ST S	LOC	LOS	0.05	0	19	2	2	0.10		
51000		LEAVEL CT		COLLEGE VIEW DR	LOC	LOS	0.05	50	30	2	2	0.10		
91000		LIBERTY ST		RIVERSIDE DR E	LOC	LOS	0.10	30	16	2	2	0.20		
92000		LIBERTY ST		BANK ST E	LOC	LOS	0.09	30	16	2	2	0.18		
93000		LIBERTY ST		COLLEGE ST E	LOC	LOS	0.07	30	19	2	2	0.14		
94000		LIBERTY ST		BROAD ST	LOC	LOS	0.08	30	19	2	2	0.16		
95000		LIBERTY ST		DINKEL AVE	LOC	LOS	0.15	30	20	2	2	0.30		
100000		LIBERTY ST		VIRGINIA AVE	LOC	LOS	0.15	34	22	2	2	0.30		
1000		LOUISE DR		JOYCE DR	LOC	LOS	0.14	50	30	2	2	0.28		
26000		MAIN ST	0.70 S	DINKEL AVE	0.44 S	DINKEL AVE	UMA	ARM	0.26	0	48	2	4	1.04
39000		MAIN ST	0.44 S	DINKEL AVE		DINKEL AVE	UMA	ARM	0.44	55	35	2	2	0.88
52000	SR 42	MAIN ST		DINKEL AVE		MT CRAWFORD AVE	UPA	ARP	0.28	55	32	2	2	0.56
53000	SR 42	MAIN ST		MT CRAWFORD AVE		OAKWOOD DR	UPA	ARP	0.51	60	36	2	2	1.02
54000	SR 42	MAIN ST		OAKWOOD DR	0.13 N	OAKWOOD DR	UPA	ARP	0.13	55	35	2	2	0.26
55000	SR 42	MAIN ST	0.13 N	OAKWOOD DR		CL	UPA	ARP	0.43	0	48	2	4	1.72
70000		MAY CT N		GREEN ST		CUL DE SAC	LOC	LOS	0.08	40	30	2	2	0.16
70500		MAY CT S		GREEN ST		CUL DE SAC	LOC	LOS	0.06	40	18	2	2	0.12
101000		MCGUFFIN AVE		GREEN ST		HIGH ST W	LOC	LOS	0.11		39	2	2	0.22
102000		MELISSA CT		BRIDGEPORT DR		CUL DE SAC	LOC	LOS	0.04	50	34	2	2	0.08
102400		MELVIN CIR		TURNER ASHBY DR		RICHARD CT	LOC	LOS	0.11	50	30	2	2	0.22

(176) Town of Bridgewater		Planning District: (06) Central Shenandoah			Construction District: (08) Staunton								
S E Q U E N C E	ROUTE(S)	FACILITY NAME	FROM	TO	F U N C T I O N A L C L A S S	F U N C T I O N A L C L A S S	L E N G T H X.XX MILE	R I G H T W I D T H FT	P A V E M E N T W I D T H FT	T Y P E O P E R	# T L	P E A K H O U R	M O V I N G L A N E M I L E S
103400		MELVIN CIR	RICHARD CT	0.06 W RICHARD CT	LOC	LOS	0.06	50	30	2	2	0.12	
104400		MELVIN CIR	0.06 W RICHARD CT	CUL DE SAC	LOC	LOS	0.13	50	30	2	2	0.26	
103000		MILESTONE CT	AMBER LA	CUL DE SAC	LOC	LOS	0.09	50	35	2	2	0.18	
1000		MILLER DR	HOLLY HILL DR	CUL DE SAC	LOC	LOS	0.16	50	30	2	2	0.32	
1000		MILLSTONE ST	MILLVIEW DR	PENSTOCK LA	LOC	LOS	0.16	60	32	2	2	0.32	
500		MILLVIEW DR	DRY RIVER RD	MILLSTONE ST	LOC	LOS	0.14	60	36	2	2	0.28	
1500		MILLVIEW DR	MILLSTONE ST	CUL DE SAC	LOC	LOS	0.15	50	28	2	2	0.30	
201000		MIRANDY CT	VICTORIA DR	CUL DE SAC	LOC	LOS	0.17		32	2	2	0.34	
401000		MOUNT CRAWFORD AVE	MAIN ST N	ECL	COL	COS	0.34		20	2	2	0.68	
301000		MOUNTAIN VIEW DR	RIVERSIDE DR	COLLEGE ST E	LOC	LOS	0.07		14	2	2	0.14	
1000		MT VIEW DRIVE	HICKORY LA	EAST COLLEGE ST	LOC	LOS	0.44	40	24	2	2	0.88	
501000		NORTHSHORE LA	RIVERSIDE DR E	RIVERSIDE DR E	LOC	LOS	0.19	50	30	2	2	0.38	
1000		NOVER ST	W VIEW ST	CUL DE SAC	LOC	LOS	0.15	40	28	2	2	0.30	
51000		OAK MEADOW DR	COLLEGE VIEW DR	CUL DE SAC	LOC	LOS	0.19	50	30	2	2	0.38	
100500		OAKWOOD DR	MAIN ST N	ECL	COL	COS	0.78		16	2	2	1.56	
101000		OLD RIVER RD	DRY RIVER RD	CW RXR	LOC	LOS	0.17		28	2	2	0.34	
102000		OLD RIVER RD	CW RXR	MAIN ST N	LOC	LOS	0.13		28	2	2	0.26	
1000		PARKSIDE DR	MT CRAWFORD AVE	0.05 N MT CRAWFORD AVE	LOC	LOS	0.05	50	34	2	2	0.10	
1000		PENNIE LAYNE	N MAIN ST	CUL DE SAC	LOC	LOS	0.21	50	30	2	2	0.42	
1000		PENSTOCK LA	MILLSTONE ST	TURBINE LN	LOC	LOS	0.06	50	28	2	2	0.12	
1000		PHYLLIS LA	JOYCE DR	MELVIN CIR	LOC	LOS	0.11	50	30	2	2	0.22	
1000		PLAZA DR	TURNER ASHBY DR	DEAD END	LOC	LOS	0.05	50	40	2	2	0.10	
101000		POPE ST	BANK ST W	HOLLY HILL DR	LOC	LOS	0.06		30	2	2	0.12	
102000		POPE ST	HOLLY HILL DR	GREEN ST	LOC	LOS	0.21		36	2	2	0.42	
103000		POPE ST	GREEN ST	BARBEE ST	LOC	LOS	0.14		36	2	2	0.28	
1000		PRICE DR	MAIN ST	CUL DE SAC	LOC	LOS	0.15	50	21	2	2	0.30	
101000		QUALITY ST	CHESAPEAKE AVE	MAIN ST N	LOC	LOS	0.10		30	2	2	0.20	
51000		RICHARD CT	MELVIN CIR	CUL DE SAC	LOC	LOS	0.06	50	30	2	2	0.12	
101000		RIVER RD N	MAIN ST N	DRY RIVER RD	COL	COS	0.18		27	2	2	0.36	
102000		RIVER RD N	DRY RIVER RD	VIEW ST W	LOC	LOS	0.25	0	25	2	2	0.50	

(176) Town of Bridgewater		Planning District: (06) Central Shenandoah			Construction District: (08) Staunton								
S E Q U E N C E	ROUTE(S)	FACILITY NAME	FROM	TO	F U N C T I O N A L C L A S S	F U N C T I O N A L C L A S S	L E N G T H X.XX MILE	R I G H T W I D T H FT	P A V E M E N T W I D T H FT	T Y P E O P E R	# T L	P E A K H O U R	M O V I N G L A N E M I L E S
103000		RIVER RD N	VIEW ST W	WCL	LOC	LOS	0.24	0	22	2	2	0.48	
201000		RIVERSIDE DR	0.09 E MT VIEW DR	COLLEGE VIEW DR	LOC	LOS	0.28		14	2	2	0.56	
1000		ROUND HILL DR	POPE ST S	END	LOC	LOS	0.06	40	24	2	2	0.12	
1000		SANDSTONE CIR	DRY RIVER RD	CUL DE SAC	LOC	LOS	0.08	60	30	2	2	0.16	
101000		SANDSTONE LA N	DRY RIVER RD	HOMESTEAD DR	LOC	LOS	0.28		37	2	2	0.56	
102000		SANDSTONE LA N	HOMESTEAD DR	SANDSTONE LA S	LOC	LOS	0.07		30	2	2	0.14	
201000		SANDSTONE LA S	SANDSTONE LA N	DRY RIVER RD	LOC	LOS	0.20		31	2	2	0.40	
1000		SECOND ST N	VIRGINIA AVE	DINKEL AVE	LOC	LOS	0.14	0	39	2	2	0.28	
2000		SECOND ST N	DINKEL AVE	COLLEGE ST E	LOC	LOS	0.14	0	15	2	2	0.28	
1000		SPRUCE LA	GROVE ST	0.03 W GROVE ST	LOC	LOS	0.03	50	26	2	2	0.06	
2000		SPRUCE LA	0.03 W GROVE ST	CUL DE SAC	LOC	LOS	0.03	50	35	2	2	0.06	
401000		STEPHEN CIR	MOUNT CRAWFORD ST	CUL DE SAC	LOC	LOS	0.09		20	2	2	0.18	
501000		SUNBRIGHT DR	VICTORIA DR	OAKWOOD DR	LOC	LOS	0.30		34	2	2	0.60	
751000		SYCAMORE CT	WEeping WILLOW LA	CUL DE SAC	LOC	LOS	0.12	50	24	2	2	0.24	
101000		THIRD ST N	COLLEGE ST E	DINKEL AVE	LOC	LOS	0.14		24	2	2	0.28	
201000		TIFFANY DR	VICTORIA DR	AMBER LN	LOC	LOS	0.20		30	2	2	0.40	
203000		TIFFANY DR	AMBER LA	BRIDGEPORT DR	LOC	LOS	0.11	50	30	2	2	0.22	
204000		TIFFANY DR	BRIDGEPORT DR	CUL DE SAC	LOC	LOS	0.07	50	30	2	2	0.14	
1000		TURBINE LN	MILLVIEW DR	CUL DE SAC	LOC	LOS	0.18	50	28	2	2	0.36	
205000		TURNER ASHBY DR	MAIN ST	MELVIN CIR	LOC	LOS	0.36	60	36	2	2	0.72	
205500		TURNER ASHBY DR	MELVIN CIR	0.03 E MELVIN CIR	LOC	LOS	0.03	60	36	2	2	0.06	
101000		VICTORIA DR	OAKWOOD DR	CUL DE SAC	LOC	LOS	0.31		36	2	2	0.62	
201000		VICTORY ST	BRUCE ST	RIVER RD N	LOC	LOS	0.08		20	2	2	0.16	
1000		VIEW ST W	BANK ST W	GREEN ST	LOC	LOS	0.27	50	30	2	2	0.54	
2000		VIEW ST W	GREEN ST	RIVER RD N	LOC	LOS	0.23	0	38	2	2	0.46	
1000		VIRGINIA AVE	MAIN ST	FIRST ST	LOC	LOS	0.10	40	30	2	2	0.20	
2000		VIRGINIA AVE	FIRST ST	SECOND ST	LOC	LOS	0.08	40	31	2	2	0.16	
3000		VIRGINIA AVE	SECOND ST	PLACID PKWY	LOC	LOS	0.04	40	31	2	2	0.08	
1000		VOLUNTEER DR	DON LITTEN PKWY	CUL DE SAC	LOC	LOS	0.07	50	24	2	2	0.14	

(176) Town of Bridgewater






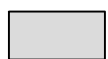


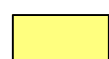

Planning District: (06) Central Shenandoah

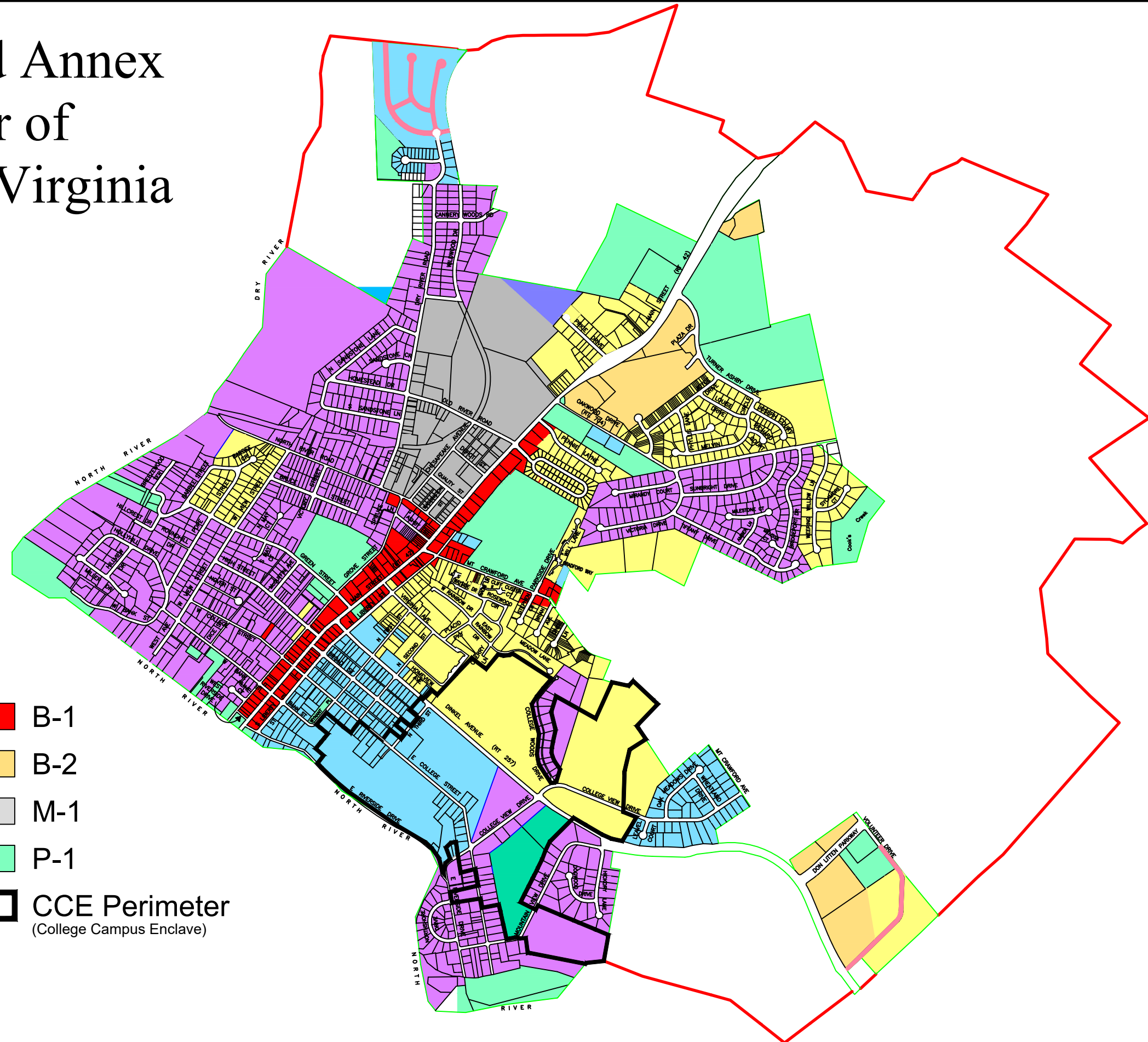
Construction District: (08) Staunton

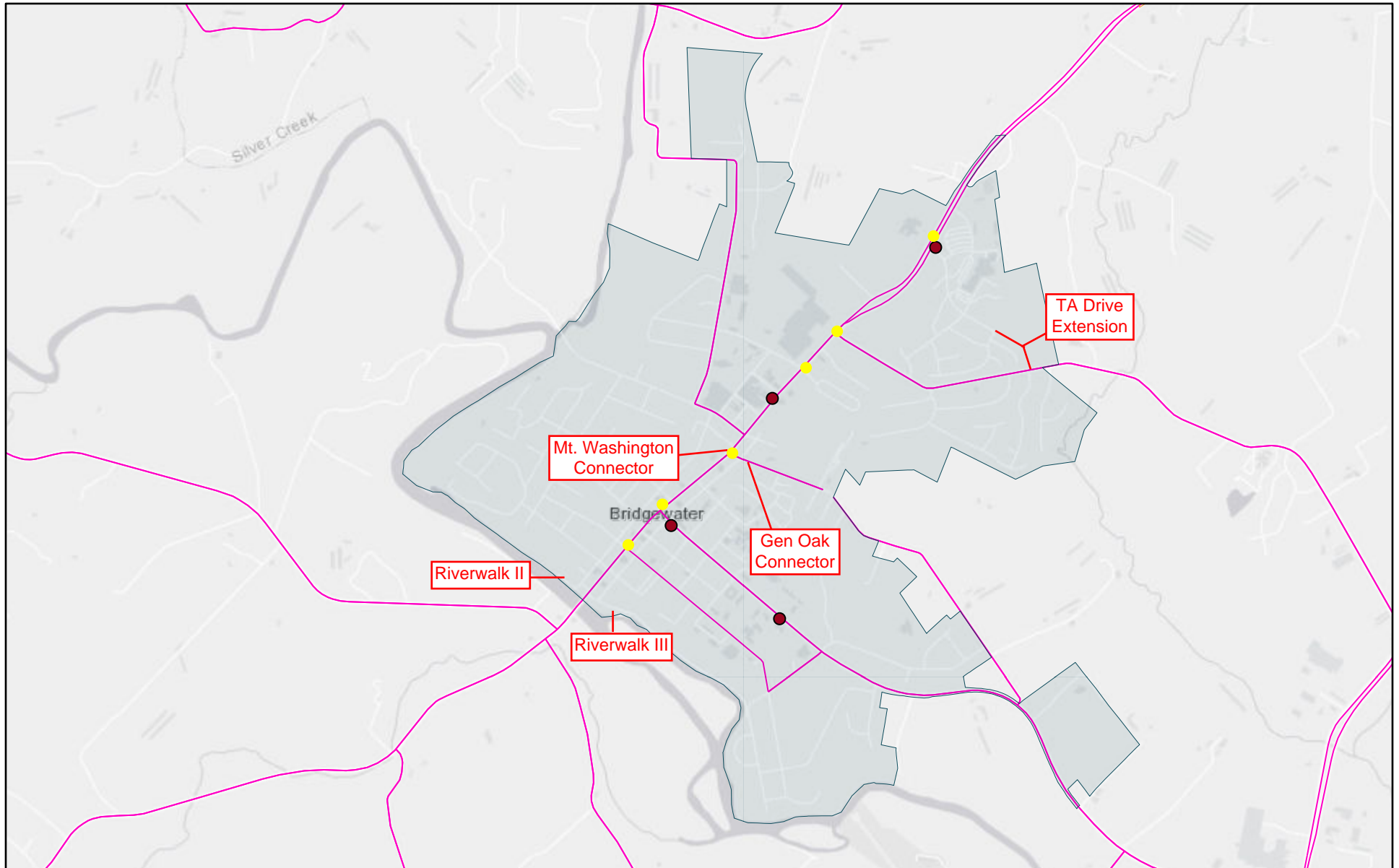
S E Q U E N C E	ROUTE(S)	FACILITY NAME	FROM	TO	F U N C T I O N A L C L A S S	F U N C T I O N A L C L A S S	L E N G T H X.XX MILE	R I D E R W I D T H FT	W I D E T H FT	T Y P E O P E R	# T L	PEAK HOUR	MOVING LANE MILES
202000		W RIVERSIDE DR	GROVE ST	MAIN ST	LOC	LOS	0.03	30	22	2	2	0.06	
101000		WASHINGTON ST	QUALITY ST	RIVER RD N	LOC	LOS	0.08		27	2	2	0.16	
111000		WEeping WILLOW LA	OAKWOOD DR	SYCAMORE CT	LOC	LOS	0.18	50	30	2	2	0.36	
111500		WEeping WILLOW LA	SYCAMORE CT	CUL DE SAC	LOC	LOS	0.11	50	30	2	2	0.22	
200300		WHEATLAND DR	COLLEGE VIEW DR	OAK MEADOW DR	LOC	LOS	0.12	50	30	2	2	0.24	
200500		WILDWOOD DR N	CANNERY WOODS DR	CUL DE SAC	LOC	LOS	0.09	50	30	2	2	0.18	
200600		WILDWOOD DR S	CANNERY WOODS DR	CUL DE SAC	LOC	LOS	0.19	50	30	2	2	0.38	
201000		WYNANT PL	COLLEGE ST E	BANK ST E	LOC	LOS	0.10		15	2	2	0.20	

Land Use and Annex Perimeter of Bridgewater, Virginia

LEGEND

	A-1		B-1
	A-2		B-2
	R-1		M-1
	R-2		P-1
	R-3		CCE Perimeter (College Campus Enclave)
	Annex Perimeter		





1/11/2023

Bus Stops Towns

● Bus Stops

■ Towns

● Crosswalk Signal and Traffic Control Upgrades

— 2021 VTrans Mid-term Needs

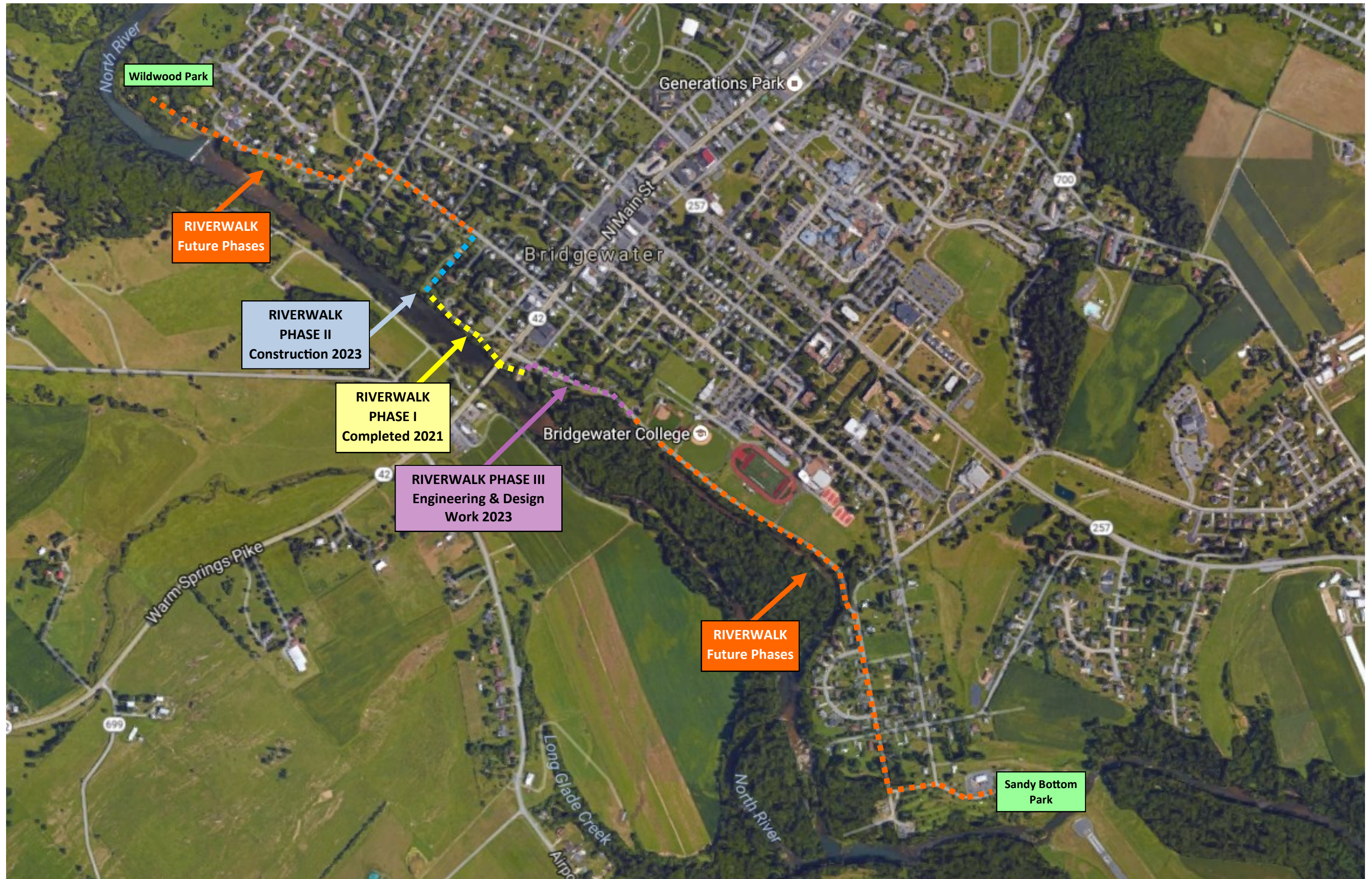
1:36,093

0 0.23 0.45 0.9 mi

0 0.38 0.75 1.5 km

Esri, HERE, VITA, West Virginia GIS, Esri, HERE, Garmin, GeoTechnologies, Inc., USGS, EPA

Generated on www.vtrans.org/interactvtrans/map-explorer on 01/11/2023

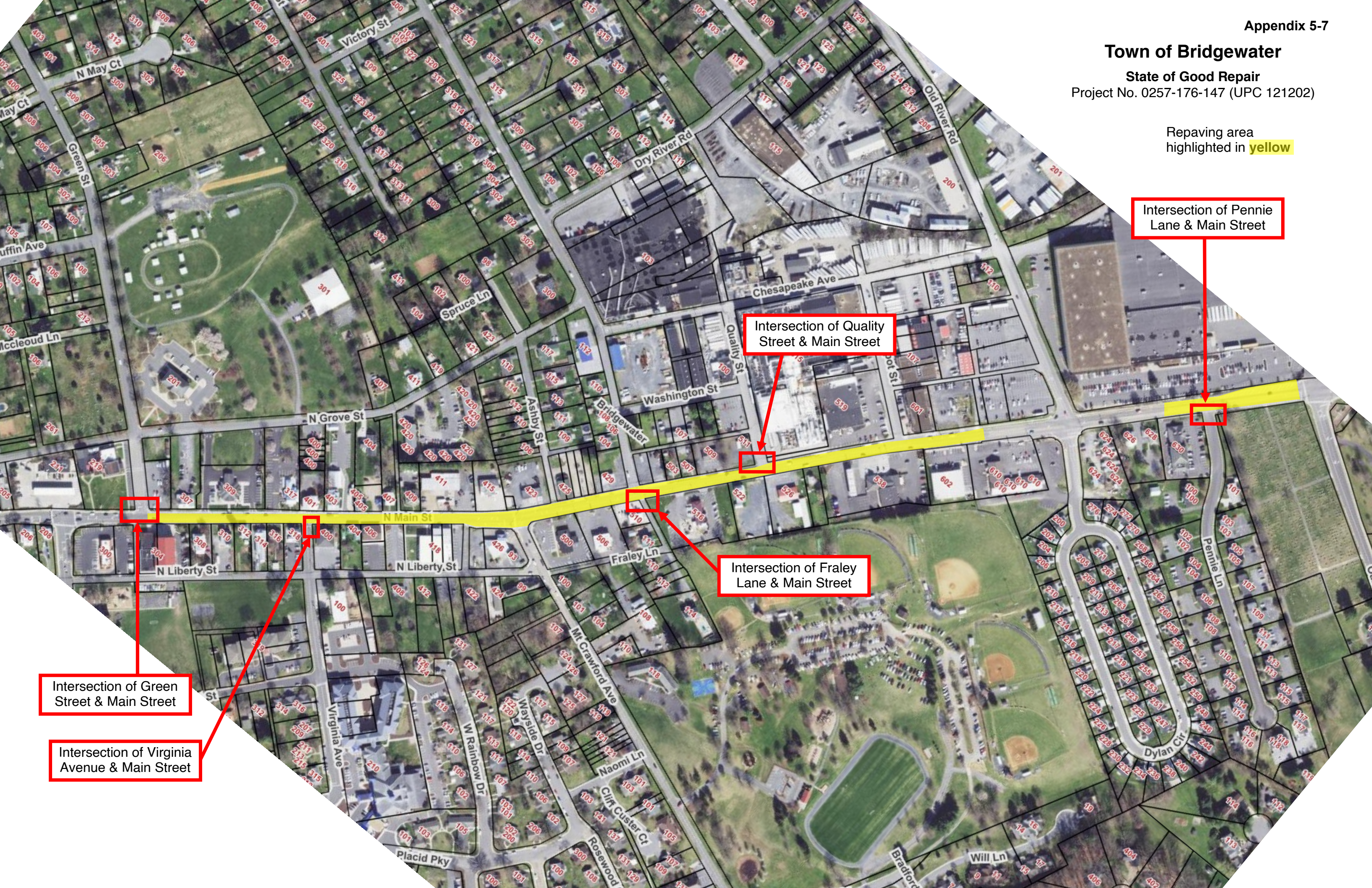


Town of Bridgewater

State of Good Repair

Project No. 0257-176-147 (UPC 121202)

Repaving area highlighted in yellow



Intersection of Pennie Lane & Main Street

Intersection of Quality Street & Main Street

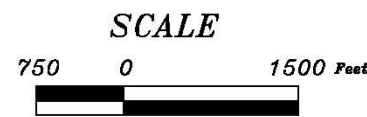
Intersection of Fraley Lane & Main Street

Intersection of Green Street & Main Street

Intersection of Virginia Avenue & Main Street

Proposed Land Use

in the Annexation Perimeter
of the
Town of Bridgewater, Virginia



LEGEND

- R-1 Single Family Residential
- R-2 Conditional (Single Family or Duplex)
- R-3 Multi-family
- C/I Commercial/Industrial
- P-1 Green Space
- Current Town Limits

