



OCTOBER 2019

THE DOWNTOWN BELLFLOWER TRANSIT ORIENTED DEVELOPMENT SPECIFIC PLAN (TOD SP)

ACKNOWLEDGMENTS

CITY COUNCIL

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Elizabeth Corpuz, *Director of Planning and Building Services*

Rowena Genilo-Concepcion, *Planning Manager*

Jason P. Clarke, *Senior Planner*

CITY CONSULTANTS

Duane Morita, *Planning and Environmental*

Gerald J. Stock, PE, TE, *Hartzog & Crabill, Inc.*

CONSULTANT TEAM

PlaceWorks

Karen Gulley, *Principal*

Cat Callaghan, AIA, *Associate Planner/Designer*

Steve Gunnells, *Chief Economist*

Fernando Sotelo, PE, PTP, INCE, *Senior Associate*

Fuscoe Engineering

Ian Adam, *Principal*

Stephanie Castle Zinn, *Senior Water Resources Specialist*

Alex Bennett, *Stormwater Specialist*

Michael Serrano, *GIS Analyst*

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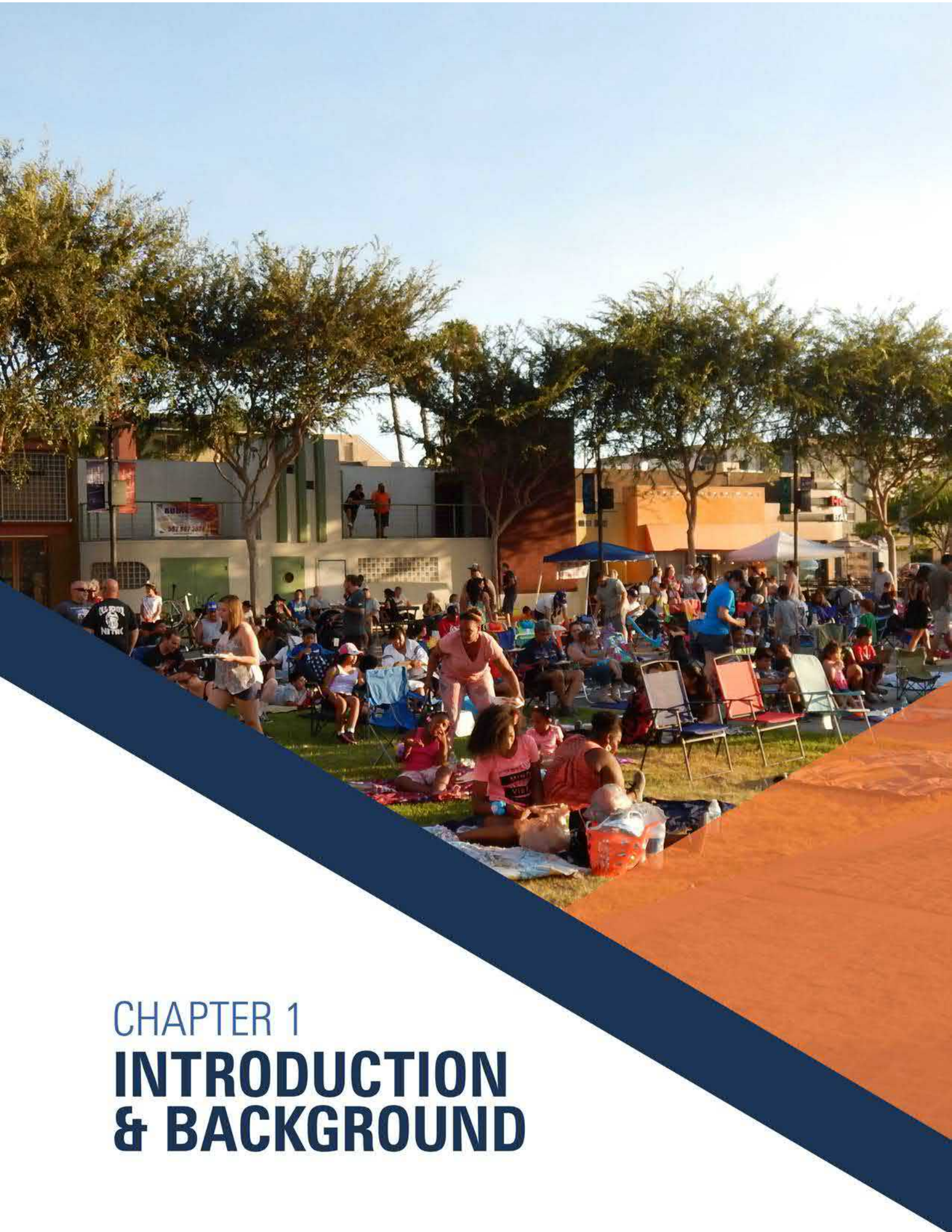
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CHAPTER 1

INTRODUCTION & BACKGROUND

1. INTRODUCTION AND BACKGROUND

1.1 OVERVIEW

The Downtown Bellflower Station Area Specific Plan provides an exciting opportunity to align the City's long-pursued goals for the downtown area with the potential benefits of the anticipated rail transit corridor—the West Santa Ana Branch Transit Corridor. This light-rail line will connect Bellflower and surrounding cities to Downtown Los Angeles to the north and Artesia to the south, and ultimately, to stations in Orange County. Bellflower's proximity to this transit line will provide opportunities to attract new development, encourage multimodal transportation, improve access to major employment centers, and reintroduce Downtown Bellflower as a thriving, experience-oriented district for residents and visitors.

Although the opening of the transit station is years away, the market is ready today to start embracing future transit and to revitalize Bellflower's charming, walkable downtown that embraces its history while valuing its modern amenities. By introducing a transit station in the downtown core, Bellflower will create a setting for new high-quality residences and a range of new commercial and entertainment activity that will support and sustain a thriving downtown environment.

This Specific Plan will be used in conjunction with the Bellflower General Plan to provide more detailed design and development criteria for individual project proposals and public improvements in the project area. The Specific Plan defines an updated land use plan, development standards, mobility plan, infrastructure improvements, design guidelines, and implementation programs for any proposed project in the Specific Plan area.

1.2 REGIONAL FOCUS ON TRANSIT-ORIENTED DEVELOPMENT

Eco-Rapid Transit Corridor is an area extending 40 miles from the City of Artesia to Hollywood Burbank Airport created to pursue development of a transit system that moves as rapidly as possible, uses grade separation as appropriate, and is environmentally friendly and energy efficient. Its development is overseen, in part, by a joint powers authority consisting of eleven members: City of Artesia, City of Bell, City of Bell Garden, City of Cudahy, City of Downey, City of Glendale, City of Huntington Park, City of

Maywood, City of Paramount, City of South Gate, and Burbank-Glendale-Pasadena Airport Authority; along with supporting agencies that include Caltrans – District 7, Metropolitan Transportation Authority (Metro), Gateway Cities Council of Governments, Southern California Association of Governments (SCAG), San Fernando Valley Council of Governments and Golden State Gateway Coalition. The Eco-Rapid Transit Corridor project was adopted into the Los Angeles County Metropolitan Transportation Authority's (Metro) 2009 Long Range Transportation Plan and is scheduled to be built by 2027. This corridor represents a segment of Metro's vast and expanding operating system. The goals of the Eco-Rapid Transit Corridor include:

- » Economic development/connecting people to jobs
- » Maximizing neighborhood and station connectivity
- » Establishing multimodal transit hubs at station locations
- » Enhancing opportunities for public spaces and commerce
- » Designing surrounding streets for people and active transportation
- » Creating affordable and accessible housing—50 percent of region's housing growth to be built in transit-oriented development
- » Managing parking effectively

1.2.1 WEST SANTA ANA BRANCH CORRIDOR

The West Santa Ana Branch (WSAB) Transit Corridor is the southern 20 miles of the full Eco-Rapid Corridor, running from Artesia to Downtown Los Angeles, generally along the Pacific Electric right-of-way (PE ROW) shown in Figure 1.1. The project was one of the "Twenty-Eight by '28" Metro projects designated for potential completion by the 2028 Summer Olympic and Paralympic Games. The WSAB Transit Corridor was allocated \$4 billion (in 2015 dollars) per Measure M and Metro's Long-Range Transportation Plan (LRTP). The project is funded by Measures R (2008) and M (2016), which fund transit improvements through one-half cent sales taxes, and well as other local, state, and federal sources.

The "Pacific Electric Right of Way/West Santa Ana Branch Corridor Alternative Analysis Report" studied the corridor along the former Pacific Electric Railway, now owned by Metro and Orange County Transportation Authority. The corridor which includes 14 cities and 15 proposed stations, was studied to explore the feasibility of bus rapid transit, street cars, light rail transit, or low speed maglev trains operating within the right-of-way.

FIGURE 1.1 Metro WSAB Corridor



The Alternative Analysis Report concluded that the West Santa Ana Branch Corridor is expected to:

- » Carry up to 70,000 daily boardings by 2040
- » Cost an estimated \$6.5 to \$6.6 billion (in 2018 dollars) in capital costs
- » Create 59,000 living wage jobs during construction
- » Generate \$6.68 billion in economic growth over 15 to 20 years
- » Result in a significant reduction in the region's carbon footprint

To support future economic development around Bellflower's station area, Metro awarded the City a Transit Oriented Development Planning Grant to prepare a specific plan for a ½-mile radius around the proposed transit station location.

1.2.2 DOWNTOWN BELLFLOWER STATION

The proposed Bellflower Station is at Bellflower Boulevard and the PE ROW, at the northern edge of downtown Bellflower. The proposed station is consistent with the City's long-standing transit-oriented development vision, policies, and plans for downtown. In addition, it was a historic stop on the PE West Santa Ana Branch and is part of the City's transportation heritage, now honored by the restored PE Depot.

Because the cities and station areas along the corridor will be much more interconnected after the rail line is built, it is important to consider the unique purposes different stations will serve in the region. When creating this Specific Plan, the City considered the corridor-level economy and implications for demand, supply, and competition. This resulted in a groundbreaking plan that attempts to create synergy between uses along the transit line, encouraging a thriving and well-rounded regional economy.

1.3 PURPOSE AND INTENT

The purpose of developing a specific plan for Downtown Bellflower is to prepare a transit-supportive specific plan that guides the future land use, mobility, and economic development in the specific plan area. The Specific Plan is intended to provide a regulatory framework for the downtown Bellflower area that includes customized land uses and development standards, provides expanded multimodal transportation choices, and identifies locations for future development potential. This Specific Plan serves as the zoning for the downtown Bellflower area and establishes policy guidance for land uses, development standards, and design guidelines.

Ultimately, the specific plan provides a collective community vision and strategy for the area that regulates land uses and design policies and standards, identifies locations for potential infill development and expanded multimodal transportation choices, and promotes a healthy lifestyle through the availability of active transportation options.

1.4 PROJECT SETTING AND BACKGROUND

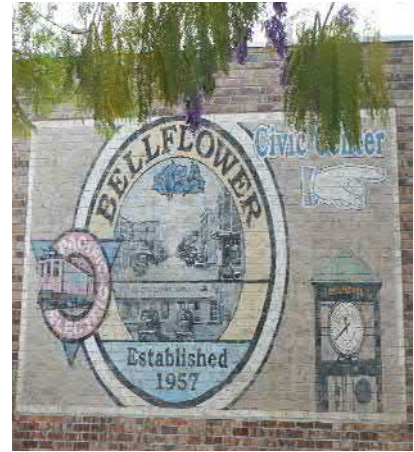
This section provides an overview of contextual characteristics and trends in the specific plan area and surrounding region. This information reflects the most-current available data and studies, including policy documents, zoning regulations, census data, market conditions, and mass transit plans and guidelines as well as input from City staff, Gateway Cities Council of Governments, and Eco-Rapid Transit.

1.4.1 HISTORY AND LOCAL CONTEXT

In its earliest days, the City of Bellflower—originally known as Somerset Ranch—consisted primarily of large parcels with open fields used by Dutch dairy farmers for grazing cows. When the Pacific Electric Railway opened in 1905, it linked residents of Bellflower to employment centers in Los Angeles. Land owners began to subdivide and sell one-acre farm lots as well as narrow, deep town-size lots (a neighborhood pattern that is still evident today). With the addition of the newly subdivided lots, Bellflower transitioned from a farming town to a diverse community with a thriving cultural, civic, and economic core. Consistent with nationwide trends, Bellflower’s population surged in the 1940s, households doubled in size, and homeowners with extra space built apartment buildings on their deep, narrow lots. Today, the City of Bellflower encompasses 6.12 square miles and is populated by nearly 80,000 people.

Bellflower is one of 88 cities in Los Angeles County and is located in the county’s southeast region. It is bounded by Downey to the north, Norwalk and Cerritos to the east, Lakewood to the south, and Paramount to the west. Well-positioned between four freeways—I-105, I-605, I-710, and SR-91 (see Figure 1.2)—its residents, workers, and visitors have access via vehicles to and from the best destinations Los Angeles County and Orange County have to offer. Once the Eco-Rapid Corridor is complete, Bellflower passengers will have access to all these locations via the regional transit system.

Bellflower primarily consists of single-family neighborhoods, corridor commercial development, active public spaces, and local employment centers. Kaiser Permanente Medical Group is the





City's largest single employer, followed by the City of Bellflower, Golden Corral, K-Mart, Tulaphorn (McDonald's), and Hollywood Sports Park. The Specific Plan area is in the southern part of the City (see Figure 1.2).

1.4.2 SPECIFIC PLAN LOCATION

The Specific Plan area generally covers a half-mile radius around the planned transit station, which is considered a relatively reasonable walking distance, based on U.S. research studies. Comprising approximately 400 acres, the Specific Plan area includes much of the area between Alondra Boulevard on the north, Flower Street on the south, Clark Avenue on the west, and Woodruff Avenue on the east, plus additional area on either side of Bellflower Boulevard to Jefferson Street on the north and Park Street on the south. The former Pacific Electric corridor runs diagonally through the specific plan area from northwest to southeast. The Specific Plan boundaries are illustrated in Figure 1.3.

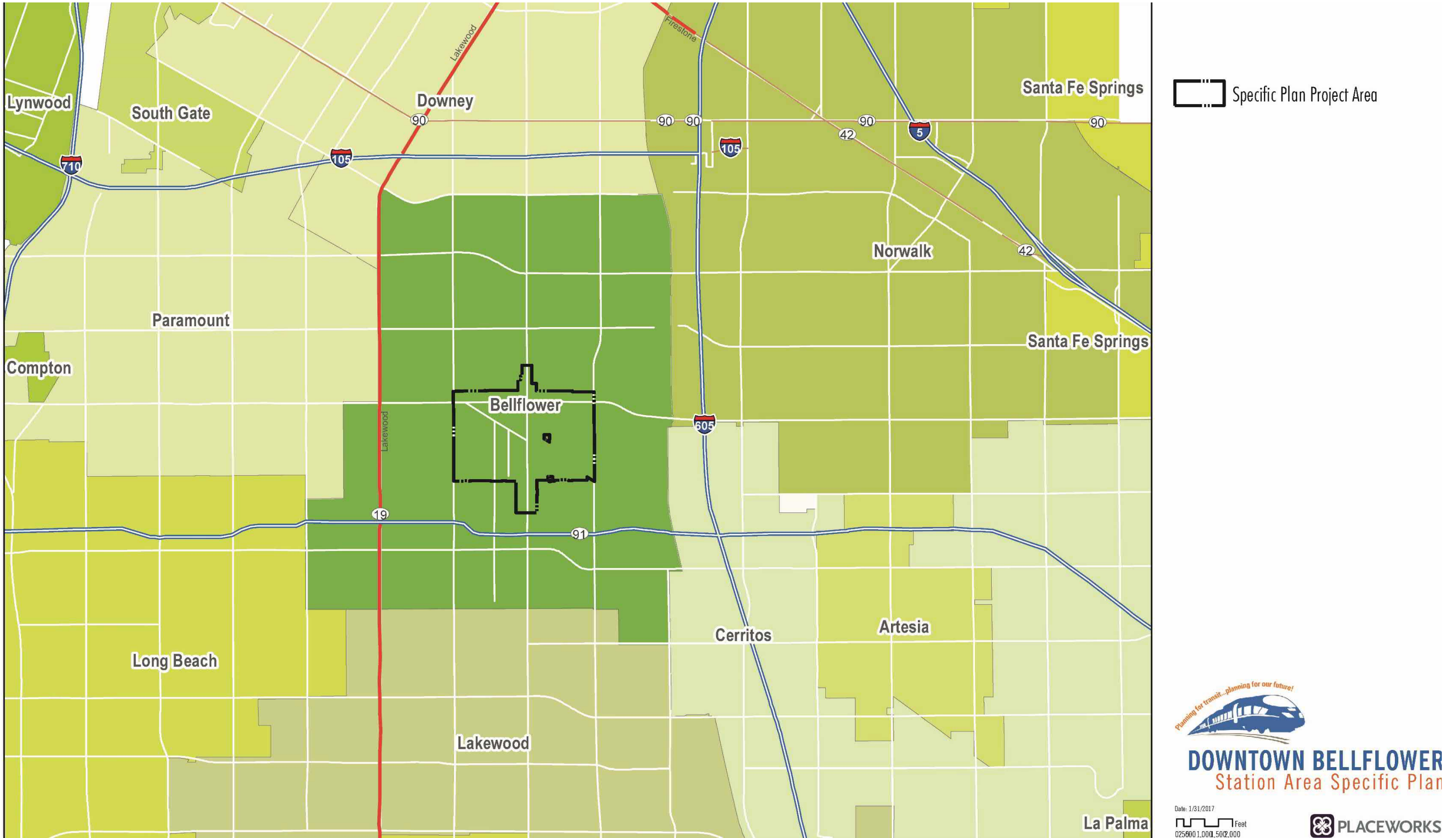
Although it represents only about 10 percent of the City's land area, the Specific Plan area has a population of 25,000, which is nearly a third of the City's entire population. The specific plan area is denser than the City on average, with 17,000 people per square mile compared to 12,680 people per square mile for the City as a whole. Employers within Bellflower supply approximately 17,000 jobs, or about 2 jobs per every 3 households. A large percentage of Bellflower residents leave the City to work; as a result, about 90 percent of Bellflower residents use a vehicle to commute to employment centers and nearly half (46.9 percent) commute for 30 minutes or longer.

1.4.3 EXISTING LAND USES AND URBAN DESIGN

Land use and urban design play a critical role in the creation of distinctive places and in establishing a unique identity for a community. This section provides an overview of the form, function, aesthetic, and experiential character in the Specific Plan area.

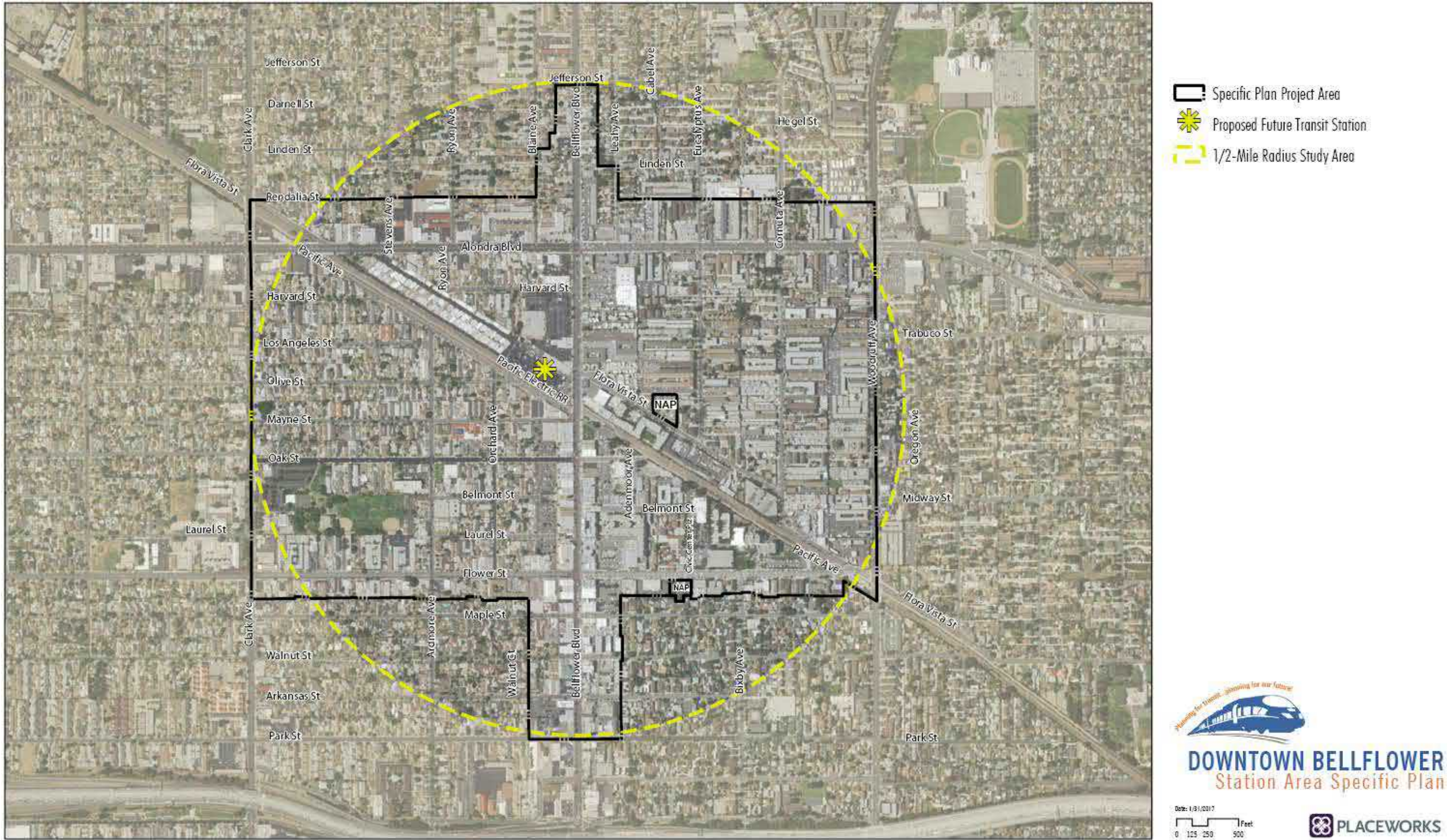
The majority of the specific plan area is residential, with homes ranging in style from traditional one-story single family detached to mid-sized multifamily properties with detached garages or accessory structures. The area is also home to several unique communities, including newly developed townhouse and mixed-use projects, mobile home communities, and large senior housing developments with full podium parking and private open space.

FIGURE 1.2 Local Context



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FIGURE 1.3 Specific Plan Area



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The Bellflower Boulevard portion of the specific plan area is almost exclusively commercial, and other commercial uses are located along Alondra Boulevard and Flower Street. These commercial properties are primarily one- and two-story retail shops with stucco facades, occasionally accented with brick or stone trim. Much of the storefront signage and facades along Bellflower Boulevard match the historic design aesthetic of the 1950s. Newer properties and businesses have modern facades and signage, creating a variable design character.

The Bellflower Boulevard corridor south of the PE ROW has a pedestrian-oriented atmosphere, but retail shops north of the ROW are set back from the street, with large parking lots and busy intersections. This creates an “auto centric” feeling and provides a gradual transition to the character of the Alondra Boulevard corridor, which caters to regional drivers and high speeds while providing a mix of retail, office, and auto-related businesses. The Flower Street corridor has clusters of small, one-story retail strip centers and detached commercial properties interspersed among residential properties. Storefront facades and signage are less elaborate than the Bellflower corridor’s and lack an overall curb appeal.

The Specific Plan area is home to Bellflower’s active Civic Center, which gives residents and visitors access to administration offices, the courthouse, and the public library. The Civic Center is also equipped with a public parking structure and surface lots. Although the surface lots and employee-designated areas of the structure are often near capacity, the majority of the parking structure is underutilized. Additional analysis about parking utilization throughout the area can be found in Appendix C. The specific plan area does not include any school facilities but does provide a United States Post Office and several religious institutions.

The Specific Plan area benefits from three parks. Library Garden is near the Civic Center and provides a place for programmed activities themed around literature and story-telling. Pirate Park is an interactive, themed pocket park centrally located along Bellflower Boulevard, close to shopping and restaurants. John S. Simms Parks is in a residential neighborhood along the western boundary of the Specific Plan area. Simms Park provides the community with sports fields, indoor auditoriums and multi-purpose rooms, playgrounds, and picnic areas.



1.4.4 MARKET ANALYSIS SUMMARY

In order to capitalize on the economic potential that typically accompanies transit development, the City conducted a market demand analysis in preparation for this Specific Plan (Appendix B). The market analysis provides an understanding of the market forces influencing demand for new development and redevelopment in the downtown area. The understanding of market forces, in turn, will influence the types of uses and standards for development that the Specific Plan will accommodate and regulate.

The market analysis covers commercial uses, including shopping, dining, entertainment, and office-based professional services. These are the types of activities one typically expects in a conventional downtown area. The market analysis does not cover residential development—current residential development projects and interest from developers pursuing or proposing entitlement of future residential development indicate there is current market demand and potential market demand going forward. The market analysis also does not cover industrial development—industrial uses typically require large areas of land, large facilities, and larger roadways and freeway access for trucks, development characteristics that conflict with the human-scale pedestrian-friendly character of the existing downtown area.

1.4.5 RETAIL DEMAND

Currently, downtown Bellflower functions partly as a convenience goods and services district and partly as an experience-oriented district. Many of the stores sell convenience goods or are restaurants, not unlike a typical strip center. But historic downtown Bellflower is very walkable, has a variety of dining options, and provides recreation and entertainment.

Strip centers prioritize easy auto access to quickly get in, satisfy a material need, and get out. In contrast, experience-oriented districts may not be as easily accessible by automobile because they prioritize ease and comfort of pedestrian circulation within the district. More importantly, recreation and entertainment—that is, things to do other than shopping—distinguish an experience-oriented shopping district from a convenience goods shopping center. Historic Bellflower already offers something unique and as such it is well-positioned to attract people in an experience-oriented climate.

By facilitating transit-oriented development in the downtown area, the Specific Plan can improve and augment the area's experience value. For experience-oriented shopping, the specific plan should encourage a mix of dining and entertainment, convenience goods, and boutique-scale comparison goods retailers. Ideally, the downtown area will attract people from the transit station and provide a place for nearby workers and residents to gather, eat, and quickly buy convenience goods.

1.4.6 MARKET ANALYSIS

To capitalize on the market demand and support the continued transition of the downtown into an experience-oriented shopping district, a 2016 Market Analysis offers a number of general suggestions for the Specific Plan:

- » Encourage a mix of dining and entertainment, convenience goods, and boutique-scale comparison goods retailers, specifically focusing business attraction efforts on specialty food stores, health and personal care stores, restaurants, and bars.
- » Continue to improve the experience of shopping in downtown in order to expand the trade area and attract consumer spending from a larger area.
- » Capitalize on the experience value of shopping and spending time in downtown and in the planned rail station to attract office-based businesses.
- » Continue to provide opportunities for a variety of housing types within the specific plan area—especially adjacent to the future station area.

The following recommendations included in the 2016 Market Analysis were incorporated in this Specific plan:

- » Seek to accommodate up to 76,000 to 116,000 square feet of new retail building space and up to 73,000 to 92,000 square feet of new office space.
- » Implement a parking management plan that maximizes existing resources, accommodates sufficient public and on-site parking for buildout development, and provides easy and comfortable pedestrian connections between parking areas and downtown businesses.
- » Ensure that there are adequate plazas and public spaces for events and activities.
- » Establish development standards that create and maintain a human-scale, pedestrian-friendly, architecturally attractive physical environment.

- » Provide public realm improvements, including bike parking and street and pedestrian lighting, that encourage a variety of transportation options and sidewalks, street furniture, gathering spaces, and landscaping that encourage and facilitate a pleasant and relaxing experience.

In addition, the specific plan can provide a framework for implementation. Specifically, the specific plan should provide direction to:

- » Form an incentive program for complementary businesses to locate in downtown, similar to the current restaurant incentive program.
- » Support the establishment of a business improvement district, or similar entity, that funds and operates public relations and advertising to market downtown Bellflower to residents throughout the subregion.

1.5 RELATIONSHIP TO OTHER RELEVANT PLANS AND PROGRAMS

The Downtown Bellflower Station Area Specific Plan was prepared to provide a critical link between City of Bellflower General Plan policies and actual development in the Specific Plan area. As a regulatory document, the Bellflower Station Area Specific Plan implements the General Plan and details a specific vision for future development projects. However, in order to understand how the new Specific Plan should be developed, it is important to review the regulatory tools and planning documents that currently govern development of the area around the future transit station. In addition to the summary in this section, a detailed Existing Conditions Report is found in Appendix A.

1.5.1 BELLFLOWER GENERAL PLAN

The City of Bellflower General plan was adopted in 1994 as a guide for private and public development throughout the City. An updated Housing Element was adopted in 2013 and identifies long-term housing goals and shorter-term policies to facilitate and community.

The Land Use Element establishes goals, policies, and implementation programs that define how new development and redevelopment will occur. Certain policies in the Land Use Element promote development in the Town Center core, striving to create a community hub where residents and visitors can come together to shop, socialize, and seek entertainment and civic services.

At the time the Circulation Element was adopted in 1997, Metro was conducting transit studies to identify areas of opportunity for expansion. Bellflower acknowledged the potential impact of regional transit development and prioritized the goal of providing residents and businesses with convenient and viable public transportation options by: maintaining the current level of transit service provided by the City; promoting the development of a multimodal transit center with downtown redevelopment plans; and enhancing the transit system's operations and efficiency. The General Plan prepared Bellflower for the development anticipated in conjunction with this Specific Plan.

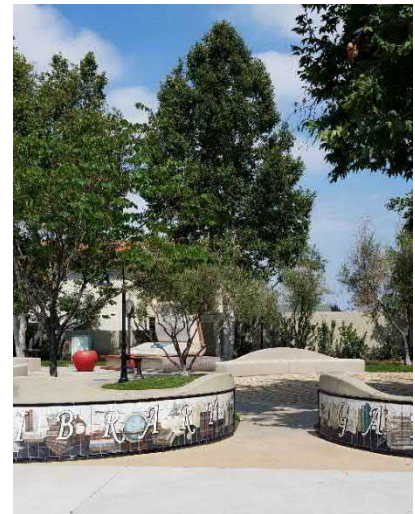
Current General Plan Designations

The General Plan Land Use Map (see Figure 1.4) shows several land use designations in the Specific Plan area. Residential land use designations include: Medium Density Residential (8.7–14 DU/net AC [dwelling units per net acre]), High Density Residential (14–22 DU/net AC), and Senior Citizen Housing (0–65 DU/net AC). Within the Specific Plan area, the two primary Commercial land use designations are General Commercial and Town Center. General Commercial designations emphasize the development of major retail centers, supermarkets, personal services, and financial institutions, typically within shopping centers or along major arterial roadways. Town Center designations are more focused on preserving and enhancing the historic character and charm of the downtown district by encouraging the development of specialty stores, restaurants, and entertainment facilities.

Additional land use designations in the Specific Plan area include Public/Quasi Public and Open Space. The Public/Quasi Public land use designations include the Bellflower Civic Center, which houses the City's post office, health center, courthouses, library, and administrative offices. The Open Space designation encourages the development of public parks, utility easements, and transportation corridors. The Open Space designation is vital to this Specific Plan, because it is utilized for the development of the forthcoming transit line along the PE ROW.

1.5.2 BELLFLOWER MUNICIPAL CODE

The Bellflower Municipal Code (BMC), Title 17, classifies and defines the zones throughout the City and in the Specific Plan area (see Figure 1.5).



Current Zoning Designations

Consistent with the existing land use and general plan designations, the majority of land in the specific plan area is zoned using traditional zoning districts, including Low, Medium, and Multiple Residential uses and General Commercial. A portion of the study area is zoned for Town Center (TC), which warrants further attention because of its unique approach to land use regulation and is described in detail below. In addition, a number of overlays are applied to property within the project area, which are summarized in this section.

Town Center District

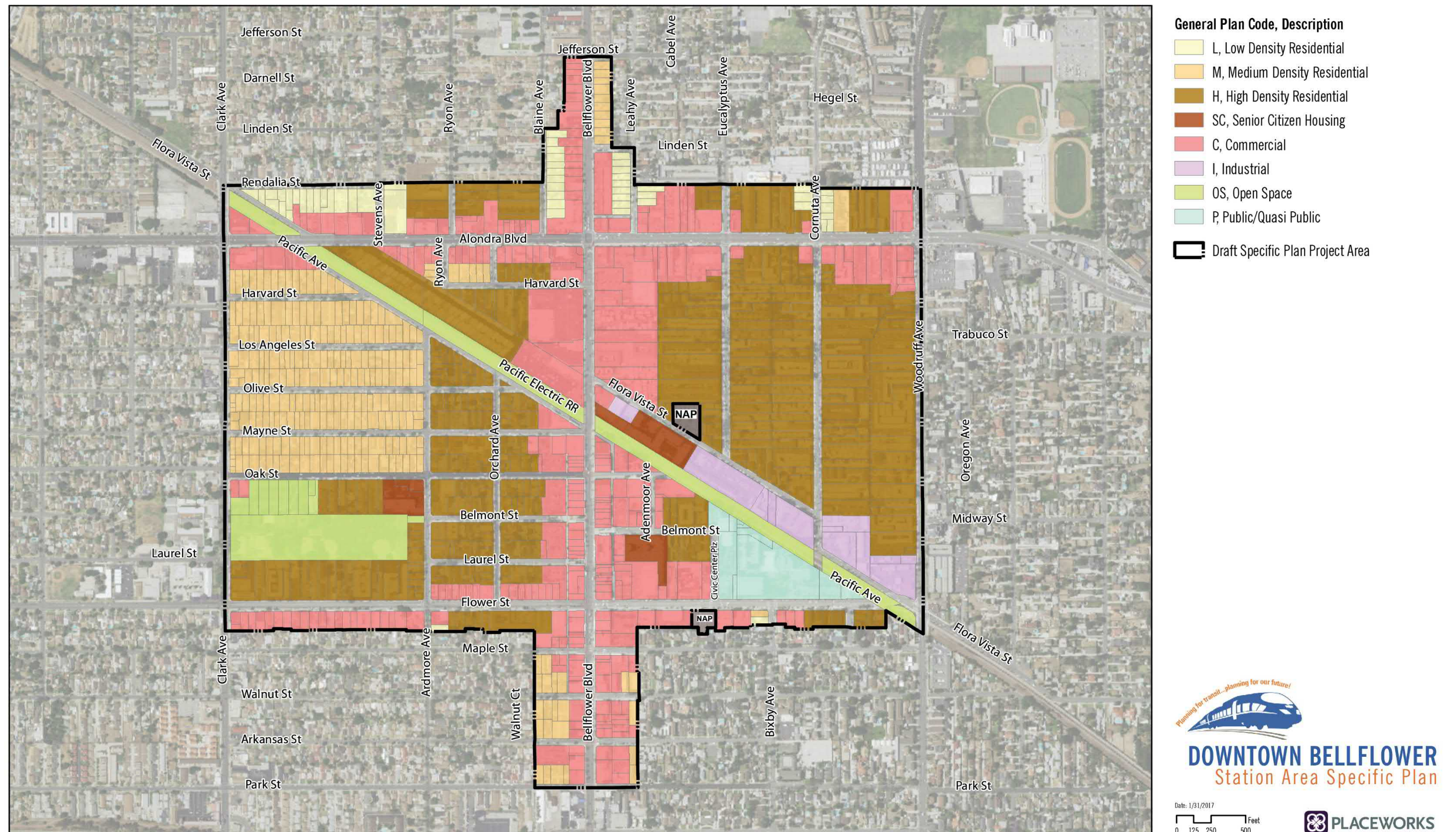
The Town Center District consists of two areas: Area 1 is defined as the traditional downtown core where buildings are typically adjacent to the sidewalk, and Area 2 is an eclectic mix of “modern” commercial establishments that lack pedestrian amenities. Allowable uses are defined by these areas, but development standards are applied consistently across Area 1 and 2.

1.5.3 OVERLAY ZONES

A number of existing overlay zones are located within the study area. An overlay zone allows the property to be developed and operated in compliance with the standard underlying zone or take advantage of new provisions in the overlay zone (e.g., increased maximum height), so long as certain conditions are met. Development within the overlay zone is optional and is voluntarily triggered by a developer.

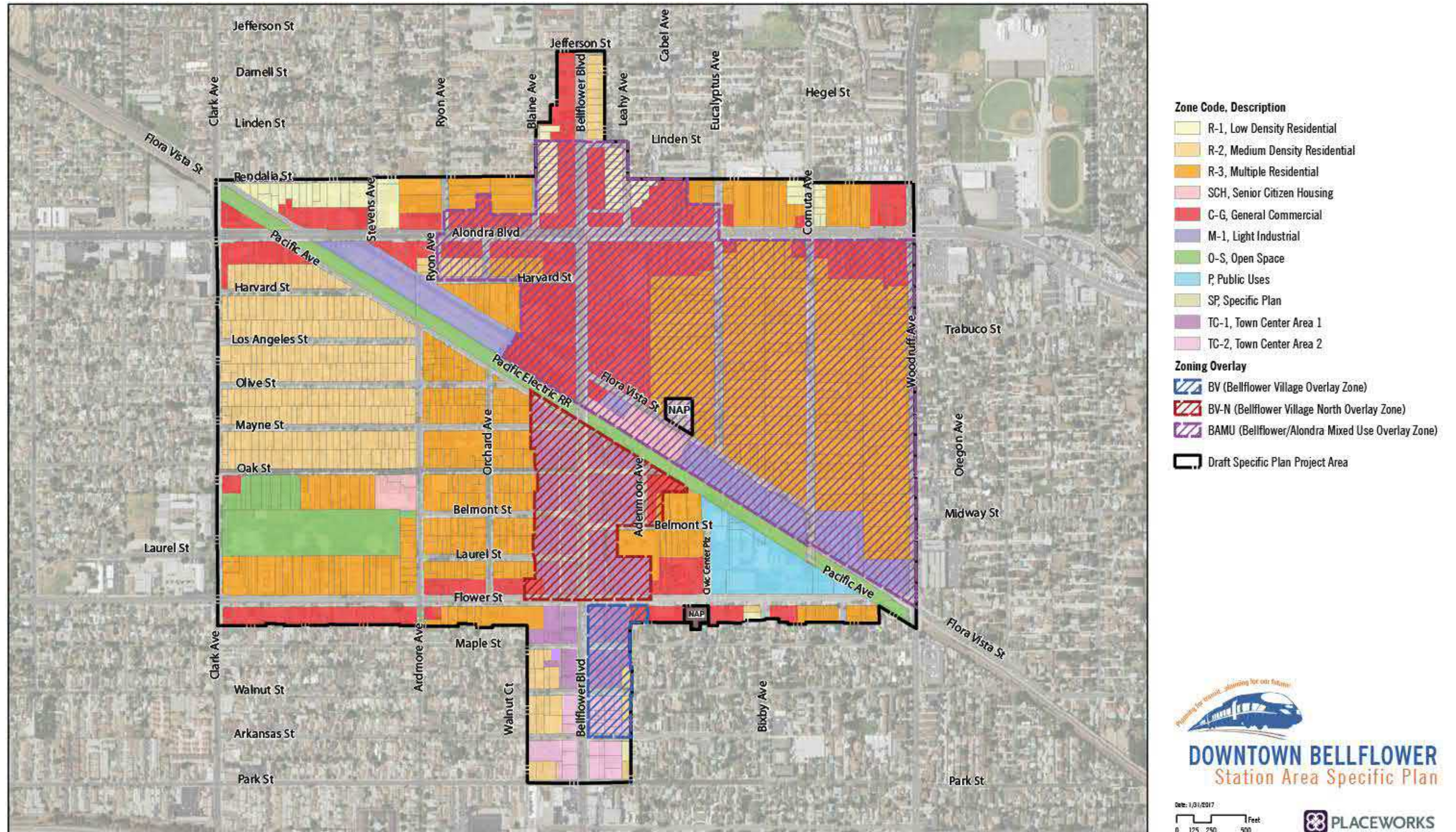
The ***Bellflower Village Overlay Zone (BVOZ)*** is centrally located in the study area and encompasses three blocks on the east side of Bellflower Boulevard between Flower Street and Arkansas Street. The area is fully developed with commercial, office, and residential land uses. The BVOZ is intended to serve as a tool to create the incentive for the reuse of land within the overlay zone boundaries where conventional and traditional zoning techniques may not accomplish the goals set forth in the Town Center District. Incentives include greater building height, the ability to incorporate residential uses into the project, and/or the ability to transfer some off-street parking obligations to another location. The BVOZ requires projects to include a public open space amenity or some form of physical interface for pedestrians. These are values that the City wants to maintain and potentially expand in the Specific Plan project area. The overlay zone refined the list of allowable uses previously identified in the Town Center District and modified some development standards, including minimum lot size, setbacks, and building height.

FIGURE 1.4 General Plan Land Use



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FIGURE 1.5 Existing Zoning Designations



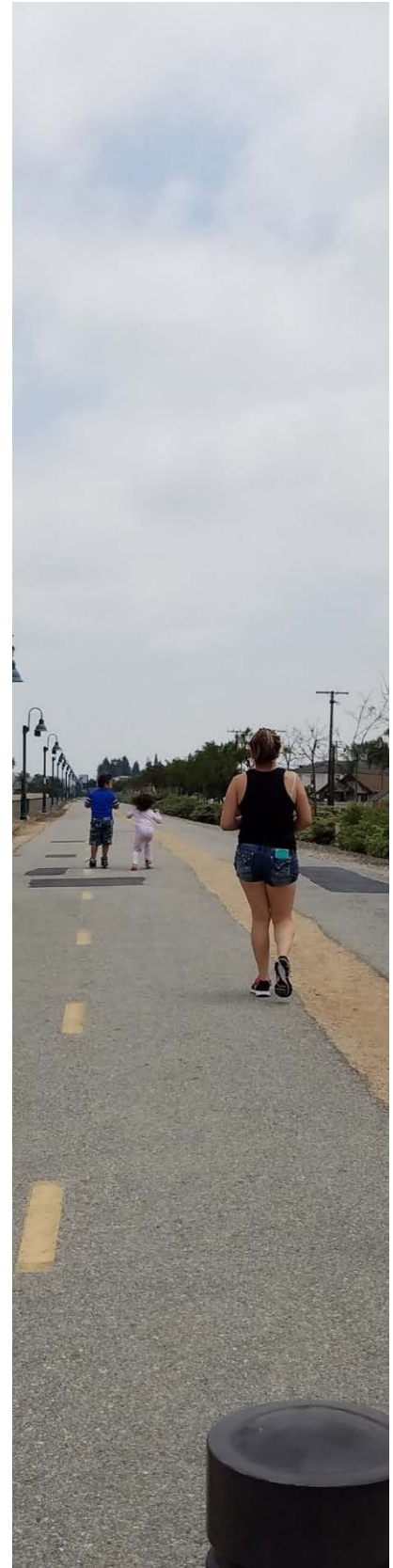
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The ***Bellflower Village Overlay Zone-North (North Overlay)*** encompasses a four-block area along Bellflower Boulevard between Flower Street to the south and the PE ROW to the north, immediately north of the BVOZ. The North Overlay reemphasizes many of the same principles originally articulated in the BVOZ, including a distinct focus on the pedestrian experience and creative parking strategies. It modified some development standards from the underlying standards identified in the Town Center District, including setbacks (which were reduced to zero) and building heights (two stories taller than allowed in the Town Center District, and one story higher than allowed in the BVOZ).

The BVOZ and North Overlay recognize that downtown Bellflower has the potential to become a creative, vital, and visionary neighborhood. However, because it is fully developed, it requires creative development and economic incentives to facilitate recycling of existing uses. The goals of the overlays are to:

1. Encourage the recycling of existing developed properties into contemporary, vital, and interesting development that will enrich downtown Bellflower.
2. Provide for more flexible regulatory procedures to realize the objectives of the City's general plan and zoning code.
3. Encourage creative approaches to use of the land through variation in building siting and appropriate mixing of commercial land uses and activities.
4. Encourage redevelopment by providing creative and unique economic opportunities to develop and use property.
5. Create a unique and vibrant neighborhood in the downtown area of Bellflower that will encourage further private investment in the City.
6. Enhance the appearance of the community through creative design of buildings, structures, and facilities.
7. Eliminate and prevent the spread of blight.
8. Revitalize, redevelop, and upgrade parcels in the overlay zone area.
9. Strengthen the City's economic base.

The Specific Plan reviews and draws upon the existing overlays for reference as the land use plan and development standards are prepared for the Specific Plan project area.





The ***Bellflower/Alondra Mixed-Use (BAMU) Overlay Zone*** applies to the area bounded by Alondra Avenue to the north, the Pacific Electric rail corridor to the south, Stevens Avenue to the west, and Woodruff Avenue to the east. The BAMU Overlay Zone is intended to provide opportunities for the development of horizontally or vertically integrated mixed-use activity centers and corridors. Commercial retail uses are encouraged on the ground floor, with offices or residential uses on upper floors. Residential development is also encouraged to enhance the availability of for-sale and rental housing to a variety of income levels. The BAMU includes a “residential subarea” where the minimum residential density is 40 DU/AC; this area satisfies the City’s RHNA allocation and so must maintain this minimum density (parcels outside of the residential subarea have a maximum density of 40 DU/AC).

1.5.4 DOWNTOWN BELLFLOWER REVITALIZATION VISION STRATEGY

The following strategies are land use actions the City could implement within 0 to 5 years to support the “rediscovery” of downtown and to help downtown Bellflower become a “completely original” place in southeast Los Angeles County.

- » Future land use strategies should incorporate the creative use of existing publicly owned property,
- » “Niche” uses, including restaurants and food uses, should be expected to include an “International Food” concept.
- » Small scale entertainment venues/activities should be recruited and nurtured within the Downtown area.
- » A series of five micro-climates (i.e., sub-areas) should be articulated within the Downtown area, and each one should include plaza areas, usable decorative features, and pedestrian connections.
- » Small public plazas should be introduced throughout the Downtown area to create public gathering spaces, and these plazas should be connected through pedestrian pathways.
- » Trees and landscaping should be incorporated into the Downtown area.
- » Retail carts, kiosks and small retail spaces should be allowed to provide additional retail interest.

The Downtown Vision Strategy was grounded in the creation of five distinct “micro-climates”:

1. Town Center Plaza (east side of Bellflower Boulevard, north of Belmont Street): Envisioned to provide a mix of residential and commercial uses, a formal civic plaza, and commercial supporting an adjacent Transit Plaza.
2. Friendship Square (west side of Bellflower Boulevard, between Laurel Street and Oak Street): An expanded public space that retains special physical features (trees, etc.) and introduces new amenities.
3. Library Garden (around the intersection of Flower Street and Civic Center Drive): Creation of a new programmed park/plaza area for small-scale events close to the Civic Center.
4. Bellflower Village (east side of Bellflower Boulevard between Maple Street and Arkansas Street): A unique collection of shops, restaurants, residential uses, and outdoor public spaces; the vision included closing Walnut Street to the depth of the commercial properties.
5. Freeway Portal (both sides of Bellflower Boulevard between Beach Street and SR-91): Provide a striking visual gateway into downtown from the freeway, promoting the features of downtown Bellflower.

The City was able to undertake many of the initiatives in the Downtown Vision Strategy including creation of Library Park and retail kiosks in the downtown area. Considering the success of the realized projects and the potential success of the remaining strategies, the Downtown Bellflower Station Area Specific Plan upholds many of the principles in the Downtown Vision Strategy. This Specific Plan will continue to encourage a vibrant center for dining and entertainment with the opportunity for small businesses and new development to be located within convenient proximity to actively programmed public plazas. Although the Specific Plan departs from the specific micro-climates proposed in the Vision Strategy, it identifies distinct subareas that are reorganized to ensure compatibility of uses and overall design appeal.

1.5.5 TOWN CENTER DESIGN GUIDELINES

In 1996, the City prepared the Town Center Design Guidelines, a series of design and architectural guidelines and recommendations for developers and property owners in the Town Center District. The guidelines did not promote any particular, single style, but the renovation and refurbishment of existing Town Center buildings. The objective was to rely upon existing architectural resources as a framework for future development, redevelopment, renovation, and refurbishment.

The Downtown Bellflower Station Area Specific Plan includes design guidelines that will carry forward appropriate portions of the existing Town Center Design Guidelines while eliminating outdated priorities and incorporating new guidelines that strengthen the continuity of design within subareas.

- » Infill design guidelines address: site planning, including setbacks, street orientation, compatibility, design integration, facade proportion, building opening proportion, horizontal rhythms, wall articulation, roofs, mechanical equipment screening, building materials, and security.
- » Storefront design and facade rehabilitation guidelines address: storefront replacement, storefront position and function, entryways, doors and windows, awnings, canopies, storefront accessories and ornamentation, rear entrance design, mechanical equipment and utilities, and selection of building materials.
- » Sign guidelines address: permitted sign types, general sign guidelines, wall signs, sign lettering, window signs, and awning signs.

1.5.6 NORTH DOWNTOWN BELLFLOWER LAND USE ECONOMIC STUDY

Conducted in 2014 by the City, the North Downtown Bellflower Land Use Economic Study evaluated the potential land use development scenarios for sites within the specific plan area. The potential land use development scenarios assessed public uses of property, development scenarios for privately owned properties, the redevelopment constraints at the time, land use economic considerations, and the long-range vision of the future development area, as articulated through the Town Center District, Revitalization Vision Strategy, and associated zoning overlays.

The study recommends that the City:

- » Proceed with development of the Moon property into a parking lot and eventual transit center facility. Although this property (on the south side of the tracks) was originally envisioned to become part of a future light rail station, the station will ultimately go on the north side of the tracks, and this property will serve primarily as a transit center for buses.
- » Proceed with a public plaza at the apex of the transit center and incorporate the Bellflower Boulevard Arch spanning Bellflower Boulevard as a public art feature. The preliminary site design for the Moon property does not have a pedestrian plaza at the apex, largely as a result of the engineering required to accommodate buses within the facility.
- » Begin discussions with the former Greek Market to allow the adaptive reuse of the site into a mixed-use development. Negotiations are ongoing on this property to realize this vision.

This study provides valuable insights into the economic viability of various development scenarios that will influence the development standards in the Specific Plan. As appropriate, design concepts in the study will be reflected in the Specific Plan, including the ideas identified in the two council study sessions that were conducted in conjunction with the study.

1.5.7 BELLFLOWER-PARAMOUNT BIKE & TRAIL MASTER PLAN

The cities of Bellflower and Paramount have recognized that bicycling is an important part of daily transportation for residents, commuters, and visitors. Together, the two cities drafted the Bellflower-Paramount Bike & Trail Master Plan. The plan aims to increase transportation options, improve safety and public health, and foster a stronger sense of community by improving bikeways, intersections and crossings, bicycle parking, and wayfinding programs. When addressing approaches to multimodal connectivity, this Specific Plan will use the Bellflower-Paramount Bike & Trail Master Plan to guide policy and program recommendations.

1.5.8 ECO-RAPID TRANSIT-ORIENTED DEVELOPMENT GUIDEBOOK

The Eco-Rapid Transit-Oriented Development Guidebook is an important tool that has been used in this specific plan process. This guidebook was developed to support member cities and stakeholders from Los Angeles to Artesia as they develop innovative planning and economic development responses to the Eco-Rapid rail transit opportunity. These cities form the West Santa Ana Branch Corridor (the Corridor), the southern portion of the full Eco-Rapid Corridor, which extends to Santa Clarita. While Metro is currently studying light rail as transit technology for the Corridor, the report refers more generically to rail systems, since a final decision on transit technology has not been made. Similarly, since a final plan for stations has not yet been adopted, the advice contained in this document contains general advice on how to plan for coordinated transit and economic development.

1.5.9 GATEWAY CITIES STRATEGIC TRANSPORTATION PLAN

The Gateway Cities Council of Governments and Metro came together to establish the Gateway Cities Strategic Transportation Plan (STP), a unified, subregional multimodal transportation improvement strategy. The STP was intended to help the Gateway Cities understand the unique transit needs of cities between two major employment and housing areas—Downtown Los Angeles to the north and Orange County to the south. The STP integrates transportation projects and needs at a regional level. It builds upon prior regional analysis by synthesizing the outcomes of corridor studies with new technical work to create a cumulative, multimodal subregional plan. The STP explores how changes or improvements to one or more transportation facilities impact the performance of others in the system. Finally, the STP establishes a detailed funding and financing strategy for Metro and the Gateway Cities Council of Governments to pursue federal and state funding.

1.6 SPECIFIC PLAN PUBLIC OUTREACH SUMMARY

Bellflower's City government has a strong track record of connecting with the community in a variety of ways. The Parks & Recreation Department actively programs public spaces with a regular Farmers Market and special events such as Food Trucks & Flicks, Summer Streetfest, and the Tree Lighting Ceremony. The City also maintains a pro-business attitude and provides a wide variety of resources and support for local businesses. It proactively communicates with residents and visitors through active social media accounts, the E-Citizen Newsletter, and a semiannual Parks & Recreation catalog. By fostering ongoing communication and public access, the City is better able to engage community stakeholders when it needs input regarding important policy, planning, and development decisions. As part of the planning process for the Downtown Bellflower Station Area Specific Plan, the project team incorporated project-specific outreach into scheduled community events.

1.6.1 COMMUNITY SURVEYS

For this Specific Plan, City staff engaged the community at two of the most heavily attended events—Food Trucks & Flicks and Streetfest. At these events, the project team passed out fact sheets, discussed the project with community members, and conducted surveys to assess the community's support, concerns, and questions regarding the project.

In addition, an online survey was developed and shared through the City's newsletters, mailing lists, and social media platforms. The survey was completed by 170 enthusiastic community members and/or visitors and allowed the project team to identify community priorities and potential obstacles. Survey participants are most excited to see new restaurants, nightlife options, and art and culture. They also look forward to the train increasing access to Downtown Los Angeles, Los Angeles beach cities, and LAX and Burbank airports. Participants shared ideas for amenities and features that would make the train-riding experience more enjoyable. The survey also provided the opportunity for community members to express concerns and address any unanswered questions. The City utilized the community's feedback to further shape the project vision and goals. Survey responses will continue to inform future community engagement initiatives.

1.7 AUTHORITY AND SCOPE

This Specific Plan was prepared in conformity with Government Code §§ 65451, *et seq.* Together with the zoning regulations adopted by the Bellflower Municipal Code (“BMC”), and other applicable law, this Specific Plan sets forth the regulations for development within the various overlay zones identified by this document. The California Government Code states that a “Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan, and further, that it may not be adopted or amended unless found to be consistent with the General Plan.” This Specific Plan document has been designed to be consistent with the City of Bellflower General Plan goals and policies. An overview of the Specific Plan’s consistency with the General Plan is in Chapter 8.

In response to the Government Code, this Specific Plan has been prepared to provide an essential link between City of Bellflower policies and development in the Specific Plan area. Functioning as a regulatory document, the Downtown Bellflower Station Area Specific Plan provides a means of implementing the City of Bellflower General Plan and detailing specific requirements and guidelines for future development in the Specific Plan area. In this regard, all future development plans, tentative parcel and/or tract maps, or other similar entitlements must be consistent with regulations set forth in this document and with all applicable City regulations.

1.8 ACTION EXPECTATIONS

The policies, standards, and guidelines in this Specific Plan define action expectations to successfully implement the specific plan. The following terms clarify the level of commitment intended and reflect the expectation or desired outcome. The application of a term to a particular policy action is a deliberate application of its definition.

1. **“Must”** is mandatory;
2. **“May”** is permissive;
3. **“May not”** means not permitted to;
4. **“Will”** expresses a future contingency or predicts action by the City or person in the ordinary course of events, but does not signify a mandatory duty; and
5. **“Should”** expresses a preference or a nonbinding recommendation.

1.9 SPECIFIC PLAN ORGANIZATION

Chapter 1—Introduction & Background

Provides a history of the project area and regional transit context while summarizing public outreach efforts and market demand analysis. It includes the Specific Plan's purpose and intent, authority and scope, and relationship to the City of Bellflower General Plan. It identifies the specific plan area and describes an overview of existing conditions.

Chapter 2—Vision & Objectives

Reveals the community's vision by outlining the purpose and objectives of the Specific Plan.

Chapter 3—Land Use Plan

Outlines permitted land uses, residential and nonresidential real estate forecasts, and opportunities and constraints to future development.

Chapter 4—Development Standards

Presents a comprehensive set of regulations governing the use and development of land within the Specific Plan area.

Chapter 5—Mobility Plan

Details multimodal mobility improvements, future transit and pedestrian connections, and analyzes existing and future parking demand.

Chapter 6—Infrastructure Plan

Focuses on the major infrastructure systems, including storm drain, wastewater, and water, and the impact future development could have on these systems.

Chapter 7—Design Guidelines

Guides the physical design related to site configuration and building design, streetscape design, public spaces, and parking.

Chapter 8—Administration & Implementation

Explains the process for project approvals, amendments, and interpretations and identifies funding and financing mechanisms, a list of implementation actions, and anticipated phasing. Includes an analysis of General Plan consistency.

Appendix A—Existing Conditions Report

Provides an overview of existing characteristics and trends in the specific plan area, and identifies opportunities and constraints that should be addressed in the Specific Plan.

Appendix B—Market Demand Analysis

Presents a summary of the market forces influencing demand for new development and redevelopment within the project area.

Appendix C—Parking Management Plan

Provides a summary of existing parking conditions and makes recommendations for effective management of parking resources to accommodate the future parking needs within the specific plan area.

Appendix D—Town Center Design Guidelines

The Town Center Design Guidelines were adopted in 1996. This document provided methods, materials, and other guidelines to encourage the development of a unified identity for center of Bellflower.



CHAPTER 2

VISION & OBJECTIVES

2. VISION AND OBJECTIVES

2.1 VISION

The Specific Plan for the Downtown Bellflower Station Area begins with a vision—a vision that describes the community’s aspirations for how this historic core area will look and function in the future, especially with a light rail station providing direct and convenient access to destinations along the corridor as well as throughout Los Angeles via the Metro Rail system. Change is coming to Bellflower, not only in terms of the future transit option, but also in terms of the new market opportunities that come with it for transit-supportive residential, commercial, office, and destination-related uses. These changes are expected to take place over the next 25 years and this Specific Plan is the foundation and primary tool for implementing these changes. The vision for this highly valued area is as follows:

Downtown Bellflower is the heart and hub of the community and it has only grown stronger and more vital since the opening of the Downtown Bellflower Station. It is safe, walkable, attractive, and economically thriving. It is a frequent destination for Bellflower residents and employees, as well as the thousands of transit riders passing through the station each month. The area has a unique blend of businesses that serve local residents and attract neighboring communities eager to visit the latest shop or restaurant and stroll the boulevard. Together with its neighborhoods of new and established homes, there is a renewed sense of community that makes Bellflower a very desirable place to live and do business in southeast Los Angeles.

Many residents have planted deep roots here, and the established neighborhoods of Bellflower, along with the new transit station are the downtown’s anchor. Clusters of retail shops, restaurants, outdoor gathering areas, and other local-serving businesses along the downtown area create fun places to gather with friends and family. Grown children come back to live, work, start businesses, and raise families because of their strong ties to the community and desirability of the neighborhoods.

The first time you enter the downtown area from either Alondra Boulevard or the 91 Freeway, you know you have arrived someplace special. What used to be vacant or rundown buildings along Bellflower Boulevard have been transformed to a thriving commercial and residential corridor that is inviting and safe to walk, cross, ride a bike, or take transit. The new transit station has been integrated into the downtown environment and works efficiently in accommodating the parking and bus transfer needs of riders on a daily basis. Numerous plazas, paseos, pocket parks, and outdoor eating areas have sprung up as integral parts of each new development project. Improved bike facilities in the Specific Plan area connect to a citywide system of bike paths connecting schools, parks, transit, and destinations along the boulevard. These improvements also promote a healthy lifestyle throughout the community. The improved lighting, landscaping, and signage that was once only along a short segment of Bellflower Boulevard has now been expanded in the study area to add to the appeal and enhance wayfinding.

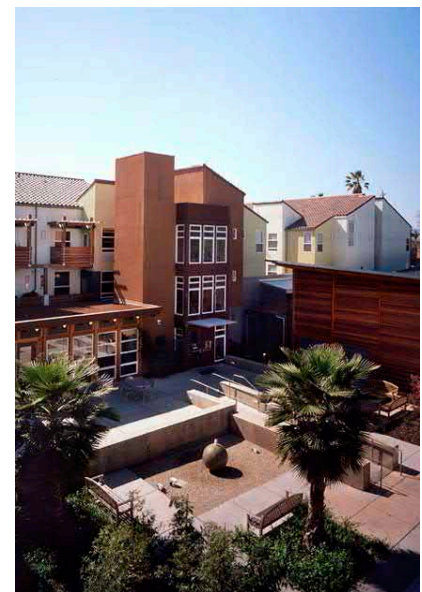
The improvements to Bellflower Boulevard and the light rail station have created a setting for new high-quality residences in the study area. This housing has replaced aging and obsolete buildings and added new life and activity to the street. The new housing complements the mix of uses along the corridors and is compatible with adjacent single-family homes. This new housing supports local businesses, provides a mix of densities, increases our property values, and meets the high demand for housing in our community.

Community pride is unmistakable—just attend a downtown event, look at our neighborhoods and streets, or talk with a local business owner. Downtown Bellflower and the new transit station is truly the heart and hub of the community.

2.2 OBJECTIVES

The Vision for the Downtown Bellflower Station Area Specific Plan has been translated into a series of objectives that has guided the detailed provisions of this document. The objectives are related to land use, urban design, zoning, mobility/transit, infrastructure, and long-term implementation:

- » Plan for High-Quality Transit and a Transit-Supporting Environment
- » Boost the Economy and “Rediscover the Downtown”
- » Extend Public Realm Improvements
- » Provide for a Balanced Mix of Uses



2.2.1 PLAN FOR HIGH-QUALITY TRANSIT AND A TRANSIT-SUPPORTING ENVIRONMENT

The Downtown Bellflower Station will be one of 15 stations along the West Santa Ana Branch Transit Corridor. Located along Bellflower Boulevard within the downtown area, the station will serve residents and workers in the City, connecting them to destinations in the greater Metro service area. Studies have shown that transit corridors and systems that provide direct connections to a region's downtown or major employment centers are significantly more likely to attract new development and have higher ridership compared to transit lines that do not serve a central business district. Other factors that influence the potential for a transit-oriented development ("TOD") include the market strength of a transit station area, the availability of appropriate opportunity sites, zoning regulations that support TOD, connectivity, and local government support. This Specific Plan addresses each of those factors and sets forth a regulatory, policy, and design framework within a half-mile radius of the station that supports transit and maximizes the economic development potential and ridership potential it brings.



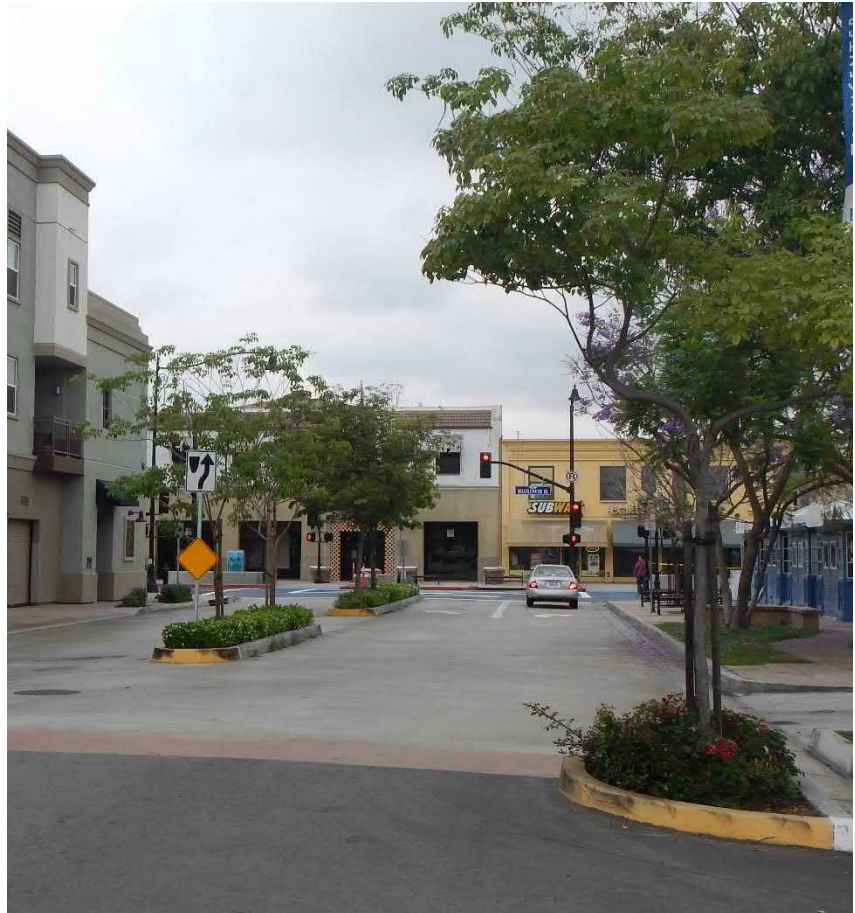
2.2.2 BOOST THE ECONOMY AND “REDISCOVER THE DOWNTOWN”

A Downtown Revitalization Vision Strategy was prepared in 2003 and contained a series of specific land use and design strategies to help the downtown become a “completely original” place in southeast Los Angeles. As summarized in Section 1.8, many of its initiatives were completed. However, the Great Recession and the ending of redevelopment agencies as a local tool significantly slowed progress in revitalizing the area. Recently, several new restaurants, a residential/retail mixed-use project, and streetscape improvements have attracted new interest in the downtown. The citizens of Bellflower have been encouraged by these changes and are anxious to see empty buildings filled with new and stable businesses, and the redevelopment of underutilized sites with additional retail and housing clustered in the downtown area. As noted in the Market Analysis Summary (Section 1.6), downtown Bellflower has the potential to distinguish itself as an experience-oriented district—one that prioritizes the ease and comfort of pedestrians and includes not only shopping and dining but recreation and entertainment. An objective for this Specific Plan is to facilitate transit-oriented development, which can improve and augment the area’s experience value. The Specific Plan lays the foundation, through updated regulations and economic incentives, to allow for a variety of new residential development—in the right locations—to boost demand for quality restaurants, coffee houses, grocery stores, and other food/beverage and retail venues. In addition, the Specific Plan is intended to support current and future economic programs offered by the City to increase the rate of new business formation, attract new businesses to the area, and assist in building-retrofit costs. The Specific Plan is also intended to address the struggling corridors that tie into Bellflower Boulevard. Some of the uses along Alondra Boulevard in particular are in a state of decline and need to transition to higher and better uses in the future.



2.2.3 EXTEND PUBLIC REALM IMPROVEMENTS

A segment of Bellflower Boulevard between the Pacific Electric Right of Way and Flower Street was significantly upgraded with street trees and planters, widened sidewalks, seating areas, bulb-outs, and pedestrian safety features, including streetlights, crosswalks, and accessible intersections. The street design calms traffic entering the downtown and sets the stage for an inviting shopping and dining environment. An objective of the Specific Plan is to identify additional street segments in the area that should be upgraded to improve connectivity to the transit station and other destinations. Improved street design is an essential ingredient to attracting new investment and ensuring that new uses will orient to and activate the street. These public realm improvements will also include concepts for entry monuments at the corners of Alondra Boulevard and Bellflower Boulevard to announce the entrance into the downtown. In addition to improvements to existing streets, the Specific Plan addresses connectivity into and around the future transit station as well as the potential for new street or pedestrian connections to Bellflower Boulevard.

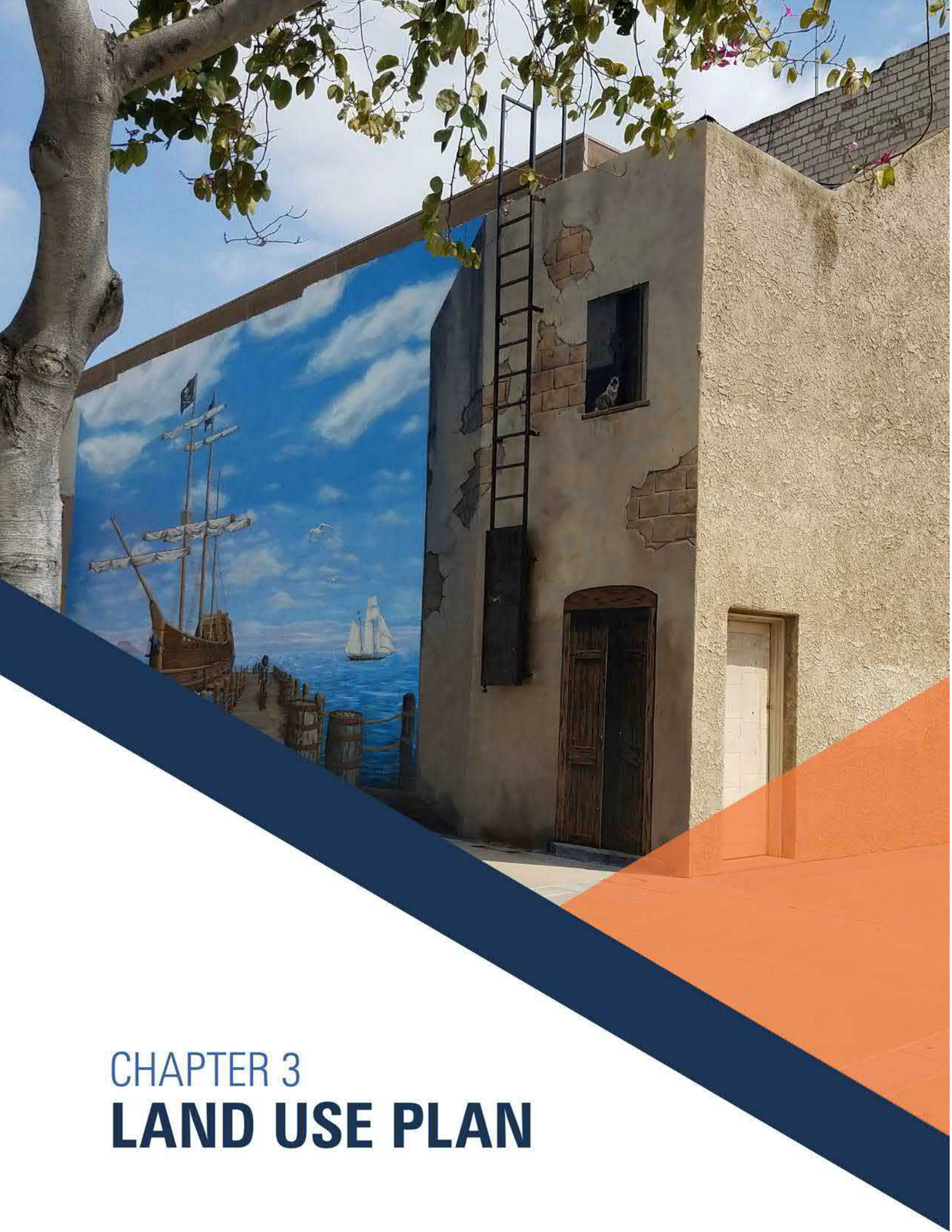


2.2.4 PROVIDE FOR A BALANCED MIX OF USES

An overarching objective of the Specific Plan is to provide for a better balance of land uses that will be supported by the current and future market. Some new local restaurants, coffee houses, and retail shops have struggled to survive over the last few years. While the City has invested in streetscape improvements, offered incentives to locate in the downtown, and created new zoning to attract development, there has been insufficient demand from existing residents and employees to sustain those uses. This Specific Plan re-examines the land use mix and provides greater flexibility for stand-alone residential projects to be built in and near the downtown. There is future demand for a wide range of housing types, including row-towns, courtyard housing, and multistory flats with above- and underground parking. And the demand for housing will continue to increase as the Eco-Rapid Line is implemented. The Specific Plan tailors the zoning to encourage new housing that will “fit” with the area, be sensitive to nearby single-family neighborhoods, and create a sustained demand for retail and services in the downtown. Along with new housing, the Specific Plan allows the potential for an increase in office demand as businesses increasingly look for locations within walkable commercial environments and near transit.







CHAPTER 3

LAND USE PLAN

1 LAND USE PLAN

The Downtown Bellflower Station Area Specific Plan establishes a framework that will allow the City to take advantage of the economic and quality of life benefits from the future Bellflower Transit Station. This section establishes the intent of the Specific Plan's future land use and the quality and character of new development or reuse. It also builds upon the overlay zoning system and creates two new overlay zones to augment the existing zones. This allows for greater flexibility for future projects to respond to the market, as the area evolves with the future inclusion of light rail.

3.1 OBJECTIVES & POLICIES

The Vision and Guiding Principles established in Chapter 2 have informed the land use objectives and policies contained below. The objectives and policies are also consistent with the General Plan. The policies serve as tools to help the City achieve the specific plan vision. The following lists the major land use objectives and policies for the Downtown Bellflower Station Area Specific Plan:

- **OBJECTIVE 1:** Create an experience-oriented district around the future Eco-Rapid Line and Downtown Bellflower that reinforces and promotes the specific plan area as a desirable and economically vital place to live, work, visit, and create businesses.
 - » **Policy 1.1:** Develop and maintain a cohesive street and building design identity.
 - » **Policy 1.2:** Encourage development that offers housing, shopping, employment, and business development options for residents and visitors.
 - » **Policy 1.3:** Encourage appropriately-scaled infill and mixed-use development.
 - » **Policy 1.4:** Accommodate the development of uses that meet the needs of businesses and residents. This includes retail, dining, healthy food, recreation, entertainment, and housing options.
 - » **Policy 1.5:** Accommodate growth within a half mile of the station area and commercial corridors that respects and responds to the existing scale and density of the neighborhood.
 - » **Policy 1.6:** Develop and maintain gateway elements.

- **OBJECTIVE 2:** Encourage a mix of uses in proximity to the transit station and along major streets that benefit from the economic opportunities afforded by the presence of the future Eco-Rapid Line, a vibrant Downtown, and the Bellflower Civic Center.
 - » **Policy 2.1:** Establish two new overlay zones with increased heights, densities, and strong urban design standards.
 - » **Policy 2.2:** Promote Flower Street as a corridor with a mix of uses that compliments existing and future development.
 - » **Policy 2.3:** Coordinate with Metro on a public plaza and an area with shopping, employment, and housing opportunities at the future Bellflower Station.
 - » **Policy 2.4:** Scrutinize development plans that have significant impact on the City to ensure the highest quality design.
 - » **Policy 2.5:** Promote the use of current and future economic tools provided by the City to assist with building-retrofit costs and establish new businesses in appropriate locations.
 - » **Policy 2.6:** Encourage the redevelopment or adaptive reuse of aging or obsolete buildings.
- **OBJECTIVE 3:** Create a pedestrian-oriented and active public realm.
 - » **Policy 3.1:** Encourage the design of pedestrian-oriented facades and uses at street level where appropriate.
 - » **Policy 3.2:** Encourage the investment in public art.
 - » **Policy 3.3:** Coordinate with the Public Works Department and private developers on changes to the right-of-way to ensure that future streetscape and development projects include sidewalks, wayfinding signage, and pedestrian amenities that enhance the public realm.
 - » **Policy 3.4:** Expand the public gathering space options through the development of the Bellflower Station Plaza and future development opportunities.

3.2 LAND USE DESIGNATIONS

The Land Use Plan contains 15 land use designations. Land use designations include base zones consistent with the City's current zoning code, and a combination of existing and new overlay zones. These zones are defined below.

Over time, the City has successfully implemented a number of overlay zones to address the downtown and nearby areas within the Specific Plan area. These overlay zones are intended to encourage new investment and development and provide opportunities for integrated mixed uses along Bellflower Boulevard and other key corridors. Existing overlays include the Bellflower Village Overlay Zone, the Bellflower Village Overlay Zone – North, and the Bellflower/Alondra Mixed-Use Overlay Zone. This document establishes two new overlay zones: the Transit Overlay Zone and the Mixed-Use Corridor Overlay Zone. These new overlay zones are tailored to enhance a major corridor and accommodate transit-oriented development adjacent to the future light rail station. The new overlay zones have provisions that include increased heights and densities and strong urban design standards. They also promote a pedestrian-oriented and active street environment. In total, the overlay zones cover 205.4 acres or 51% of the Specific Plan area.

The land use designations and the overlay zones can be seen in Figure 3.1: *Land Use Plan*.

3.2.1 LOW DENSITY RESIDENTIAL ZONE (R-1)

Intended to provide an attractive, healthy and safe environment featuring single-family detached homes and complementary recreational, religious, educational and other community facilities that are the basic elements of a balanced neighborhood. Density 0-8.7 du /ac. (See image 1).



Image 1 Single family housing

3.2.2 MEDIUM DENSITY RESIDENTIAL ZONE (R-2)

Establishes a density range that supports the development of quality, well-designed detached and attached living environments as either condominium or apartments, near transit and other services. Example residential types include, but are not limited to: small-lot single-family detached homes, single-family attached homes (duplexes), garden apartments, townhouses, and courtyard housing. Density 8.7-14 du/ac. (See image 2).



Image 2 Multiple detached homes in close proximity



Image 3 Multifamily housing



Image 4 Pedestrian-oriented commercial buildings



Image 5 Light industrial building



Image 6 Bellflower bicycle and pedestrian path



Image 7 Bellflower City Hall

3.2.3 MULTIPLE RESIDENTIAL ZONE (R-3)

Intended to provide for quality, well-designed multifamily living environments, as either condominiums or apartments. Typified by townhomes, courtyard housing and stacked-flats near transit and other services. Housing within this designation will bring people into close living arrangements, therefore, dwelling unit privacy, outdoor common and private open space, security, and overall quality design will be of the utmost importance. Density 14-22 du/ac. (See image 3).

3.2.4 GENERAL COMMERCIAL ZONE (C-G)

Intended to provide adequate areas along Bellflower's primary corridors to serve the convenience commercial and service needs of the community and the surrounding area. This includes shopping centers with a major tenant, such as a supermarket and/or drug store and includes ancillary activities such as retail sales, eating establishments, household goods, personal services, and financial institutions (see image 4).

3.2.5 LIGHT INDUSTRIAL DISTRICT (M-1)

Intended to provide for the development and establishment of base of the City. These uses include fabrication, manufacturing, assembly or processing of materials that are in processed form and which do not in their maintenance, assembly, manufacture or plant operation create smoke, gas, odor, dust, sound, vibration, soot or lighting to any degree which might be termed obnoxious or offensive to any person residing in, or conducting business in, this or any other zoning district in the City (see image 5).

3.2.6 OPEN SPACE ZONE (O-S)

This Open Space designation is applied to the existing parks within the area as well as the Pacific Electric Right-of-Way which includes a multi-purpose trail. The intent is to recognize that open space land within the City is a limited and valuable resource which must be conserved wherever possible. This designation may also include public or quasi-public recreation centers (see image 6).

3.2.7 PUBLIC USES ZONE (P)

Intended to protect and perpetuate institutional uses maintained and operated by Federal, State, County, District, City and other public agencies. It can also include libraries, museums, and educational uses (see image 7).

3.2.8 SENIOR CITIZEN HOUSING (R-3 SCH)

This designation represents existing senior citizen housing projects that have formerly been designated as an R-3 SCH District pursuant to Chapter 17.36 of the Bellflower Municipal Code. Density 0-65 du/ac. (See image 8).



Image 8 Bellflower Friendship Manor senior housing

3.2.9 TOWN CENTER DISTRICT AREA 1 (TC-1)

Intended to provide for the development and redevelopment of the City's downtown area. Area 1 is located south of the Southern Pacific RR right-of-way, north of Walnut Street, and is defined as the traditional downtown core. This area contains a variety of traditional retail storefronts which are a testament to the architectural heritage of Bellflower. Buildings within Area 1 are typically located adjacent to the sidewalk extending from property-line-to-property-line with storefronts primarily oriented towards Bellflower Boulevard, and parking lots commonly located behind. Area 1 has the greatest diversity and variety of specialty retail stores, offices, and restaurants in Bellflower and is the most pedestrian active (see image 9).



Image 9 Mixed use development

3.2.10 TOWN CENTER DISTRICT AREA 2 (TC-2)

Intended to provide for the development and redevelopment of the City's downtown area. Area 2 is located south of Walnut Street, north of the Artesia (91) Freeway. Area 2 contains both sidewalk adjacent storefronts and buildings located towards the rear of the parcel, with vehicular parking lots oriented towards the street. Area 2 also contains many "modern" commercial establishments which lack pedestrian amenities, inward oriented office uses, and stand-alone commercial "islands," such as service stations, which are surrounded by parking lots and circulation aisles (see image 10).



Image 10 Bellflower Bagels

3.2.11 OVERLAY ZONES

Bellflower Village Overlay Zone (BVOZ)

The intent of the Bellflower Village Overlay Zone is to encourage the development and redevelopment of the City's downtown area into a creative, vital and visionary neighborhood featuring a robust mix of residential and commercial uses. The intent of this designation is to accommodate and promote traditional specialty retail uses, enhance pedestrian activities through street furniture, landscape, and hardscape amenities, promote entertainment activities, respect established traditional building designs, encourage well designed and pedestrian oriented signage and promote inviting storefront design. Commercial retail uses are encouraged on the ground floor with offices or residential uses on upper floors. Downtown uses are intended to stimulate commerce by promoting a mixture of specialty



Image 11 Restaurant in Bellflower Village Overlay Zone (BVOZ)

stores, restaurants, offices, and entertainment facilities designed to attract people and enhance the pedestrian experience (see image 11).



Image 12 Residential development in the Bellflower Village North Overlay Zone (BVOZ-N)

Bellflower Village North Overlay Zone (BVOZ-N)

The intent of the Bellflower Village North Overlay Zone is to encourage the development and redevelopment of the City's downtown area into a creative, vital and visionary neighborhood featuring a robust mix of residential and commercial uses. The intent of this designation is to accommodate and promote traditional specialty retail uses, enhance pedestrian activities through street furniture, landscape, and hardscape amenities, promote entertainment activities, respect established traditional building designs, encourage well designed and pedestrian oriented signage and promote inviting storefront design. Downtown uses are intended to stimulate commerce by promoting a mixture of specialty stores, restaurants, offices, and entertainment facilities designed to attract people and enhance the pedestrian experience (see image 12).

Bellflower/Alondra Mixed-Use Overlay Zone (BAMU)

Referred to as the Bellflower/Alondra Mixed-Use Overlay Zone, the intent of this overlay is to encourage opportunities to develop horizontally or vertically integrated mixed-use projects near transit, along corridors, and around activity centers. Commercial retail uses are encouraged on the ground floor with offices or residential uses on upper floors. Residential development is encouraged to enhance the availability of for-sale and rental housing to a variety of income levels. Walkability and access to public transit are paramount in site design. Multi-use projects need to be integrated into an overall project design and connected to other adjoining uses by plazas, promenades, and landscaped corridors. Density up to 40 du/ac and 1.0 FAR.

Mixed-Use Transit Overlay Zone (MUTOZ)

Allows for the highest density/intensity of development within the City, adjacent to the transit station. This overlay designation allows for joint development of residential, retail and services at the transit station itself, as well as a focused area to the north. The intent is to provide convenient access to transit for residents, as well as encourage businesses that cater to fast-paced, on-the-go transit riders seeking convenient goods and services. The focus of this overlay designation is on creating a pedestrian-friendly environment, including plaza areas and enhanced connectivity to the station and nearby downtown shops and restaurants. Uses and activities are designed together in an integrated fashion to create a dynamic urban environment. Uses may be mixed in a horizontal or vertical arrangement. Typical residential types could include

stacked flats with structured or underground parking. Residential development emphasizes quality and offers a variety of amenities. Density up to 100 du/ac and 2.0 FAR.

Mixed-Use Corridor Overlay Zone (MUCOZ)

Encourages lower intensity/density mixed-use development along the Flower Street corridor that is compatible with the low-scale character of the small storefront businesses and mix of residential housing types. This designation is intended to provide greater flexibility for properties to transition over time as both residential and local-serving retail and services fronting and activating the street. Suitable housing types along the corridor include row-towns, courtyard housing, and attached single-family product types up to 22 dwelling units per acre. The intent is to encourage the development of new residential and business uses in close proximity to the downtown. Density up to 22 du/ac and 0.5 FAR.

FIGURE 3.1 Illustration of the Bellflower Station Plaza



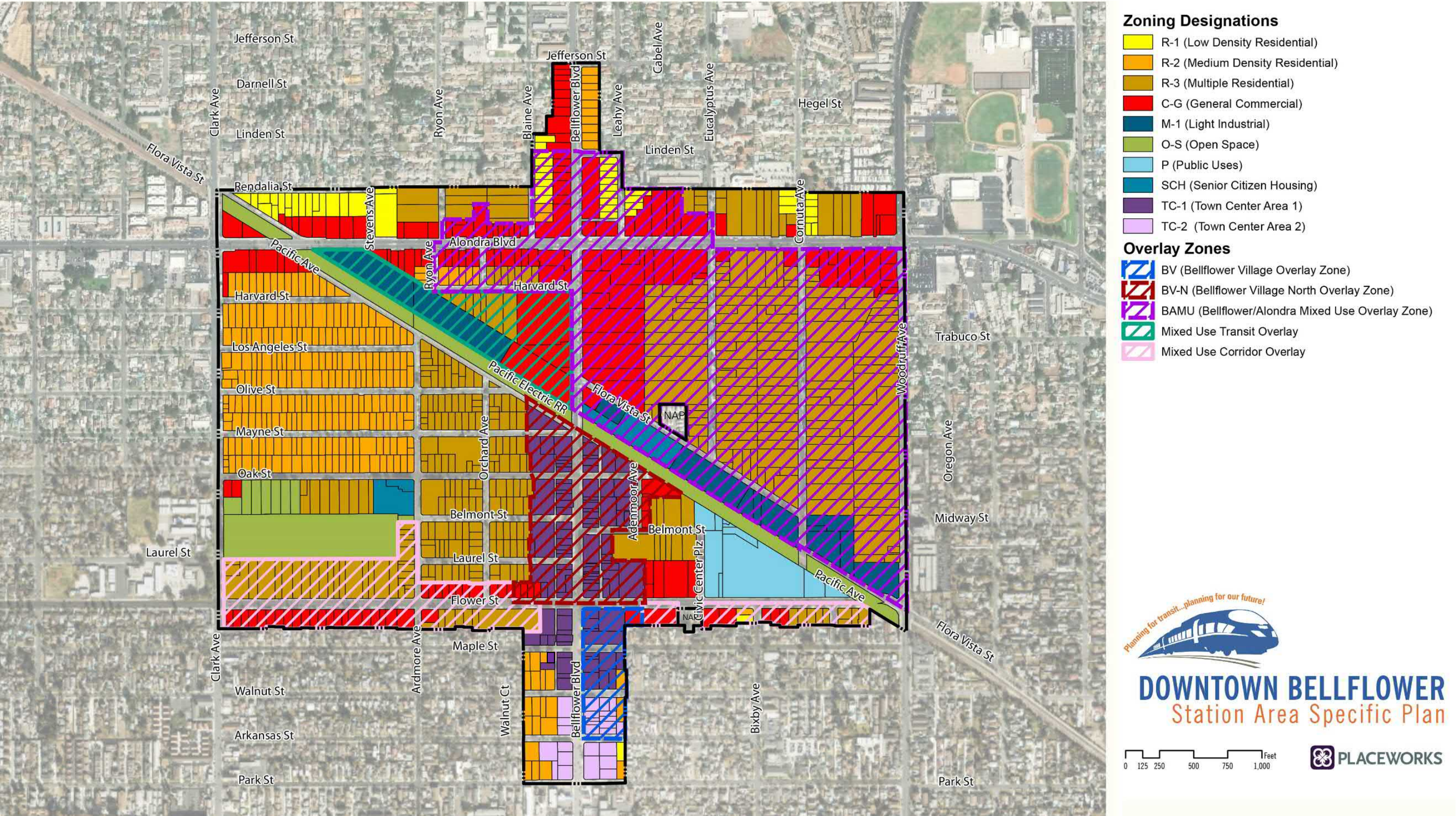
3.3 LAND USE PLAN

The Land Use Plan responds to the goals and objectives stated in Section 3.1 by providing a healthy balance of market-driven, private-sector uses with a wide range of public-serving uses. The Land Use Plan is shown on Figure 3.2 and the Statistical Summary is provided in Table 3.1.

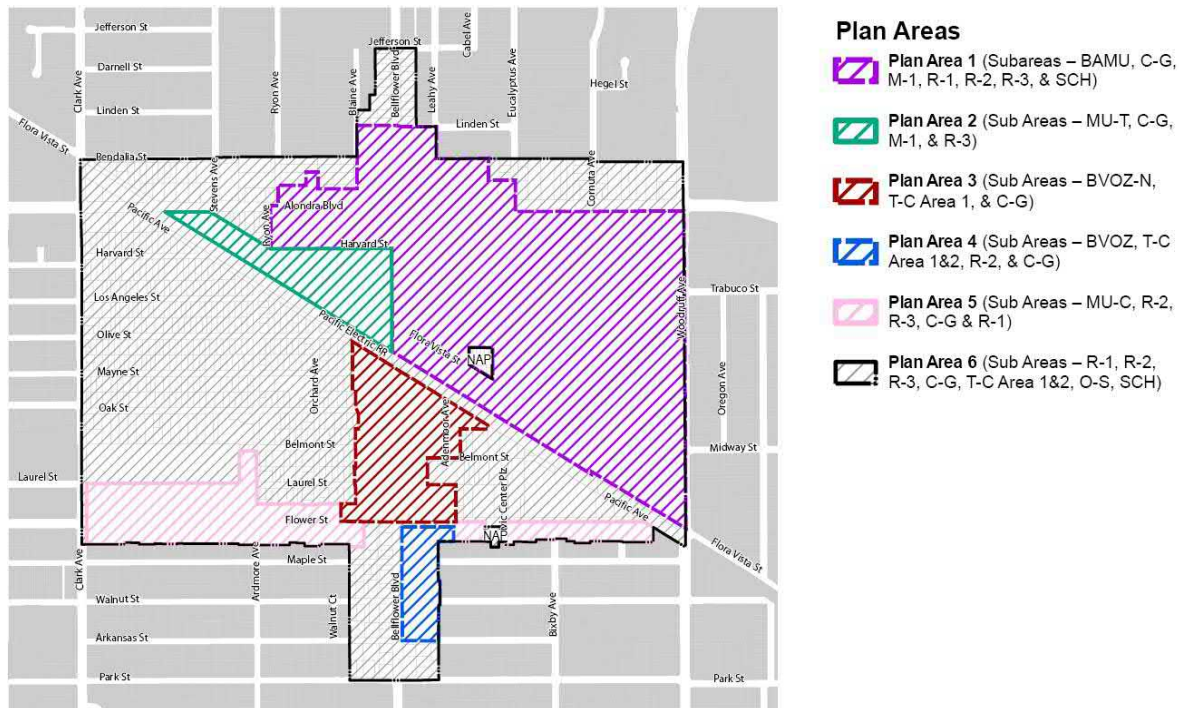
TABLE 3.1 Land Use Plan Statistical Summary

	NO OVERLAY	BVOZ	BVOZ-N	BAMU	CORRIDOR	TRANSIT	TOTAL
LAND USE DISTRICT	<i>Acres</i>	<i>Acres</i>	<i>Acres</i>	<i>Acres</i>	<i>Acres</i>	<i>Acres</i>	<i>Acres</i>
General Commercial (C-G)	14.48	0.36	1.52	31.79	8.38	7.21	63.73
Light Industrial (M-1)				9.54		6.64	16.18
Open Space (O-S)	25.34						25.34
Public Uses (P)	8.46						8.46
Low Density Residential (R-1)	6.43			3.16	0.18		9.77
Medium Density Residential (R-2)	43.72	0.51		1.36	0.13		45.72
Multiple Residential (R-3)	42.02			64.32	14.29	2.66	123.29
Senior Citizen Housing	1.76			2.78			4.53
Town Center 1	2.50	3.50	15.54				21.54
Town Center 2	3.51	1.62					5.13
ROW	40.44	1.09	6.98	21.26	6.67	0.00	76.44
Total (excluding ROW, excluding existing SP area)	148.22	5.98	17.06	112.95	22.97	16.51	323.69

FIGURE 3.2 Land Use Plan



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FIGURE 3.3 Planning Areas

3.4 KEY LAND USE AND URBAN DESIGN FEATURES OF THE SPECIFIC PLAN

3.4.1 ONGOING REVITALIZATION OF THE CORRIDORS

Over the past decade, the City has focused significant attention on revitalizing the stretch of Bellflower Boulevard within the historic downtown area. And the strategies for economic development and public realm improvements have paid-off with new restaurants, and vertical mixed-use development concentrated in the downtown area. With these trends and the future transit station, this Specific Plan focuses changes on the remaining key corridors within the study area – Bellflower Boulevard (north of the rail right-of-way), Alondra Boulevard, and Flower Street. Recommendations for right-of-way and streetscape improvements to make it easier for people to travel to/from the transit station are included in Chapter 5, Mobility. The improvements are integrated with the land use regulations so that higher-intensity residential and commercial development activates the street and further creates a pedestrian- and transit-friendly environment. This is accomplished through a system of overlay zones that are designed to incentivize this transition. The illustrative below shows how row-town housing can be integrated with commercial uses along Flower Street.



Image 13 New multifamily development



Image 14 Lively facades in the Bellflower Town Center



3.4.2 INFILL OPPORTUNITIES WITH OVERLAY ZONES

A key component of the Downtown Bellflower Station Area Specific Plan is the incentive for new mixed-use districts that are well-integrated with surrounding uses. This incentive is found in the system of Overlay Zones, which are designed to encourage higher density/intensity development based on development standards and design guidelines that emphasize form and the creation of an active, high-quality environment. These standards and guidelines address the essential elements for creating vibrant places, including orientation towards the street, outdoor dining and plazas, corner gateways and promenades, the siting of buildings and parking areas, and internal pedestrian connectivity. The Overlay Zones also allow for a greater range of housing types that are more suitable to a downtown and transit-oriented environment. Numerous infill opportunities exist within the study area. Figure 3.4, Opportunity Site Analysis, illustrates one example of how new development can be woven into the study area based upon the development standards and guidelines of the Bellflower/Alondra Mixed-Use Overlay (BAMU) and the Mixed-Use Transit Overlay.

FIGURE 3.4 Opportunity Site Analysis



PRODUCT EXAMPLES



3.4.3 JOINT DEVELOPMENT POTENTIAL AT METRO STATION

As Metro continues to plan for the final location/configuration of the station and associated parking, the City has identified the northwest corner of Bellflower Boulevard and the Pacific Electric right-of-way as a preferred location. This Specific Plan includes new overlay zones, including the Transit Overlay Zone – that encourages future joint development projects between Metro and a developer, such as residential, transit-supporting commercial, and parking for both transit-riders and residents. The Transit Overlay Zone extends beyond the future station itself and incorporates adjacent properties to encourage a larger, well-integrated development project. Access to the station and future residential component is a challenge given the existing development pattern. Therefore, internal circulation would require a solution involving multiple properties. Shown below is an illustrative demonstrating the potential for a joint development project between Metro and a developer, and redevelopment of adjacent parcels.

FIGURE 3.5 Conceptual Program of the Station and Transit Oriented Development

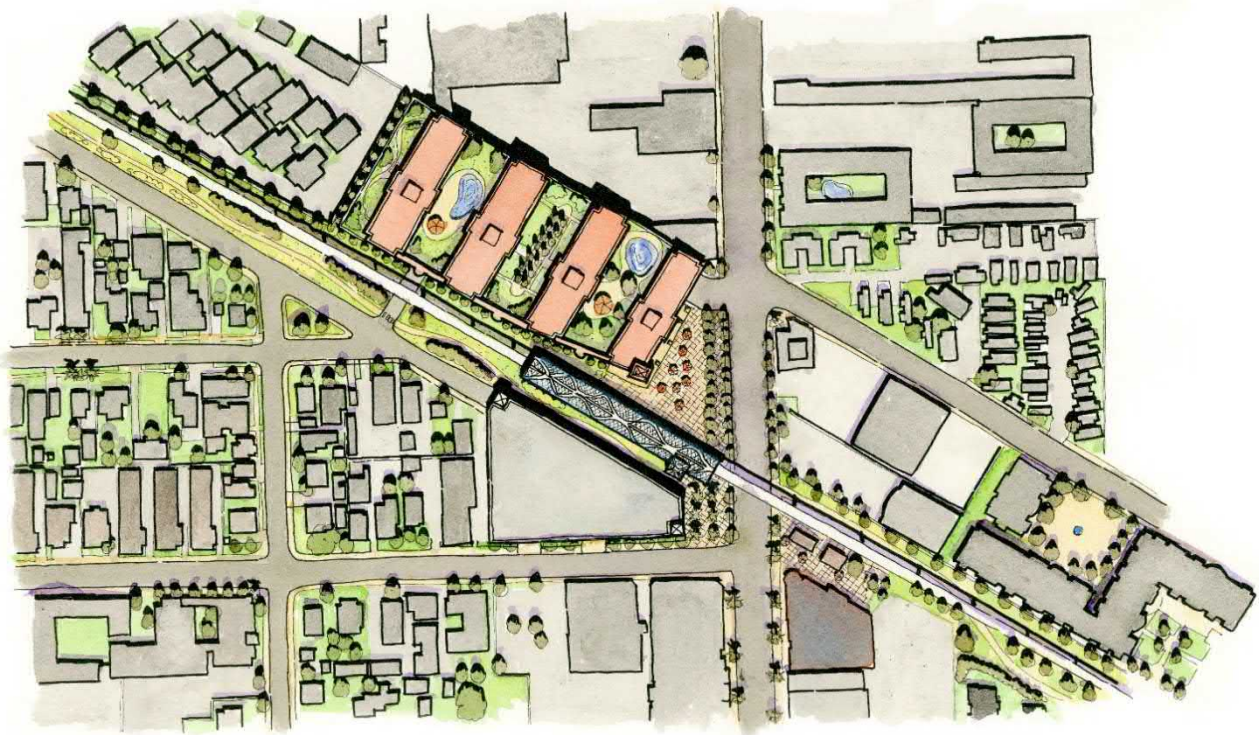




Image 15 Restaurants face Bellflower Boulevard; sidewalks have complementary trees and street furniture



Image 16 Facades are transparent and engage passersby

3.4.4 RELATIONSHIP OF BUILDINGS TO THE STREET

Building placement and orientation have an impact on the ways people experience a place – both on foot and in a car. Visually interesting buildings that are oriented to the street activate a corridor and contribute to the vibrancy of an area. Locating parking behind buildings, placing buildings closer to the street edge, and integrating outdoor spaces that are safe and welcoming are key design elements that are addressed in the development standards and design guidelines. In addition, the design of a building's ground floor entrance or facade, plus the landscape and hardscape setback areas determine if it is pedestrian-friendly, offers privacy and security where needed, and sets a quality image for a corridor. This Specific Plan establishes design guidelines for ground floor frontages to ensure that the transition between the proposed development and the public realm is consistent with the City's goal of providing a safer, pedestrian-friendly environment. A key benefit of this approach is that it makes transit access easier and more convenient.



Image 17 Pirate Park

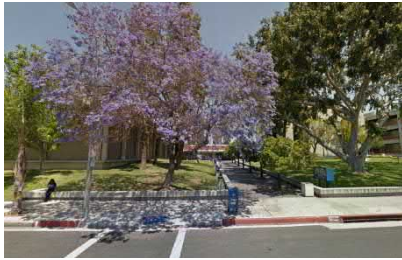


Image 18 Bellflower Civic Plaza



Image 19 Enhanced sidewalks and crosswalks on Bellflower Boulevard

3.4.5 OUTDOOR PLACES

Outdoor places, or people places, are spaces that extend the indoor environment to the outdoors. They are essential components of placemaking and contribute significantly to the character and activity level of a place. The outdoor places are created through the clustering of buildings and landscapes to define “green” rooms, plazas, courtyards, terraces, outdoor dining areas, roof top gardens and recreation areas. For the busy arterials within the study area – portions of Bellflower Boulevard, Alondra Boulevard and Flower Street, the location and design of outdoor spaces require special attention. Within the study area, there are many existing outdoor places that are used by the community. These include:

- » Pirate Park along Bellflower Boulevard, which is the transformation of a former parking area into a uniquely themed pocket park for residents and visitors to the downtown. (See image 17).
- » Bellflower Civic Plaza, which is part of the downtown and where annual movie nights and other City-sponsored community events are held. (See image 19).
- » Enhanced sidewalks on Bellflower Boulevard that encourage strolling and sitting; as well as outdoor dining areas that further draw people to the area. (See image 20).

Additional planned and proposed outdoor places within the study area include:

- » Proposed outdoor plaza associated with the future transit station, with retail and services adjacent to the plaza area. The final design of the station and other uses of the site must include a public plaza fronting onto Bellflower Boulevard that provides a convenient location for transit users to wait for trains, as well as creates a special gateway to Bellflower’s historic downtown.





PACIFIC ELECTRIC RAILWAY
PASSENGER AND FREIGHT STATION
SOUTHERN PACIFIC TICKETS

BELLFLOWER

WESTERN UNION

697

16336

RAILWAY EXPRESS AGENCY

TICKET OFFICE
MOTOR TRANSIT
STAGES

CITY OF BELLFLOWER

Pacific Electric Train Depot

Amtrak Project to Improve California
Partially Funded by the Rivers and Mountains Conservancy





CHAPTER 4

DEVELOPMENT STANDARDS

4. DEVELOPMENT STANDARDS

4.1 PURPOSE

This chapter provides detailed regulations for development and land uses within the Specific Plan area and describes how these regulations will be used as part of the City's review process. The Development Standard Code is intended to implement the vision and objectives as stated in Chapter 2 of this Specific Plan. These regulations were crafted to produce the types of development and revitalization envisioned by the City through a combination of traditional and form-based regulations.

4.2 APPLICABILITY

This chapter is applicable to all property within the Specific Plan area. These development standards supersede all provisions, standards, and requirements of the Bellflower Municipal Code ("BMC") zoning regulations, except in those instances where the Specific Plan development standards remain silent. In instances where these development standards do not address specific issues or other relevant considerations, then the regulations set forth in the BMC apply. In the event of a conflict between the BMC and the Specific Plan, the Specific Plan takes precedence over the BMC. The Director is authorized to provide administrative determinations regarding the Specific Plan. Such administrative determinations must be in writing and may be appealed in accord with the BMC.

The development standards for the Specific Plan are organized by zoning classification. For most classifications, the zoning standards from the BMC are applied. However, for two new designations, the Mixed-Use Transit Overlay Zone (MUTOZ) and the Mixed-Use Corridor Overlay Zone (MUCOZ), the standards have been tailored for this Specific Plan. The two new overlays provide optional, alternative standards to those in the underlying zone and apply only to new development not additions to existing buildings.

Development and use of property within this Specific Plan area must comply with this Specific Plan.

Allowed uses within the Special Plan area are regulated in Section 4.4, *Permitted Uses*.

4.3 DEVELOPMENT STANDARDS

4.3.1 REFERENCE TO EXISTING ZONING CLASSIFICATIONS

All development within the following classifications of the Downtown Bellflower Station Area Specific Plan must comply with the BMC:

- » Low Density Residential (R-1) - must comply with Chapter 17.24 of the BMC. (See Figure 4.1)
- » Medium Density Residential (R-2) - must comply with Chapter 17.28 of the BMC. (See Figure 4.1)
- » Multiple Residential (R-3) - must comply with Chapter 17.32 of the BMC. (See Figure 4.1)
- » Senior Citizen Housing (SCH) - must comply with Chapter 17.36 of the BMC. (See Figure 4.1)

FIGURE 4.1 Residential Zoning Classifications Key Map



- » General Commercial (C-G) - must comply with Chapter 17.44 of the BMC. (See Figure 4.2)
- » Light Industrial (M-1) – must comply with Chapter 17.52 of the BMC . (See Figure 4.2)
- » Open Space (O-S) – must comply with Chapter 17.64 of the BMC . (See Figure 4.2)
- » Public Uses (P) – must comply with Chapter 17.56 of the BMC . (See Figure 4.2)
- » Town Center 1 (TC-1) and Town Center 2 (TC-2) – must comply with Chapter 17.48 of the BMC . (See Figure 4.2)

FIGURE 4.2 Non-Residential Zoning Classifications

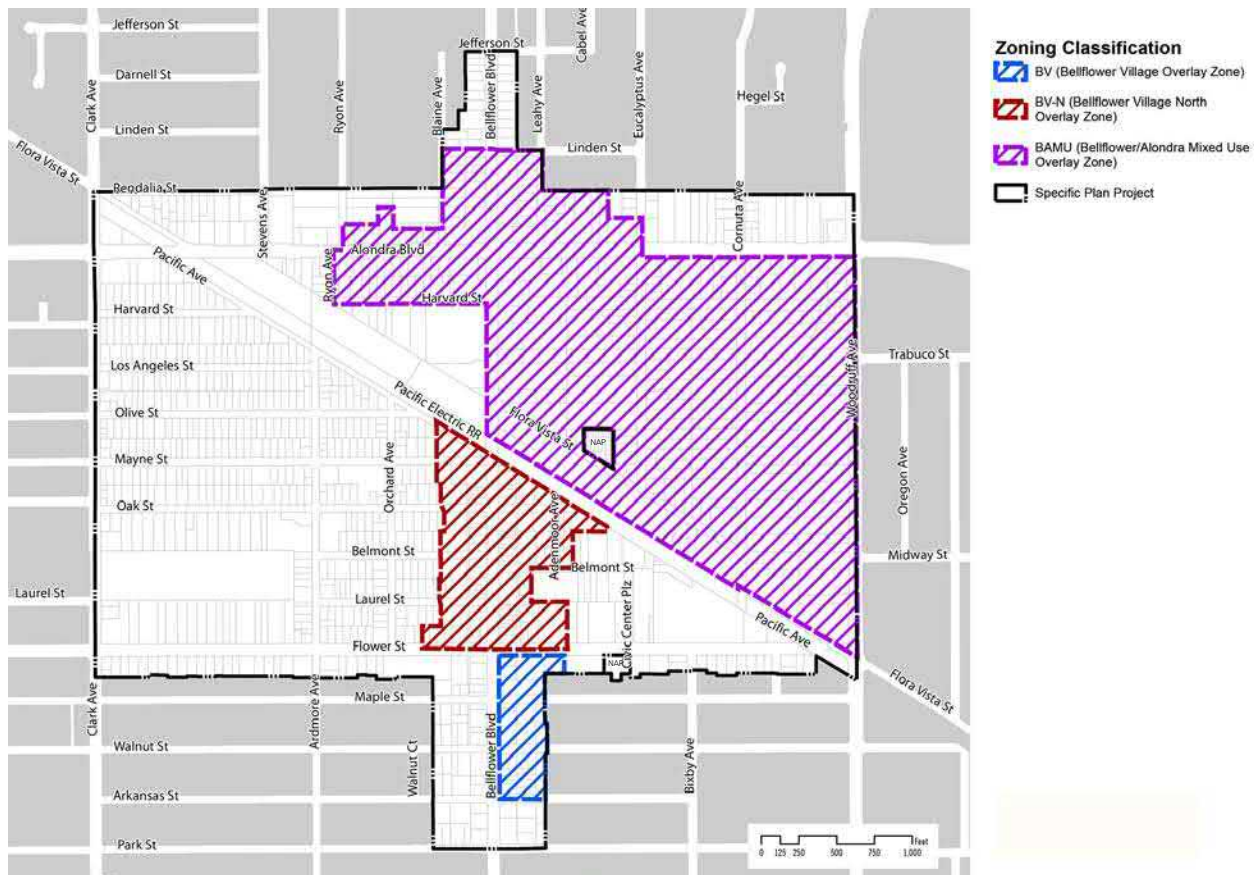


4.3.2 BELLFLOWER VILLAGE OVERLAY ZONE (BVOZ)

The Bellflower Village Overlay Zone was the first of three overlay zones previously adopted to incentivize private sector investment within the downtown area. Adopted in May 2008, the development standards and design guidelines of this Overlay Zone provide an alternative set of development standards and allowable uses beyond those contained in the underlying zoning designations. This Overlay Zone is based on the following objectives:

- » Encourage the recycling of existing developed properties into contemporary, vital and interesting development that will enrich downtown Bellflower;
- » Provide for more flexible regulatory procedures by which the objectives of the City's General Plan and regulations will be realized;
- » Encourage creative approaches to the use of land through variation in siting of buildings and the appropriate mixing of commercial land uses and activities;
- » Encourage redevelopment by promoting creative and unique economic opportunities to develop and use property;

FIGURE 4.3 Existing Overlay Zones Key Map



- » Create a unique and vibrant neighborhood within the downtown area of Bellflower that will encourage further private investment in the City;
- » Enhance the appearance of the community through creative design of buildings, structures, and facilities;
- » Eliminate and prevent the spread of blight;
- » Revitalize, redevelop, and upgrade those parcels within the Overlay Zone area; and
- » Strengthen the City's economic base.

The Bellflower Village Overlay Zone also supports transit-oriented development by increasing the number of stories allowed to three (and conditionally allowing up to four stories), and by allowing for multi-family residential in a mixed-use or stand-alone format. The development standards for this Overlay Zone must comply with Section 4.9 of this Plan.

4.3.3 BELLFLOWER VILLAGE OVERLAY ZONE-NORTH (BVOZ-N)

This Overlay Zone focuses on the heart of downtown Bellflower. Like the BVOZ Overlay Zone, the City determined that the underlying conventional zoning was not successful in promoting the transformation of the area into a creative, pedestrian-friendly, mixed-use environment. Adopted in May 2011, the Overlay provides an opportunity to utilize alternative zoning, including development standards and special design guidelines to achieve a mixed-use residential/commercial environment that activates the street, and incorporates additional public open spaces. Several projects have been developed applying this Overlay Zone, including the mixed-use development at 16632 Bellflower Boulevard. The urban design objectives for this area include:

- » Mixed-Use Theme. Promote development pursuant to the goals and objectives in the Mixed-Use Land Use designation of the Bellflower General Plan;
- » Create a Vibrant Commercial Destination. Create an attractive and unique neighborhood that would attract travelers and commuters who use Bellflower Boulevard as a transportation corridor, which will eventually provide access to the Bellflower Transit Station. This Overlay would also further the fulfillment of the "Connection of Plazas" concept adopted in the Downtown Bellflower Revitalization Vision Strategy (January 2003);

- » Protect the Adjacent Residential Neighborhoods. Through innovative and thoughtful design techniques, maintain and preserve the integrity of the adjacent single-family residential neighborhoods;
- » Village Design Theme. Allow for the use of varying architectural design themes but require that each design is thoughtfully blended and is compatible with adjoining and adjacent development. The urban design goal is to foster a vibrant, unique and exciting, pedestrian-friendly village atmosphere that creates a “sense of place”;
- » Incremental Development. Base the approval of individual incremental development projects on a single concept plan that provides defined ranges of flexibility to accommodate market changes;
- » Public Spaces. Provide for public spaces that serve both the commercial visitor as well as the local resident. For the development incentives to be available, proposed projects must include a public open space amenity as specified in the Overlay; and
- » Land Use Flexibility. Maintain the land use and development options so that as individual properties become available for development, there are choices between commercial, residential or mixed-use, as provided in current market conditions. Although live-work and mixed-use is not a strict requirement within the Overlay Zone, it is strongly encouraged.

The Bellflower Village Overlay Zone-North extends from Flower Street to the future transit station area. The existing standards and incentives also support transit-oriented development by increasing the number of stories allowed to four (and conditionally allowing up to five stories), and by allowing for multi-family residential in a mixed-use or stand-alone format. The development standards for this Overlay Zone must comply with section 4.10 of this Plan.

4.3.4 BELLFLOWER/ALONDRA MIXED-USE OVERLAY ZONE (BAMU)

The BAMU extends from the transit right-of-way on the north, to Alondra Boulevard. It was adopted in February 2014 and includes both development standards and tailored design guidelines. The BAMU Overlay Zone is intended to:

- » Stimulate economic development and reinvestment through design and development standards that allow property owners flexibility to adjust to market forces;
- » Create a pedestrian-oriented mix of uses with convenient access between area neighborhoods, housing, employment centers, public transit and retail services;
- » Accommodate land uses and patterns of development that can support multiple modes of transportation including public transit, bicycles, and walking that encourage and support the realization of transit-oriented development with the BAMU Overlay Zone;
- » Facilitate well-designed new mixed-use development projects that combine residential and nonresidential uses (e.g., office, retail, business services, personal services, public spaces and uses, other community amenities, etc.) and integrate public transit opportunities to promote a better balance of jobs and housing;

FIGURE 4.4 BAMU Development Opportunity



- » Ensure compatibility with adjacent existing single-family neighborhoods and harmonious integration with existing commercial areas;
- » Encourage the development of a unique character through design standards and guidelines that encourage features (including, without limitation, landscaping, street furniture, niche or linear parks, public places, courtyards, public transportation shelters) designed to integrate the public realm with development on adjacent private property; and
- » Provide additional property development opportunity while preserving existing property rights. This intent is achieved by providing additional development rights in compliance with this chapter, which property owners may exercise under certain conditions, while retaining all development rights conferred by the underlying zone to property owners in the BAMU Overlay Zone. Incentives and advantages include allowing a greater range and mix of uses and specifying more permissive dimensional specifications (including, without limitation, greater floor area ratio, lot coverage ratio, and height; reduced setbacks).

The BAMU supports transit-oriented development by increasing the number of stories allowed up to five directly across the street from the future transit station and encouraging mixed-use with residential in a vertical or stand-alone configuration. The Overlay also includes a residential sub-area along Eucalyptus Avenue and Cornuta Avenue that requires a minimum of 40 dwelling units per acre. The development standards for this Overlay Zone must comply with section 4.7 of this Plan.

4.4 MIXED-USE TRANSIT OVERLAY ZONE (MUTOZ)

Purpose and Intent

The Mixed-Use Transit Overlay is envisioned as an active living environment with easy access to both the transit station and Bellflower's downtown. The standards are intended to foster mixed-use environment with high-density residential, retail and services, an outdoor plaza/dining area, and parking for the transit station. This classification is limited to 9.9 acres adjacent to the future transit line, west of Bellflower Boulevard, on the north side of the tracks. Development within this area must provide convenient and direct pedestrian access from buildings and parking areas to the transit station, as well as sufficient curb-side drop-off and pick-up for buses and autos. Sites may be exclusively residential or non-residential or a mix of both with residential developed above or beside non-residential uses.

Development Standards

The following development standards regulate new site and building development by establishing standards for intensity, density, building height, open space, parking and other elements. All new buildings must comply with Chapter 7 (Design Guidelines).

FIGURE 4.5 Mixed-Use Transit Overlay Zone Key Map



TABLE 4.1 MUTOZ Development Standards

BUILDING PLACEMENT & FORM	MINIMUM	MAXIMUM
Building Density	Density measure as dwelling units per net acre (du/ac) and net square feet per dwelling unit (sqft/du)	
Residential	40 du/ac ¹ , 1,000 sqft/du	100 du/ac ¹ , 430 sqft/du
Building Intensity	Floor Area Ratio (FAR) ²	
Non-Residential	No Minimum	2.0 FAR ³
Setback Requirements (Residential & Non-Residential)		
Front Abutting Bellflower Blvd.	0 ft	15 ft ⁴
Front Abutting Harvard St.	10 ft	20 ft
Abutting Interior Road or Dr.	5 ft	15 ft
Abutting Rear	10 ft	No maximum
Abutting Interior Property Line	5 ft	No maximum
Permitted Encroachments within Setback Areas	<ul style="list-style-type: none"> • Walkway connections to building entrances. • Vehicular and bike access ways. • Public art displays, fountains, planters, outdoor seating areas, decorative trash receptacles, public plazas, or other similar amenities and attractive street furnishings that create public gathering areas. • Cornices, eaves, belt courses, sills, buttresses, fireplaces, windows, fully screened mechanical equipment, or other similar features may encroach no more than four (4) inches for each one (1) foot of the width of such side yard, and may extend or project into a required front or side yard not more than thirty (30) inches. • Awnings, canopies and arcades. • Balconies may encroach no more than three (3) ft. 	

Notes:

1. Additional density is permitted in accordance with BMC Chapter 17.41, *Density Bonus*.
2. See BMC Chapter 17.08.010 for a complete definition.
3. Podium and underground parking is not counted toward FAR.
4. Greater setbacks for entry plazas, courtyards or outdoor dining patios may be permitted, subject to the discretion of the Reviewing Authority.

TABLE 4.1 MUTOZ Development Standards

BUILDING PLACEMENT & FORM	MINIMUM	MAXIMUM
Permitted Encroachments within Setback Areas (cont.)	<ul style="list-style-type: none"> Outdoor dining may encroach into street-adjacent setback if a minimum 5-foot-wide clear and unobstructed sidewalk is provided. Outdoor dining located in the front or side of the building must be designed with either a low fence or wall, at least 40-inches high, when fronting a street. Outdoor dining located within a courtyard must also include a low fence or wall if fronting a street. 	
Building Heights	Number of stories does not include mezzanines or lofts	
Residential and Non-Residential	3 stories	7 stories
Ground Floor Ceiling Height – Residential	10 ft ¹	No Maximum
Ground Floor Ceiling Height – Non-Residential	12 ft ¹	No Maximum
Building Width along Frontage	Measured as percent of lot width	
Along Bellflower Boulevard	60%	No maximum
Permitted Building Frontage Types	See Chapter 7.3.2 for Frontage Design Guidelines	
Along Bellflower Boulevard	Shopfront, Forecourt, Arcade, Terrace	
Along Harvard Street	Shopfront, Forecourt, Arcade, Terrace	
SITE PLANNING	MINIMUM	MAXIMUM
Open Space and Landscaping		
Non-residential open space	15% of lot ²	No maximum
Residential Private and/or Common Open Space	100 sf per unit	No maximum
Public sidewalk along Bellflower Boulevard	Sidewalk must be improved with street trees, shrubs, and street furniture consistent with streetscape in Downtown Bellflower. Streetscape design must be coordinated with bus stops, and curb-side drop-off and pick-up zones	

Notes:

1. All developments must comply with Building Codes.
2. See Section 4.3.7 for Publicly-Accessible Open Space Standards.

TABLE 4.1 MUTOZ Development Standards

SITE PLANNING	MINIMUM	MAXIMUM
Direct Public Transit Connection	A direct pedestrian and bicycle connection must be provided to public transportation facilities, or amenities that service public transit facilities such as benches, informational kiosks, walkways, parking facilities or other similar amenities.	
Landscape Requirements within Setback Areas	<p>Setbacks abutting public rights-of-way, internal roads and alleys must be improved with landscaping and/or hardscape as follows:</p> <ul style="list-style-type: none"> » For ground-floor residential, the area between residential patios and the sidewalk/walkway must be fully landscaped. » For ground-floor commercial or live/work units, up to 80 percent of the setback area may be paved. » Landscape and fencing requirements of Chapter 17.48 apply. » Setback areas must be permanently maintained in a neat and orderly manner. <p>Required setbacks abutting interior property lines and setbacks between buildings must be landscaped with trees, other plantings, and walkways. Fences, walls, and hedges must comply with Chapter 17.48 of the BMC.</p>	
For ground-floor residential, the area between residential patios and the sidewalk/walkway must be fully landscaped.	See Section 4.3.8 for Parking Standards	
For ground-floor commercial or live/work units, up to 80 percent of the setback area may be paved.	Allowed within the buildable areas of the site. However, the ground floor of all street adjacent structures must be screened with a building space with a minimum interior depth of 30 feet. Facades along the upper floors must be designed with features to screen views of vehicles.	
Landscape and fencing requirements of Chapter 17.48 apply.	Entrance allowed along Bellflower Boulevard. Residential parking must be separated from retail and Metro parking via separate entrance or internal gate. Subterranean parking may also be allowed under streets and alleys with the approval of an encroachment permit.	

Notes:

All public right-of-way improvements are subject to review and approval by the Public Works Director and may require an encroachment permit.

4.5 MIXED-USE CORRIDOR OVERLAY ZONE (MUCOZ)

Purpose and Intent

The Mixed-Use Corridor Overlay Zone is envisioned as a walkable residential and commercial corridor that provides local services and employment along with multifamily living. The standards are intended to provide a single plan that guides the incremental transition into a pedestrian-friendly environment and provides property owners a mix of uses to flexibly reinvest within the area. This overlay includes 30.2 acres along the north and south side of Flower Street from Clark Avenue to west of Bellflower Boulevard and on the south side of Flower Street from west of Bellflower Boulevard to Woodruff Avenue. Development within this area must promote connections to the street to create an enjoyable and economically vibrant corridor. New uses should relate to each other and be compatible in use and form with adjacent residential neighborhoods. Sites may be exclusively residential or non-residential or a mix of both with residential developed above or beside non-residential uses.

FIGURE 4.6 Mixed-Use Corridor Overlay Zone Key Map



Development Standards

The following development standards regulate new site and building development by establishing standards for intensity, density, building height, open space, parking and other elements. All new buildings must comply with Chapter 7 (Design Guidelines).

TABLE 4.2 MUCOZ Development Standards

BUILDING PLACEMENT & FORM	MINIMUM	MAXIMUM
Building Density	Density measure as dwelling units per net acre (du/ac) and net square feet per dwelling unit (sqft/du)	
Residential	14 du/ac ¹ , 3,100 sqft/du	22 du/ac ¹ , 2,000 sqft/du
Building Intensity	Floor Area Ratio (FAR) ²	
Non-Residential	No Minimum	0.35 FAR ³
Setback Requirements (Residential & Non-Residential)		
Front Abutting Flower Blvd. west of Bellflower Boulevard	5 ft	15 ft ⁴
Front Abutting Flower Blvd. east of Bellflower Boulevard	5 ft	10 ft ⁴
Front Abutting Bixby Street	10 ft	20 ft
Front Abutting Ardmore Ave. and Orchard Ave.	5 ft	15 ft
Abutting Interior Road or Drive	5 ft	15 ft
Abutting Rear Property Line, not adjacent to single-family residential	10 ft	No maximum
Abutting Rear Property Line, Adjacent to single-family residential	20 ft	No maximum
Abutting Interior Property Line	5 ft	No maximum
Permitted Encroachments within Setback Areas	<ul style="list-style-type: none"> • Walkway connections to building entrances. • Vehicular and bike access ways. 	

Notes:

1. Additional density is permitted in accordance with BMC Chapter 17.41, Density Bonus.
2. See BMC Chapter 17.08.010 for a complete definition.
3. Podium and underground parking is not counted toward FAR
4. Greater setbacks for entry plazas, courtyards or outdoor dining patios may be permitted, subject to the discretion of the Reviewing Authority.

TABLE 4.2 MUCOZ Development Standards

BUILDING PLACEMENT & FORM	MINIMUM	MAXIMUM
Permitted Encroachments within Setback Areas (cont.)	<ul style="list-style-type: none"> Public art displays, fountains, planters, outdoor seating areas, decorative trash receptacles, public plazas, or other similar amenities and attractive street furnishings that create public gathering areas. Cornices, eaves, belt courses, sills, buttresses, fireplaces, windows, fully screened mechanical equipment, or other similar features may encroach no more than four (4) inches for each one (1) foot of the width of such side yard, and may extend or project into a required front or side yard not more than thirty (30) inches. Awnings, canopies and arcades. Outdoor dining may encroach into street-adjacent setback if a minimum 5-foot-wide clear and unobstructed sidewalk is provided. Outdoor dining located in the front or side of the building must be designed with either a low fence or wall, at least 40-inches high, when fronting a street. Outdoor dining located within a courtyard must also include a low fence or wall if fronting a street. 	
Building Heights	Number of stories does not include mezzanines or lofts	
Residential and Non-Residential	3 stories	7 stories
Ground Floor Ceiling Height – Residential	10 ft ¹	No Maximum
Ground Floor Ceiling Height – Non-Residential	12 ft ¹	No Maximum
Building Width along Frontage	Measured as percent of lot width	
Along Flower Street	60% ¹	No maximum

Notes:

1. All developments must comply with Building Codes.
2. See Section 4.3.7 for Publicly-Accessible Open Space Standards.

TABLE 4.2 MUCOZ Development Standards

SITE PLANNING	MINIMUM	MAXIMUM
Permitted Building Frontage Types	See 7.3.2 for Frontage Design Guidelines	
Along Flower Street	Shopfront, Forecourt, Terrace, Stoop, Porch	
Along All Other Streets	Shopfront, Forecourt, Terrace, Stoop, Porch ²	
Open Space and Landscaping		
Non-residential open space	15% of lot ¹	No maximum
Residential Private and/or Common Open Space	100 sf per unit, minimum dimension of 8 feet (width and length)	No maximum
Public sidewalk along Flower Street	Sidewalk must be improved with street trees, shrubs, and street furniture consistent with section 5.6 Street Trees and section 7.4 Public Realm Design.	
Landscape Requirements within Setback Areas	Setbacks abutting public rights-of-way, internal roads and alleys must be improved with landscaping and/or hardscape as follows: » For ground-floor residential, the area between residential patios and the sidewalk/walkway must be fully landscaped. » For ground-floor commercial or live/work units, up to 80 percent of the setback area may be paved. » Landscape and screening requirements of Chapter 17.48 apply. » Setback areas must be permanently maintained in a neat and orderly manner. Required setbacks abutting interior property lines and setbacks between buildings must be landscaped with trees, other plantings, and walkways. Fences, walls, and hedges must comply with Chapter 17.48 of the BMC	
Parking Standards	See Section 4.3.8 for Parking Standards	
Above Ground Parking Structure	Allowed within the buildable areas of the site. The ground and upper floors of all street adjacent structures must be designed with features to screen views of vehicles.	
Driveways	As individual properties redevelop, driveways and vehicular access must be minimized to the greatest extent possible, consolidated driveways are encouraged along Flower Street.	

Notes:

1. See Section 4.3.7 for Publicly-Accessible Open Space Standards
2. All public right-of-way improvements are subject to review and approval by the Public Works Director and may require an encroachment permit.

4.5.1 OPEN SPACE STANDARDS—PUBLICLY-ACCESSIBLE OPEN SPACE.

This section provides standards for publicly accessible open space areas in order to ensure a high level of pedestrian connectivity and activity between the public realm and the private realm.

- A. **Minimum Size.** All new nonresidential development must provide publicly accessible open spaces as a percentage of the total development site area as indicated in Sections 4.3.5 and 4.3.6 above.
- B. **Eligible Areas.** Publicly accessible open space areas must not include parking, driveway, or rear setback areas, but may include front and side setback areas if they are integrated into the overall design of the project.
- C. **Ground-Level Installation.** Plazas, courtyards, or other similar publicly accessible open space areas must be installed at ground level and must be incorporated into the design of the development.
- D. **Visibility and Accessibility.** Public open space areas must be visible and accessible from the public rights-of-way to engage the interest of pedestrians and encourage public use. They must also include necessary features to ensure sufficient site security and visibility (e.g., planting locations, appurtenances and lighting).
- E. **Landscaping and Hardscapes.** A combination of landscape and hardscape materials must be used in the design of these areas and must include the following components:
 - 1. Hardscape paving may include brick, stone, interlocking concrete pavers, textured concrete, and/or impressed patterned concrete. Hardscape elements may include, without limitation, seating areas, potted plant materials, water features, and public art installations.
 - 1. All plant materials must be in proportion to the height and mass of the building and be permanently maintained and irrigated.
- F. **Minimum Height to Width Ratios.** To achieve sunlight and air circulation in required publicly accessible open space areas, the following minimum height to width ratios must be provided:

1. Enclosed open space (i.e., open space that is enclosed on four (4) sides, such as a courtyard): 2 to 1 ratio. The required open space must have a width of at least one-half the height of the adjacent building facade (measured perpendicularly from the facade). This requirement applies to all sides of the required open space.
2. Open space that is open on one (1) or more sides: 3 to 1 ratio. The required open space must have a width of at least one-third the height of the adjacent building facade (measured perpendicularly from the facade). This requirement applies to all sides of the required open space.

G. Design Configuration.

1. Sharing of the required publicly-accessible open space ("quasi-public space") for nonresidential uses and the required common open space for residential uses may be allowed by the City when it can be demonstrated that the open space will provide direct benefit to residents of the project and the general public subject to the following limitations:
 - a. Up to thirty (30) percent of the required open space for residential uses in a horizontal mixed-use project may be provided as quasi-public open space within the nonresidential component of the project; or
 - b. Up to fifty (50) percent of the required open space for residential uses in a vertical mixed-use project may be provided as quasi-public open space within the nonresidential component of the project.
 - c. The minimum dimension (length and width) of shared common open space areas must be twenty (20) feet. These areas must be located at grade and be accessible for use by the general public.
 - d. Quasi-public open space areas may include common outdoor dining areas that are not used exclusively by any individual business.
 - e. Quasi-public open space areas are areas located on private property and accessible to the general public. These areas must include pedestrian-oriented

amenities, including enhanced seating, lighting, paving, landscaping, public art, water features, and other similar features deemed appropriate by the Director.

2. Publicly accessible open space areas must be located and configured as any one of the following:
 - a. Forecourt. The publicly accessible open space area is located along a recessed center section of the front facade of the building.
 - b. Front. The publicly accessible open space area is located along the street facing frontage of the building as illustrated.
 - c. "L" Shaped. The publicly accessible open space area is located along the front and side of the lot.
 - d. Paseo or Central Courtyard. The publicly accessible open space area is located on the side of the building or along a center pedestrian paseo or courtyard.

4.5.2 PARKING STANDARDS

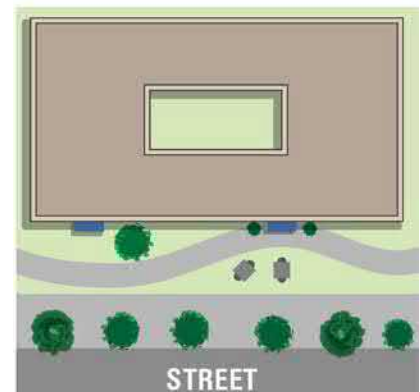
The Bellflower Station Area Specific Plan area will promote several smart growth strategies, such as mixed-use developments in the vicinity of a rail-served transit station and an accessible and well-connected bikeway and pedestrian network.

Parking is an important public and private asset and should be managed for efficient use. It is a finite resource and must be managed to provide convenient access and high usage. Parking utilization of approximately 85 to 95 percent is considered "full parking."

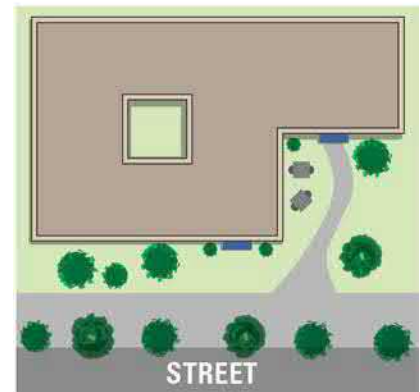
A Parking Management Plan was prepared to provide strategies to better manage parking resources, accommodate future parking needs with implementation of the Specific Plan, and provide appropriate funding mechanisms for the parking program. This technical report can be found in Appendix C. The report summarizes existing parking conditions in the Specific Plan area, evaluates long-term trends to assess future conditions and identify needs, and makes recommendations for planning and management of parking in the area.



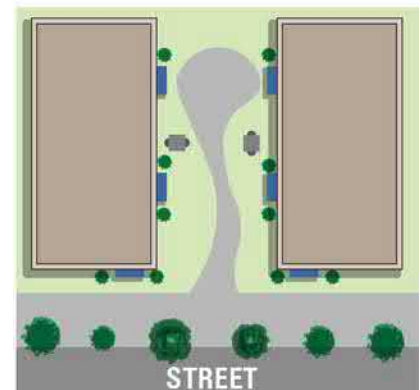
a. Forecourt



b. Front



c. "L" Shaped



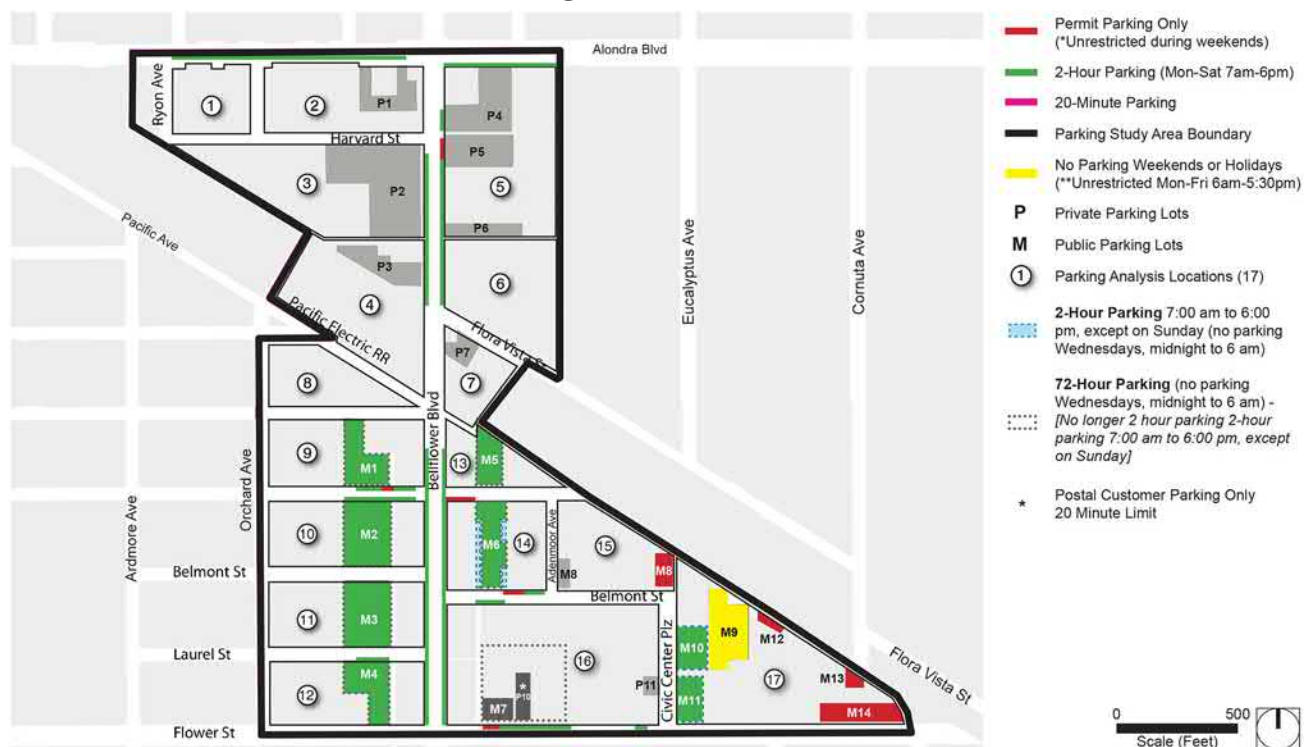
d. Paseo or Central Courtyard



A parking inventory occurred in August 2016 to determine the quantity of public and private parking spaces in the study area. The inventory for spaces included public parking lots owned by the City; the County of Los Angeles (the Courthouse); public on-street curbside parking along the street network; and private lots. The inventory for private spaces only included parking lots available to the public such as at retail centers, supermarkets, and banks. No private residential spaces were included in the inventory since they are generally not accessible to the public.

The study area was divided into 17 “parking areas” to evaluate parking conditions, trends, and needs in the Specific Plan area. Table 4.3 of this Plan summarizes the existing parking inventory in the Specific Plan area. It shows that the total number of spaces available in the Specific Plan area is 2,190, of which 1,005 are in public parking lots, 729 are in private parking lots, and the remaining 456 spaces are on-street (curbside) spaces.

FIGURE 4.7 Evaluated Parking Areas



conditions for increased travel via non-auto trips. As a result, parking utilization may be less as development occurs under the Specific Plan, compared to existing conditions. This allows for a tailored reduction of current parking requirements for the Mixed-Use Transit Overlay Zone and Mixed-Use Corridor Overlay Zone.

TABLE 4.3 **Parking Inventory**

PARKING AREA	CURBSIDE - BELLFLOWER BLVD.	CURBSIDE - OTHER	PUBLIC PARKING LOTS	PRIVATE PARKING LOTS	TOTAL
1	–	30	–	–	30
2	–	39	–	60	99
3	7	13	–	186	206
4	7	25	–	34	66
5	10	11	–	219	240
6	12	–	–	63	75
7	–	12	–	29	41
8	–	37	–	–	37
9	5	32	56	–	93
10	7	29	83	–	119
11	6	26	98	–	130
12	5	28	44	–	77
13	9	9	52	–	70
14	7	6	90	–	103
15	–	13	28	9	50
16	8	32	–	129	169
17	–	31	554	–	585
Total	83	373	1,005	729	2,190

Note: Private parking areas other than commercial areas that are available to the public are not included.

Table 4.4 below establishes the parking standards for the MUTOZ and MUCOZ. Where these standards are silent on a particular use, BMC Chapter 17.88, *Off-Street Parking Requirements*, applies.

TABLE 4.4 MUTOZ and MUCOZ Parking Standards

USE	MINIMUM	SPECIAL PROVISIONS
Mixed Use Transit		
Residential	Parking may be unbundled from the units to better manage available parking and to reduce housing costs for residents using transit or other means in-lieu of a private automobile.	
Studio Apartment	1 spaces / DU or as determined by the Director	Covered
1 Bedroom	1 spaces / DU or as determined by the Director	Covered
2 Bedrooms or more	2 spaces / DU or as determined by the Director	Covered
Guest	.15 spaces / DU or as determined by the Director	Required guest parking may be transferred to designated off-site parking locations or within non-residential parking area on the same site pursuant to the provisions of a DDA or similar agreement approved by the City Council
Non-Residential		
Retail	1 space / 400 SF	Required on-site parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council
Office	1 space / 600 SF	Required on-site parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council
Restaurant	1 space / 200 SF 1 space / 300 SF if no eating on premises	Required on-site parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council

TABLE 4.4 MUTOZ and MUCOZ Parking Standards

USE	MINIMUM	SPECIAL PROVISIONS
Mixed Use Corridor		
Residential	Parking may be unbundled from the units to better manage available parking and to reduce housing costs for residents using transit or other means in-lieu of a private automobile.	
Studio Apartment	1 spaces / DU or as determined by the Director	Covered
1 Bedroom	1 spaces / DU or as determined by the Director	Covered
2 Bedrooms or more	2 spaces / DU or as determined by the Director	Covered
Guest	.15 spaces / DU or as determined by the Director	Required guest parking may be transferred to designated off-site parking locations or within non-residential parking area on the same site pursuant to the provisions of a DDA or similar agreement approved by the City Council
Non-Residential		
Retail	1 space / 420 SF	Required on-site parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council
Office	1 space / 660 SF	Required on-site parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council
Restaurant	1 space / 200 SF 1 space / 300 SF if no eating on premises	Required on-site parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council

4.5.3 PARKING MANAGEMENT STRATEGIES FOR THE SPECIFIC PLAN AREA

The Parking Management Plan also includes recommendations for the City to consider in managing parking effectively and facilitating a walkable “park-once” environment. Parking needs to accommodate numerous users. Priority, however, should be given to residents, visitors and patrons who have downtown as a destination to live, shop, dine and recreate. The public parking

supply should be managed to achieve approximately 85 percent utilization to optimize usage of this public resource.

The recommendations consider and leverage the density and diversity of land uses, the transit station, and the multimodal circulation features in the Bellflower Station Area Specific Plan. They prioritize and value short-term visitor and patron access as a key element to make the specific plan area vibrant and economically vital. Recommendations are for the short term (1 to 2 years), medium term (2-5 years), and long term (over 5 years).

Short Term

Implement Time Limits. The City should manage parking to promote high turnover at the most desirable locations, especially curbside spaces along Bellflower Boulevard. Time limits increase turnover and convenience to users, promoting economic vitality. Time limits would also discourage long-term parking of employees in the most desirable areas, along Bellflower Boulevard from Mayne Street to Flower Street. These are the most desirable spaces in the downtown area, with the best accessibility to businesses along Bellflower Boulevard. Time limits would increase the parking turnover at these desirable spaces. As the parking occupancy increases because of development of the downtown area with more vibrant and dense uses, time limits could be expanded to the City-owned surface lots near Bellflower Boulevard (lots M1 to M6 in Figure 4). The City should keep time limits standardized to the extent feasible.

Promote Valet Parking Services and Designate Passenger Loading Areas. Technologies like ride services and micro transit help reduce parking demand. Such services, however, increase demand for pick-up and drop-off areas. The Specific Plan area will experience development of restaurant and entertainment uses, among others, that are heavily utilized by ride-hailing services, which will likely result in a reduction in parking demand and increased demand for passenger loading areas. Valet services would maximize parking use associated with restaurant and entertainment uses and are a strategy to control the demand for parking.

Medium Term

Adopt a Parking Occupancy Target. The parking occupancy target should be set to emphasize efficient use of resources and ensure parking availability for employees, residents, and visitors to the Specific Plan area. The public parking supply should be

managed to achieve approximately 85 percent utilization to optimize usage of this public resource.

Monitor Parking Supply. Parking management requires a commitment and ongoing monitoring, management, and evaluation. Parking utilization surveys should be conducted periodically to evaluate utilization and ensure that parking availability is maintained.

Promote Shared Parking for New Developments. Parking districts are particularly well suited to locations with multiple small property owners in a compact, pedestrian-oriented setting such as the Bellflower Station Area Specific Plan. The City already has several large public parking lots along Bellflower Boulevard south of the Pacific Electric right-of-way that consolidate parking in the area to serve multiple businesses. This setting provides good conditions to implement and expand the existing parking district to a larger area. For areas with larger parcels—such as large commercial uses along Bellflower Boulevard south of Alondra Boulevard—the City could review opportunities for land uses with different peak parking demands to promote shared parking agreements. The City should also work with Metro to balance the parking needs of transit users with needs of residents and visitors in the Specific Plan area, looking for opportunities for shared parking with the proposed transit station.

Promote Unbundled Parking in New Developments. Unbundled parking is the practice of selling or leasing parking spaces separately, rather than automatically including the parking spaces with the purchase or lease of the commercial or residential use. Unbundling parking manages parking demand by having residents only pay for the parking spaces they actually need. This is an efficient and fair practice, since occupants are not forced to pay for parking they do not need and can adjust their parking needs over time.

Signage. The Specific Plan area has opportunities to improve signage for off-street public parking locations and the parking structure near the Civic Center. Real-time parking information should be required for any new parking structures in the Specific Plan area. Access from Bellflower Boulevard to the Civic Center parking structure should be a pleasant walking experience with wide and well-lit sidewalks and pedestrian-scaled wayfinding signage.

Long Term

Update Parking Supply Ratios. Parking requirements may be reduced for future developments if parking occupancy surveys confirm lower utilization levels.

Promote Transportation Demand Management (TDM). Land use intensification and the development of the transit station with rail service will improve conditions to make TDM measures effective. The City can reduce parking demand by providing residents and workers in the Specific Plan area with transit benefits, access to car-share vehicles, bike parking, and other TDM strategies that would curb demand for parking. This would decrease parking demand, allowing for a reduction in parking ratios and greater increase in density in the Specific Plan area.

Convert Existing Parking to Other Uses. Parking occupancy throughout the Specific Plan area should be monitored. If there are underutilized parking lots, the City may convert the underutilized parking lots to open space, residential, office, and other uses. The proposed uses must comply with the Specific Plan zoning for the site and meet the objectives of the Specific Plan. Traffic, parking demand with the proposed uses, and other infrastructure conditions should be reviewed. From a parking standpoint, uses with different parking characteristics should be given preference to provide opportunities for shared parking.

4.5.4 NOISE ATTENUATION

Noise attenuation applies to any of the land use classifications that include residential or other noise sensitive development.

Applicants for new noise-sensitive development (e.g., residential) must demonstrate to the Director that all habitable rooms would meet the 45 dBA CNEL interior noise standard before the City issues building permits. This can be accomplished with enhanced construction design or materials, such as upgraded dual-glazed windows and/or upgraded exterior wall assemblies.

In addition, applicants must demonstrate that residential outdoor common areas and recreational areas within the 60 dBA CNEL noise contour of the transit line (which is the level considered normally acceptable for multifamily residential uses) will achieve 60 dBA CNEL to ensure compatibility with the ambient noise environment, or 65 dBA CNEL for areas where the acoustical analysis demonstrates that achieving a level of 60 dBA CNEL is clearly not feasible. Noise reduction measures could include

increased setback from the rail right-of-way, shielding with noise barriers, or placing outdoor noise-sensitive areas behind buildings.

4.6 PERMITTED USES

Permitted uses for each zone are identified in Table 4.5 below. Uses are permitted (P), permitted by Conditional Use Permit (CUP), Minor Conditional Use Permit (MCUP), or not permitted (—) in the MUTOZ and MUCOZ. Uses are subject to the special use or development regulations indicated in the “Notes” column. The following designations must comply with the BMC or applicable specific plan: Low Density Residential, Medium Density Residential, Residential Multiple, Senior Citizen Housing, Light Industrial, Town Center Area 1, Town Center Area 2, Commercial, Public, Open Space.

TABLE 4.5 MUTOZ and MUCOZ Allowable Uses

LAND USE OR ACTIVITY	NOTES	MUTOZ	MUCOZ
General Commercial			
Ambulance Service		—	P
Animal Care			
Animal/pet sales		—	P
Animal boarding & day care		—	CUP
Animal grooming, indoor only		—	P
Animal medicine/veterinary		—	P
Animal training		—	CUP
Animal/pet supplies, indoor only		—	P
Art Galleries/Museum		—	—
Athletic, health, and fitness clubs		P	P
Automobile Related Sales & Service			
Automobile repair & service		—	—
Auto customization & service		—	—
Auto parts & accessories		—	P
Automobile sales, new & used		—	—
Auto stereo/alarm installation		—	—
Automobile carwash		—	CUP

TABLE 4.5 MUTOZ and MUCOZ Allowable Uses

LAND USE OR ACTIVITY	NOTES	MUTOZ	MUCOZ
Tire Stores, New or Used		—	—
Banks and Savings and Loans			
Automated teller machines		P	P
Drive thru teller		—	—
Barbershops, Beauty & Nail Salons		P	P
Darts		P	—
Billiard parlors – primary use	Zoning Code § 17.44.190	—	P
Catering Services		—	P
Carpet, Flooring & Tile, Retail Only		—	P
Check Cashing & Cash Advance		—	—
Child Day Care	Zoning Code § 17.44.200	CUP	CUP
Copy Centers, Reproduction Centers		P	P
Drive Thru – Component to Retail		—	—
Dry Cleaners		P	P
Fortunetelling		—	CUP
Game Arcades		—	—
Interior Decorating Showrooms		—	P
Laundromats	Zoning Code 17.44.240	—	CUP
Locksmiths		—	P
Mailboxes, Mailroom, Postal Stores		P	P
Massage Establishments			
Acupressure spas		—	P
Day spas		—	P
Massage parlors		—	—
Registered massage schools		—	—
Mortuary		—	—
Optician, Optometrists, Optical Shops		—	P
Outdoor Vendor Carts		CUP	CUP

TABLE 4.5 MUTOZ and MUCOZ Allowable Uses

LAND USE OR ACTIVITY	NOTES	MUTOZ	MUCOZ
Pawn Shops		—	—
Photography Studios		—	P
Picture Frames – Sales/Service		—	P
Recycling Facilities, Stand-Alone		—	—
Sporting Goods		—	—
Storage, Wholesale or Retail		—	—
Tattoo Shops/Body Piercing		—	—
Upholstery Shops		—	—
Specialty Commercial			
Alcohol			
Cocktail lounge, bar, pub		CUP	—
On-site, accessory to food uses		CUP	CUP
Off-site sale, liquor, beer & wine		—	CUP
On-site, accessory all other uses		CUP	CUP
On-site brewery, micro-brewery, or winery in conjunction with other permitted activity		—	MCUP*
On-site tasting rooms		—	MCUP*
Antique Shops		—	P
Apparel and Clothing Stores		P	P
Appliance Stores – Large, Retail Only		—	—
Appliance Stores – Small, Retail Only		—	P
Art Stores/Artist Supplies		—	P
Bakeries, Retail Only		P	P
Bicycle Shops		P	P
Bookstores, New & Used		P	P
Camera & Photography, Retail		P	P
Candy & Confectionaries		P	P

TABLE 4.5 MUTOZ and MUCOZ Allowable Uses

LAND USE OR ACTIVITY	NOTES	MUTOZ	MUCOZ
Computer Games/Accessories – Sales		—	P
Convenience Stores/Mini Mart ¹		P	P
Costume Shops, Sales & Rental		—	—
Drug & Pharmacy		P	P
Dry Goods		P	P
Electronics/Computers – Retail		—	P
Food Uses – Retail (Non-Drive-Thru)			
Coffeehouse/tea		P	P
Delicatessens		P	P
Food court/Casual dining/Fast food		P	P
Grocery, fresh produce/meat		—	P
Health/organic/vitamins		P	P
Ice cream, yogurt, smoothies		P	P
Restaurants/cafés		P	P
Fabric/Linens		—	P
Florists/Floral		P	P
Furniture/ Home Goods		—	P
Gift Shops/Stationery/Greeting Cards		P	P
Gun Sales – Accessory Use		—	CUP
Hardware/Home Improvement			
Less than 3,000 sq. ft. floor area		—	P
3,000 sq. ft. floor area & greater		—	CUP
Hobby & Crafts Sales		P	P
Hotels		CUP	—
Internet/Computer Access		P	P
Internet gaming/café	Zoning Code § 17.44.220	—	CUP
Jewelry/Gems/Coins Sales		—	P
Leather Goods/Luggage		P	P

TABLE 4.5 MUTOZ and MUCOZ Allowable Uses

LAND USE OR ACTIVITY	NOTES	MUTOZ	MUCOZ
Medical Supplies, Retail Only		—	P
Music			
Instrument sales – new or used		—	P
Instrument repair – accessory		—	P
Lessons – accessory		—	P
Lessons – primary		—	CUP
Music supplies		—	P
Recorded music – new or used		—	P
Recording studio		—	CUP
Sound equipment sales		—	P
Office Supplies		—	P
Outdoor Sales Area		CUP	CUP
Paint & Wallcovering – Retail		—	P
Party Supply Stores		—	P
Patio/BBQ Furniture/Equipment		—	—
Shoe Stores, Including Repair and Shoe Shine		P	P
Smoke/Cigar Shop			
Electronic cigarette and/or electronic liquid shop		—	CUP
Surplus Stores (Army/Navy)		—	P
Thrift Store		—	—
Toy Store		—	P
Trophy Shop		—	P
Watch & Clock Sales/Repair		—	P
Commercial Office/Administrative			
Administrative/Professional Offices		P	P
Medical Offices		—	P
Residential			
Home Occupations	Municipal Code § 5.08.450	P	P

TABLE 4.5 MUTOZ and MUCOZ Allowable Uses

LAND USE OR ACTIVITY	NOTES	MUTOZ	MUCOZ
Live-Work (no ground floor residential)		P	P
Lofts		MCUP*	MCUP*
Mixed-Use (no ground floor residential)		P	P
Mobile Homes		—	—
Multiple-Family			
Attached		MCUP*	MCUP*
Detached		MCUP*	MCUP*
Senior Housing		—	—
Single-Family Detached Residences		—	P
Other Uses/Miscellaneous			
Public Assembly Uses			
Religious institutions	Zoning Code § 17.48.125	—	CUP**
Banquet/fraternal halls	Zoning Code § 17.48.125	—	CUP
Community centers	Zoning Code § 17.48.125	—	CUP
Parking Lots/Parking Structures		P	P
Public/Private Utility/Communication		—	CUP
Private Storage/Mini Storage		—	—
Open Space & Recreation			
Athletic fields		—	—
Bike rentals		P	P
Bowling alleys		—	P
Movie theaters		—	P
Skating rinks		—	P

Notes:

1. Only permitted in conjunction with mixed-use developments.

*MCUP: Building Design subject to Design Review Committee and use subject to notification process

**Less than 5,000 square feet of floor areas requires an MCUP.

4.7 BELLFLOWER/ALONDRA MIXED-USE (BAMU) OVERLAY ZONE

4.7.1 PURPOSE AND INTENT.

Purpose. The purpose of this chapter is to provide land use and development standards and regulations that implement the goals and policies of the Bellflower General Plan and other similar long-range planning documents to encourage the development of an integrated, mixed-use, pedestrian and transit friendly environment.

Intent. The BAMU Overlay Zone is intended to:

- A. Stimulate economic development and reinvestment through design and development standards that allow property owners flexibility to adjust to market forces;
- B. Create a pedestrian-oriented mix of uses with convenient access between area neighborhoods, housing, employment centers, public transit and retail services;
- C. Accommodate land uses and patterns of development that can support multiple modes of transportation including public transit, bicycles, and walking that encourage and support the realization of transit-oriented development with the BAMU Overlay Zone;
- D. Facilitate well-designed new mixed-use development projects that combine residential and nonresidential uses (e.g., office, retail, business services, personal services, public spaces and uses, other community amenities, etc.) and integrate public transit opportunities to promote a better balance of jobs and housing;
- E. Ensure compatibility with adjacent existing single-family neighborhoods and harmonious integration with existing commercial areas;
- F. Encourage the development of a unique character through design standards and guidelines that encourage features including, without limitation, landscaping, street furniture, niche or linear parks, public places, courtyards, public transportation shelters) designed to integrate the public realm with development on adjacent private property; and
- G. Provide additional property development opportunity while preserving existing property rights. This intent is achieved by providing additional development rights in compliance with this chapter, which property owners may exercise under

certain conditions, while retaining all development rights conferred by the underlying zone to property owners in the BAMU Overlay Zone. Incentives and advantages include allowing a greater range and mix of uses and specifying more permissive dimensional specifications (including, without limitation, greater floor area ratio, lot coverage ratio, and height; reduced setbacks).

Relationship to other City Plans and Documents. The City of Bellflower adopted its 2014-2021 General Plan Housing Element in 2013. The BAMU Overlay Zone is intended to implement adequate sites requirements adopted in the General Plan Housing Element.

Relationship to Other Related Plans and Projects. Draft plans from regional agencies show alignment options that pass through the City and include a station on Bellflower Boulevard just North of Mayne Street. The Greenway Trail (aka “West Branch Greenway Multi-Modal Transportation Corridor”) connects to this location as well. The potential future alignment of a transit system and the existing Greenway Trail will provide a high level of functional connectivity with future land uses and activities in the BAMU Overlay Zone.

4.7.2 APPLICABILITY

This section describes the applicability of BAMU Overlay Zone standards to a property when the property is located within two zones – a base zone (also referred to as the “underlying zone” established by the zoning map and the BMC - e.g., Multiple Residential (R-3), General Commercial (C-G), Light Industrial (M-1), or another within the BMC) and the BAMU Overlay Zone.

Relationship Between Base Zone Standards and Overlay Zone Standards. For property within the BAMU Overlay Zone, the regulations in this chapter allow mixed-use development as an alternative to the stand-alone commercial, office, or residential types of development allowed under the base (underlying) zone standards.

A. Base Zone Standards.

1. The underlying base zone provisions apply; unless the developer opts to develop the property under the BAMU Overlay Zone provisions.
1. New projects may be developed in compliance with the existing underlying base zone.

2. For legal nonconforming uses (i.e., uses that do not comply with the provisions of the base zone), BMC Chapter 17.92 (Nonconforming Buildings and Uses) applies.
- B. Option to Apply BAMU Overlay Zone Standards.
1. The owner or developer, with the owner's consent, of any property within the BAMU Overlay Zone may choose to develop in compliance with the standards and procedures in this chapter.
 2. Projects being developed under the BAMU Overlay Zone standards require Director's review. In granting the approval, the reviewing authority must find that the proposed development complies with this chapter.
- C. Other Applicable Regulations. Other applicable regulations can be found in section 4.8, Specific Use Development Standards.
- D. Effect of BAMU Overlay Zone Design Guidelines and BAMU Overlay Zone Community Design Framework and Public Improvements Plan. Projects within the BAMU Overlay Zone are subject to the regulations in the BAMU Overlay Zone Design Guidelines (the "Guidelines") and the BAMU Overlay Zone Community Design Framework and Public Improvements Plan (the "Plan"). If there is a conflict between the standards in this chapter and the Guidelines or the Plan, then the standards of this chapter applies.

4.7.3 DESCRIPTION OF BAMU OVERLAY ZONE AREA

- A. BAMU Overlay Zone General Description. The BAMU Overlay Zone applies to the area bounded by Alondra Avenue to the north (including properties north of Alondra Avenue at the intersection of Bellflower Boulevard), the Pacific Electric rail corridor to the south, Stevens Avenue to the West and Woodruff Avenue to the East, as shown on the Zoning Map. The BAMU Overlay Zone is intended to provide opportunities for the development of horizontally or vertically integrated mixed-use activity centers and corridors. Commercial retail uses are encouraged on the ground floor with offices or residential uses on upper floors. Residential development is encouraged to enhance the availability of for-sale and rental housing to a variety of income levels. Walkability and access to public transit are encouraged.

- B. BAMU Overlay Zone Residential Sub-Area (BAMU-R). The BAMU Overlay Zone Residential Sub-Area (BAMU-R) is the area bounded by Alondra Boulevard to the north, Flora Vista Street to the south, Eucalyptus Avenue to the west, and Woodruff Avenue to the east. BAMU-R provides for additional residential opportunities to implement the provisions of the 2014-2021 General Plan Housing Element, Program 25. The Residential Sub-Area permits the exclusive development of higher density residential development at a minimum of 40 du/ac as a matter of right. The BAMU Overlay Zone Residential Sub-Area (BAMU-R) will remain in effect for the 2014-2021 RHNA 5th Cycle Planning Period. The City may also revoke the provisions of the BAMU-R if the City meets its RHNA obligation before the conclusion of the 2014-2021 RHNA 5th Cycle Planning Period.

4.7.4 PERMITTED USES IN BAMU OVERLAY ZONE

Table 4.6 lists uses permitted (P), permitted by conditional use permit (C), and not permitted (–) in the BAMU Overlay Zone, subject to the special use or development regulations indicated in the “additional requirements” column.

Supplemental definitions for land uses currently not contained within this chapter are included in BMC Chapter 17.08.

TABLE 4.6 BAMU Allowable Uses

LAND USE OR ACTIVITY	BAMU	REFERENCES/NOTES
General Commercial		
Ambulance Service	—	
Animal Care		
Animal/pet sales	P	
Animal boarding & day care	CUP	
Animal grooming, indoor only	P	
Animal medicine/veterinary	P	
Animal training	CUP	
Animal/pet supplies, indoor only	P	
Art Galleries/Museums ¹	CUP	
Automobile Related Sales & Service		
Automobile repair & service	—	
Auto customization & service	—	
Auto parts & accessories	P	
Automobile sales, new & used	—	
Auto stereo/alarm installation	—	
Automobile carwash	—	
Banks and Savings and Loans		
Automated teller machines	P	
Drive thru teller	—	
Barbershops	P	
Beauty Shops & Nail Salons	P	
Billiards/Darts		
Billiard parlors – primary use	—	
Billiards – accessory, 2 tables max	P	
Billiards – accessory, more than 2	CUP	BMC § 17.44.190
Darts – accessory only	P	
Bridal Shops	P	
Catering Services	P	
Carpet, Flooring & Tile, Retail Only	P	

TABLE 4.6 BAMU Allowable Uses

Check Cashing & Cash Advance	—	
Child Day Care	CUP	BMC § 17.44.200
Collection Centers (Reverse Vending Machines Only - Ancillary to a Grocery-Retail [Non-Drive-Thru], Indoor Use Only), Subject to the Requirements of BMC § 17.44.350)	CUP	
Copy Centers, Reproduction Centers	P	
Drive Thru – Component to Retail	—	
Dry Cleaners	P	
DVD & Video Rental (no adult businesses)	P	
Fortunetelling	—	
Game Arcades	—	
Interior Decorating Showrooms	P	
Laundromats	CUP	
Locksmiths	P	
Mailboxes, Mailroom, Postal Stores	P	
Masseur-Masseuse, Massage Establishments, Acupressure Spas, Day Spas and Similar Uses	—	
Mortuary	—	
Museum/Art Galleries	P	
Optician, Optometrists, Optical Shops	P	
Outdoor Vendor Carts	CUP	
Pawn Shops	—	
Photography Studios	P	
Picture Frames – Sales/Service	P	
Sporting Goods	P	
Storage, Wholesale or Retail	—	
Tattoo Shops/Body Piercing	—	
Tire Stores, New or Used	CUP	
Tuxedo Shops, Sales & Rental	P	
Upholstery Shops	CUP	

TABLE 4.6 BAMU Allowable Uses

Specialty Commercial		
Alcohol		
Cocktail lounge, bar, pub	CUP	
On-site, accessory to food uses	CUP	
Off-site sale, liquor, beer & wine	CUP	
On-site, accessory all other uses	CUP	
Antique Shops	P	
Apparel and Clothing Stores	P	
Appliance Stores – Large, Retail Only	P	
Appliance Stores – Small, Retail Only	P	
Art Stores/Artist Supplies	P	
Bakeries, Retail Only	P	
Bicycle Shops	P	
Bookstores, New & Used	P	
Camera & Photography, Retail	P	
Candy & Confectionaries	P	
Computer Games/Accessories – Sales	P	
Convenience Stores/Mini Mart	CUP	
Costume Shops, Sales & Rental	P	
Drug & Pharmacy	P	
Dry Goods	P	
Electronics/Computers – Retail	P	
Food Uses – Retail (Non-Drive-Thru)		
Coffeehouse/tea	P	
Delicatessens	P	
Food court/fast food	P	
Grocery, fresh produce/meat	P	
Health/organic/vitamins	P	
Ice cream, yogurt, smoothies	P	
Restaurants/cafés	P	
Fabric/Linens	P	

TABLE 4.6 BAMU Allowable Uses

Florists/Floral	P	
Furniture/Home Goods	P	
Gift Shops/Stationery/Greeting Cards	P	
Gun Sales – Accessory Use	CUP	
Hardware/Home Improvement		
Less than 3,000 sq. ft. floor area	P	
3,001 sq. ft. floor area & greater	CUP	
Hobby & Crafts Sales	P	
Hotels & Motels	CUP	BMC § 17.44.240
Internet/Computer Access		
Internet café – primary activity	P	BMC § 17.44.220
Internet – accessory, 3 or less	P	
Internet – accessory, 4 or more	P	
Internet access only – wireless	P	
Internet gaming	CUP	
Jewelry/Gems/Coins Sales	P	
Leather Goods/Luggage	P	
Liquor Stores (see Alcohol)	CUP	
Medical Supplies, Retail Only	P	
Microbrewery – On-Site (see Alcohol)	CUP	
Music		
Instrument sales – new or used	P	
Instrument repair – accessory	P	
Lessons – accessory	P	
Lessons – primary	CUP	
Music supplies	P	
Recorded music – new or used	CUP	
Recording studio	CUP	
Sound equipment sales	P	
Office Supplies	P	
Outdoor Sales Area	CUP	

TABLE 4.6 BAMU Allowable Uses

Paint & Wallcovering – Retail	P	
Party Supply Stores	P	
Patio/BBQ Furniture/Equipment	P	
Shoe Stores, Including Repair	P	
Smoke/Cigar Shop	CUP	
Electronic cigarette and/or electronic liquid shop	CUP	
Surplus Stores (Army/Navy)	P	
Thrift Store	—	
Toy Store	P	
Trophy Shop	P	
Watch & Clock Sales/Repair	P	
Commercial Office/Administrative		
Administrative/Professional Offices	P	
Accountant/tax preparer	P	
Administrative offices, general	P	
Architecture/designer	P	
Attorneys/legal services	P	
Computer programming software	P	
Consulting offices	P	
Engineer	P	
Financial services	P	
Insurance	P	
Real estate	P	
Travel agencies/ticket agencies	P	
Medical Offices		
Chiropractic	P	
Dental	P	
General & specialty medical	P	

TABLE 4.6 BAMU Allowable Uses

Residential		
Home Occupations	P	BMC § 5.08.450
Live-Work (no ground floor residential)	P	
Lofts	P	
Mixed-Use (no ground floor residential)	P	
Multiple-Family		
Attached	P	
Detached	P	
Senior Housing	P	
Single-Family Residences	P	
Other Uses/Miscellaneous		
Public Assembly Uses (such as fraternal halls/clubs, health/fitness centers, religious facilities, dance studios, karate/martial arts studio and theaters) ¹	CUP	BMC § 17.48.125
Parking Lots/Parking Structures	P	
Public/Private Utility/Communication	P	
Private Storage/Mini Storage	—	
Swap Meet, Indoor & Outdoor	—	

Notes:

1. Art galleries, museums, and religious facilities may be allowed with a Minor Conditional Use Permit if the total gross floor area proposed is less than 5,000 square feet.

4.7.5 PERMITTED USES IN BAMU RESIDENTIAL SUB-AREA OVERLAY ZONE

Table 4.7 lists uses permitted (P), permitted by conditional use permit (C), and not permitted (—) in the BAMU Residential Sub-Area (BAMU-R), subject to the special use or development regulations indicated in the “additional requirements” column. Uses within the BAMU Residential Sub-Area are reserved for exclusive residential development.

Supplemental definitions for land uses currently not contained within this chapter are included in BMC Chapter 17.08.

Uses Not Listed. In order to ensure that the BAMU Overlay Zone permits all similar uses, if a use is not specifically listed as permitted, permitted with a conditional use permit, or prohibited, then the Director must determine if the proposed use is similar to a use listed in the above matrix.

The determination of the Director is based upon the evaluation of the characteristics of the proposed use with the goals, objectives and policies of the General Plan, as well as with the goals and objectives of the Bellflower Village Overlay Zone.

The determination of the Director may be appealed pursuant to the provisions of BMC Chapter 17.

TABLE 4.7 BAMU Residential Sub-Area Allowable Uses

LAND USE OR ACTIVITY	BAMU	REFERENCES/NOTES
Residential		
Home Occupations	P	BMC § 5.08.450
Live-Work (no ground floor residential)	P	
Lofts	P	
Mixed-Use (no ground floor residential)	P	
Multiple-Family		
Attached	P	
Detached	P	
Senior Housing	P	
Transitional and Supportive Housing	P	BMC § 17.16.180

4.7.6 DEVELOPMENT REGULATIONS.

This section provides development intensity and standards that govern development on properties specifically located in the BAMU Overlay Zone. See Table 4.8, BAMU Overlay Zone Development Density & Intensity, and Table 4.10, BAMU Development Standards, as well as related illustrations. For the purpose of this Specific Plan, mixed-use projects must comply with nonresidential standards when no mixed-use standards exist.

- A. Exception BAMU Residential Sub-Area. Development standards in the BAMU does not apply to uses within the Residential Sub-Area. The Residential Sub-Area must comply with the standards and provisions provided in Table 4.9 of this title.

TABLE 4.8 BAMU Overlay Zone Development Density & Intensity

DEVELOPMENT FEATURES	
Density Standards	Maximum density for residential uses expressed as dwelling units per net acre.
Residential Uses ²	40 du/ac
Intensity Standards	Maximum floor area ratio (FAR) for nonresidential uses.
Nonresidential Uses ¹	1.0 FAR

Notes:

1. Podium and underground parking is not counted toward floor area ratio (FAR).
2. For residential uses in the BAMU Residential Sub-Area, 40 du/ac is the minimum requirement.

TABLE 4.9 Residential Sub-Area Development Regulations

DEVELOPMENT FEATURES	
Density Standards	Minimum density for residential uses expressed as dwelling units per net acre
Residential Uses ¹	40 du/ac
Intensity Standards	Maximum floor area ratio (FAR) for nonresidential uses
Nonresidential Uses	Not permitted
Site Area	Minimum lot area for residential uses
	10,000 square feet
Height of Primary Structure	Maximum permitted height for residential structures
	4 stories or 50 feet, whichever is less
Yards	Yard calculation are determined by the following formula
Front	20 feet

TABLE 4.9 Residential Sub-Area Development Regulations

Interior Side	2-story: 10 feet 3 or more stories: 15 feet
Street Side	10 feet, or 10% of lot width whichever is greater, up to 20 feet
Development Features	
Rear	2-story: 20 feet 3 or more stories: 25 feet
Alley	N/A
Building Separation	Minimum separation distance between residential structures
	15 feet
Building Bulk	
	Building bulk requirements must comply with the standards and provisions of the "2/3" rule set forth in BMC § 17.32.100
Usable Open Space	
Private Open Space	150 square feet, with a minimum dimension of 8 feet
Group Open Space	A minimum of 25% of the total land area
Driveways	Minimum driveway width
	Minimum of 24 feet. Refer to BMC §17.88.020 for additional standards
Off-Street Parking	Minimum standards for off-street parking
	2 covered parking spaces for first 2 bedrooms of a dwelling unit
	1 parking space must be provided for each bedroom after the first 2 bedrooms
	1 guest unit must be provided for each unit for designated use of visitors
Minimum Floor Area	Minimum floor area by unit type
	Bachelor and single: 500 square feet
	One-bedroom: 700 square feet
	Two-bedroom: 900 square feet
	Three-bedroom or more: 1,100 square feet
Other Standards	For standards and provisions not described herein, the standards and provisions of BMC Chapter 17.32 applies

Notes:

1. Density standards are minimum standards.

TABLE 4.10 BAMU Overlay Zone Development Standards

DEVELOPMENT FEATURES	
Building Placement Standards	
Build-To-Zone	The area between the minimum and maximum setbacks within which the principal building's front facade (building facade line) is to be located. See "Build-to-Zone" in BMC Chapter 17.08 (Definitions). See Figure 4.8
Front (C)	Maximum floor area ratio (FAR) for nonresidential uses.
Along Alondra Blvd & Bellflower Blvd	0 - 15 ft
Front (D)	
All Other Streets	0 - 15 ft
Street Side Setback (E)	0 - 15 ft
Development Features	
Setback	Minimum and maximum required setbacks. See Figure 4.9
Front Setback (F)	
Along Alondra Blvd & Bellflower Blvd	0 ft (min); 15 ft (max)
Front Setback (G)	
All Other Streets	0 ft (min); 15 ft (max)
Street Side Setback (H)	0 ft (min); 15 ft (max)
Interior Side Setback (2) (I)	5 ft (min); No max
Rear Setback (2) (J)	10 ft (min); No max
Building Frontage Length	% of building built. See "Build-To-Zone" in Chapter 17.08 (Definitions). See Figure 4.10
Within 300 ft of Street Intersections (K)	65%
Over 300 ft from Street Intersections (L)	50%
Building Standards	
See "Underground Levels" and "Mezzanines/Lofts" in BMC Chapter 17.08 (Definitions). See Figure 4.11	
Number of Stories (3) (M)	5 max
Maximum Height (3) (N)	60 ft
Underground Levels (O)	Allowed
Mezzanines/Lofts (4) (P)	Allowed

TABLE 4.10 BAMU Overlay Zone Development Standards

• Building Frontage Types	See “Frontage Type Standards” in section 4.7.6 (Development Regulations). See Figure 4.11
Along Alondra Blvd, Bellflower Blvd, Flora Vista Street within 300 ft of Intersections (Q)	Live-Work
	Office
	Storefront
Elsewhere (R)	Live-Work
	Office
	Residential
	Storefront
Site Planning Standards	
Parking Standards	See Figure 4.12 (Parking Standards)
Surface Parking (S)	20 ft min setback from front lot line; 15 ft min setback from side lot line
Garage/Tuck-Under Parking (T)	Prohibited along front lot lines
Underground/Podium Parking (U)	Allowed beneath building footprint
Above-Ground Parking Structure (5) (V)	Allowed if screened from views from public right-of-way and adjacent single-family residential districts
Open Space Standards	
Publicly Accessible Open Space (nonresidential)	See section 4.7.11(Open Space Standards – Publicly Accessible Open Space)
	15% of net lot area

Notes:

1. Podium and underground parking is not counted toward floor area ratio (FAR).
2. For residential uses in the BAMU Residential Sub-Area, 40 du/ac is the minimum requirement.

FIGURE 4.8 **Build-to-Zone Standards**

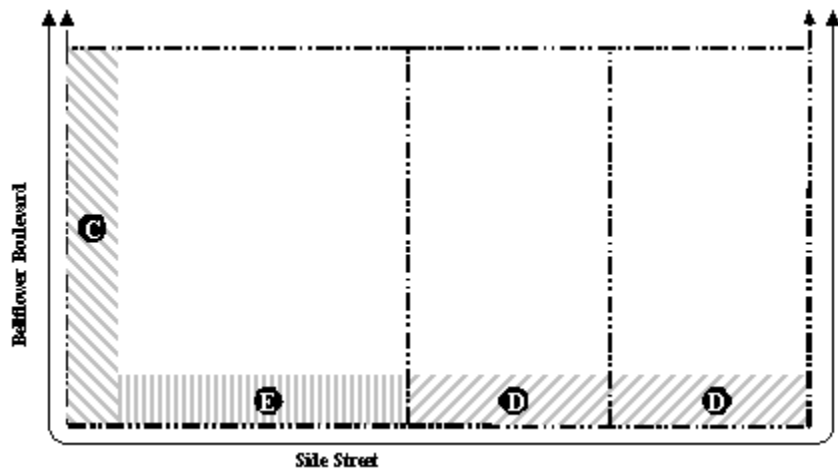


FIGURE 4.9 **Setback Standards**

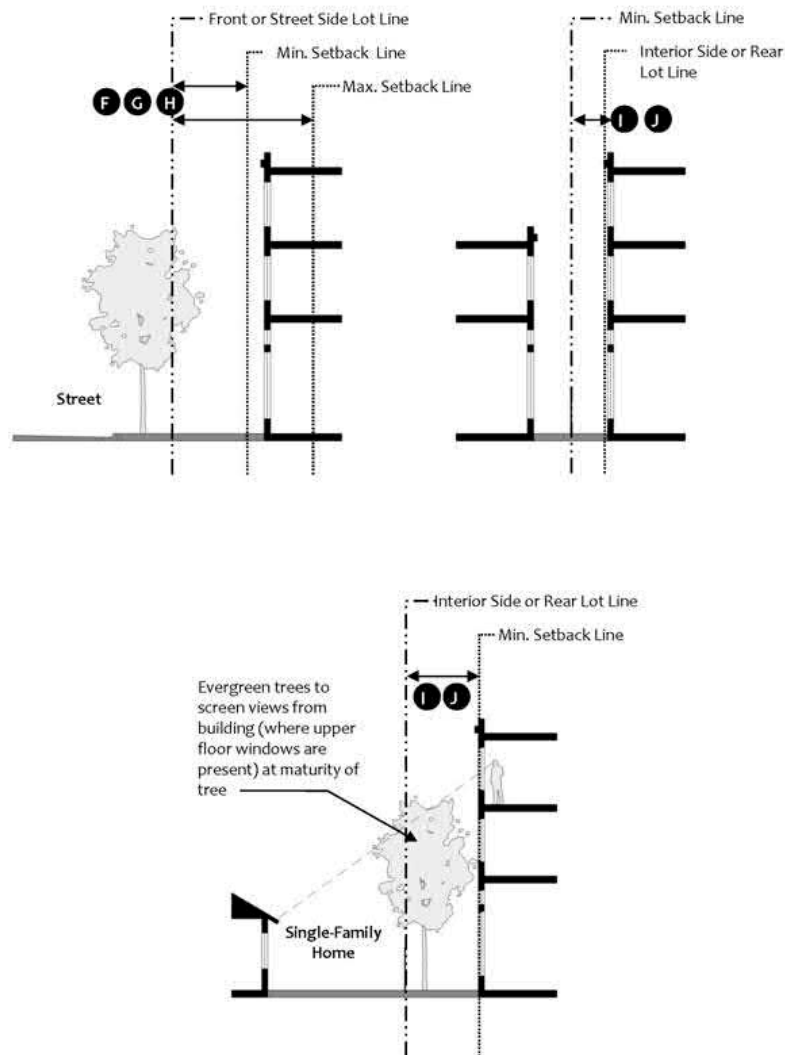


FIGURE 4.10 Building Frontage Length

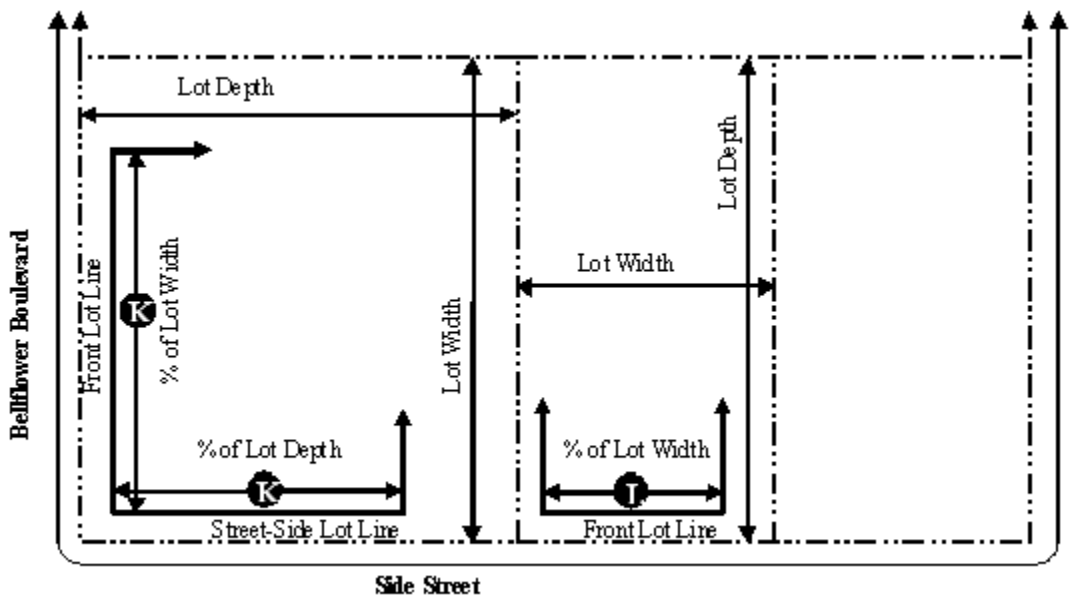


FIGURE 4.11 Building Standards and Building Frontage Types

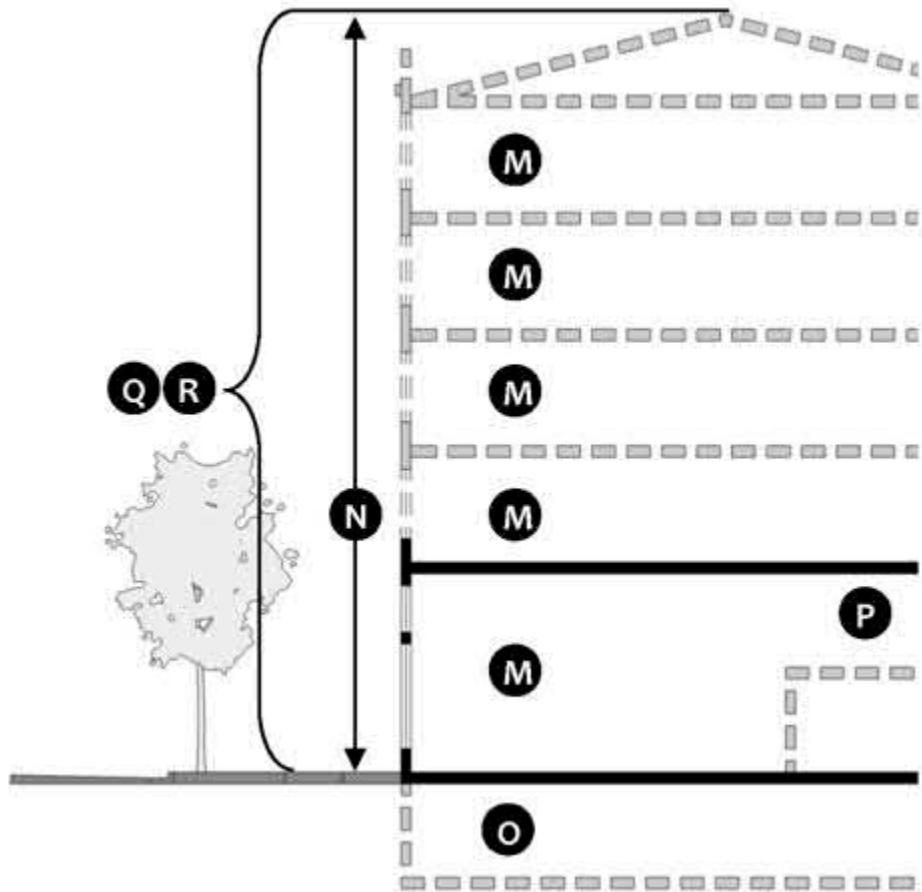
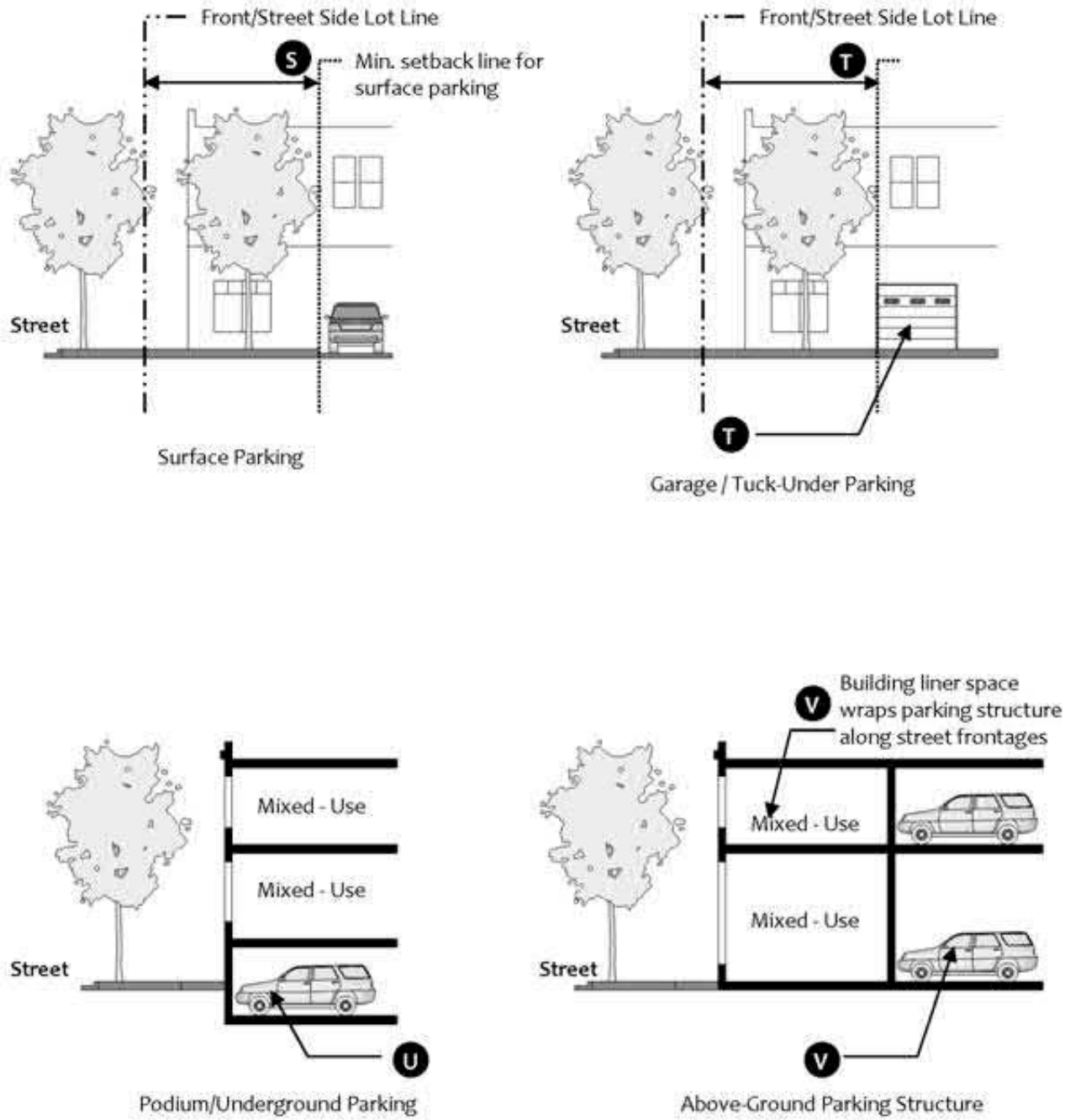


FIGURE 4.12 **Parking Standards**



4.7.7 FRONTAGE TYPE STANDARDS

This section provides frontage type standards for buildings in the BAMU Overlay Zone. In this Specific Plan, Live/Work is defined as follows:

Live/Work. A dwelling unit that incorporates non-residential uses. Permitted non-residential live/work uses are restricted to permitted uses within the zone in which it is located. Live/Work uses must comply with all of the following:

- A. Live/Work units must be internally accessible between the residential area and the non-residential area. The non-residential area must be directly accessible to a non-resident from the ground level via an entry/exit separated from a residential entry/exit;
- B. An employee of the non-residential use must reside in the dwelling unit. However, the non-residential use may have employees that do not reside within the dwelling unit;
- C. Live/Work uses must observe similar operational and delivery hours and walk-in/client visits as other permitted uses within the zone in which it is located.
- D. Outdoor storage is not permitted;
- E. Live/Work uses cannot store or generate hazardous materials or employ hazardous processes;
- F. Noise, vibration, dust, odors, fumes, smoke, heat, electrical interference or other similar nuisances from Live/Work activities cannot be perceived beyond the individual unit; and
- G. Live/Work activities cannot increase pedestrian or vehicle traffic beyond that ordinarily associated with the zone in which it is located nor can it reduce the number of required off-street parking spaces available for use.

Types of Frontages.

- A. **Live-Work/Office Fronts.** A frontage that reinforces both residential and work activities that can occur in the building. The elevation of the ground floor is located at or near the grade of sidewalk to provide direct public access to the building. Entrances and windows are provided on the front of the facade to provide eyes on the street and direct sidewalk access to commercial and office uses. The front setback (if provided) may be improved with landscaping or as an extension of the public sidewalk to create a more pedestrian-friendly environment.

- B. **Residential Fronts.** A frontage that reinforces the residential character and use of the building. The elevation of the ground floor is elevated above the grade of the lot to provide privacy for residences by preventing direct views into the home from the sidewalk. Entrances and windows are provided on the front of the facade to provide eyes on the street and direct sidewalk access to the building. Stoops are allowed to project into the front setback to enhance entrances. The front setback is primarily improved with landscaping.
- C. **Storefronts.** A frontage that reinforces the commercial character and use of the ground floor of the building. The elevation of the ground floor is located at or near the grade of sidewalk to provide direct public access into the building. Large storefront display windows are provided on the front of the facade to encourage visual access to merchandise displays and to encourage window shopping. Awnings or marquees are provided over storefront windows and entrances. The front setback (if provided) is primarily improved as an extension of the public sidewalk to create a more pedestrian-friendly environment.

TABLE 4.11 Live-Work/Office Frontage Standards

FRONTAGE STANDARDS ¹		SEE FIGURE 14.13
A	Elevation of Ground Floor	The ground floor elevation must be located near the elevation of the sidewalk to minimize the need for external steps and external ADA ramps at public entrances.
B	Minimum Ground Floor Interior Height	12 feet minimum (floor-to-floor height) (commercial ready)
C	Ground Floor Unit Entrances	All ground floor tenant spaces that have street frontage must have entrances on a facade fronting a street. All other ground floor uses may have a com-mon lobby entrance along the front facade or private entrances along other facades.
	Upper Floor Unit Entrances	Entrances to upper floor units may be provided through a common lobby entrance and/or by a com-mon entrance along a facade fronting a street.
	Recessed Entrances	Entrances may be recessed into the facade.
D	Ground Floor Windows	At least 40% of the surface area of the ground floor facade ² must be occupied by windows ³ .
E	Upper Floor Windows	At least 25% of the surface area of each upper floor facade ² must be occupied by windows ³ .
F	Awnings and Marquees	Awnings or marquees may be provided over store-front windows and entrances. Awning and marquees may project up to 6 feet from the facade and extend over the sidewalk provided that at least 8 feet of vertical clearance is provided.
G	Projecting Elements (Balconies, Roof Overhangs, Shade Structures, and Bay Windows)	Projecting elements on upper floors may project 3 feet from the facade and project into the setback.
H	Sidewalk and Setback Treatment	The public sidewalk must be improved with street trees with an average spacing of 30 feet on-center and pedestrian-scaled street lights (no taller than 14 feet). If the front facade is set back from the public sidewalk, the setback must be landscaped and/or improved as an extension of the public sidewalk.

Notes:

1. See section 4.10 (Live-Work Development).
2. As measured by multiplying the width of the facade by the floor-to-floor height. Opening in the facade (such as entrances to parking facilities or covered outdoor hallways/entrances) must be subtracted from the surface area calculation.
3. All parts of the window (e.g., head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing must be included as "window" in the calculation. Portions of the window that are not visible on the elevation drawing (such as a window that is blocked by a solid balcony wall) cannot be included in the calculation.

FIGURE 4.13 Live-Work/Office Frontage Standards

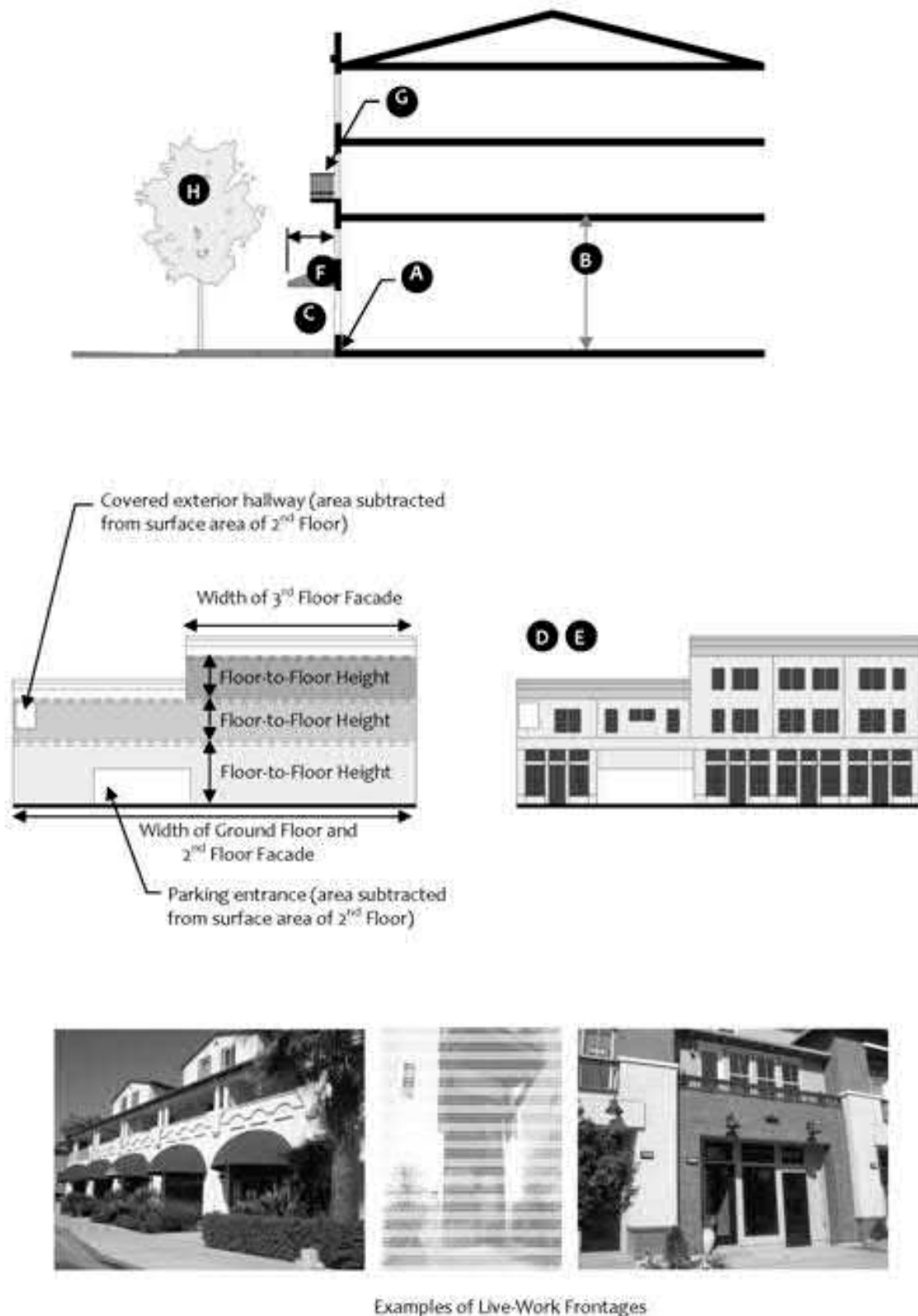


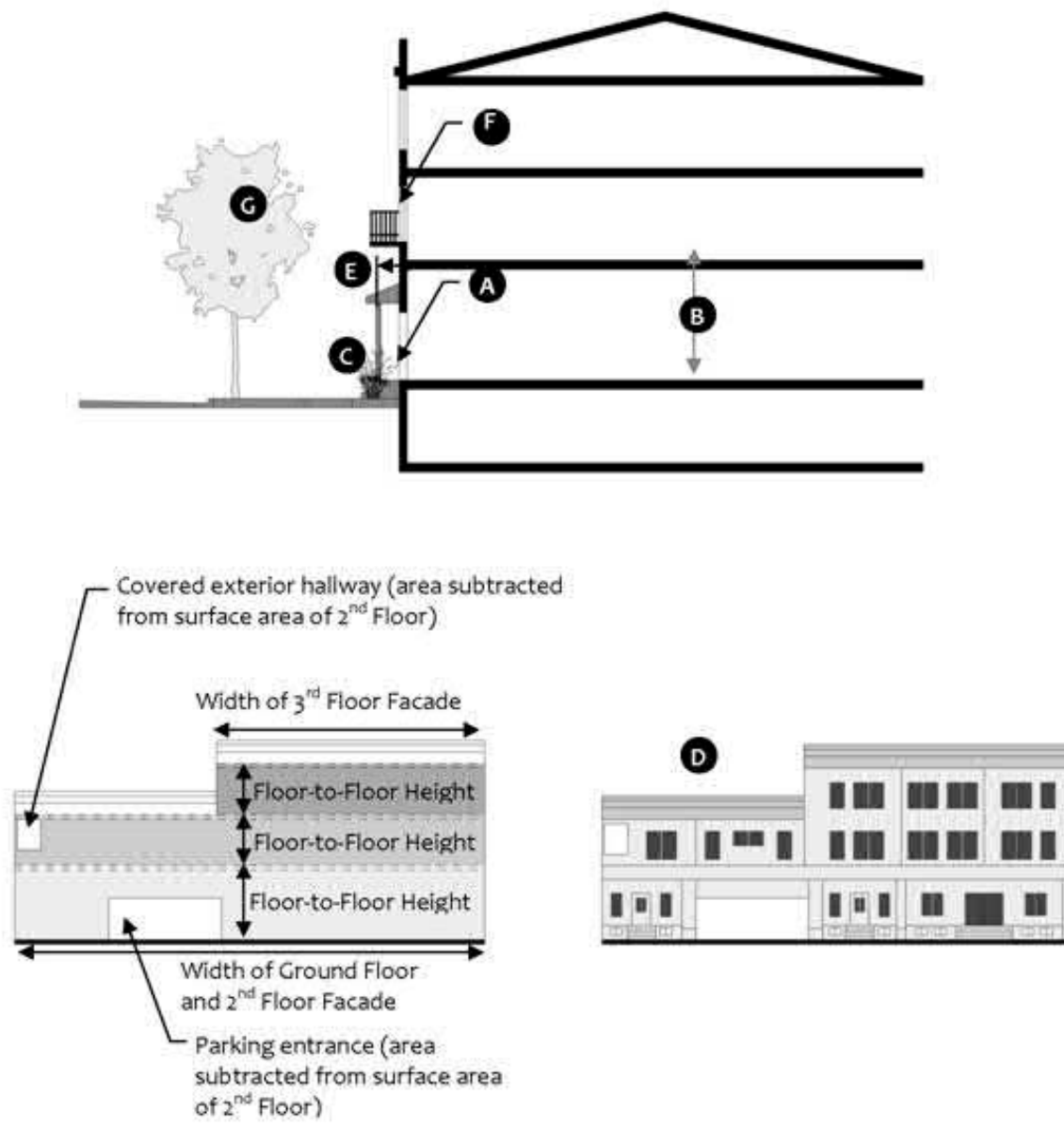
TABLE 4.12 Residential Frontage Standards

FRONTAGE STANDARDS ¹		SEE FIGURE 14.14
A	Elevation of Ground Floor	The ground floor elevation must be located within 6 feet of the ground surface of the adjacent sidewalk or walkway.
B	Minimum Ground Floor Ceiling Height	10 feet minimum (floor-to-floor height)
C	Ground Floor Unit Entrances	Entrances to ground floor units that have street frontage may be provided through a common lobby entrance and/or by private entrances from the adjacent sidewalk.
	Upper Floor Unit Entrances	Entrances to upper floor units may be provided through a common lobby entrance and/or by a common entrance along a facade fronting a street.
	Recessed Entrances	Entrances may be recessed into the facade.
D	Ground and Upper Floor Windows	At least 25% of the surface area of the ground and upper floor facade (1) must be occupied by windows ² .
E	Stoops and Front Porches	Stoops and front porches may be provided in front of building and unit entrances. Stoops and front porches may project up to 5 feet from the facade and project into the setback.
F	Projecting Elements (Balconies, Roof Overhangs, Shade Structures, and Bay Windows)	Projecting elements on upper floors may project 3 feet from the facade and project into the setback.
G	Sidewalk and Setback Treatment	The public sidewalk must be improved with street trees with an average spacing of 30 feet on-center and pedestrian-scaled street lights (no taller than 14 feet). If the front facade is set back from the public sidewalk, the setback must be landscaped (excluding stoops/front porches and paved paths to building entrances).

Notes:

1. As measured by multiplying the width of the facade by the floor-to-floor height. Opening in the facade (such as entrances to parking facilities or covered outdoor hallways/entrances) must be subtracted from the surface area calculation.
2. All parts of the window (e.g., head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing must be included as "window" in the calculation. Portions of the window that are not visible on the elevation drawing (such as a window that is blocked by a solid balcony wall) cannot be included in the calculation.

FIGURE 4.14 Residential Frontage Standards



Examples of Residential Frontages

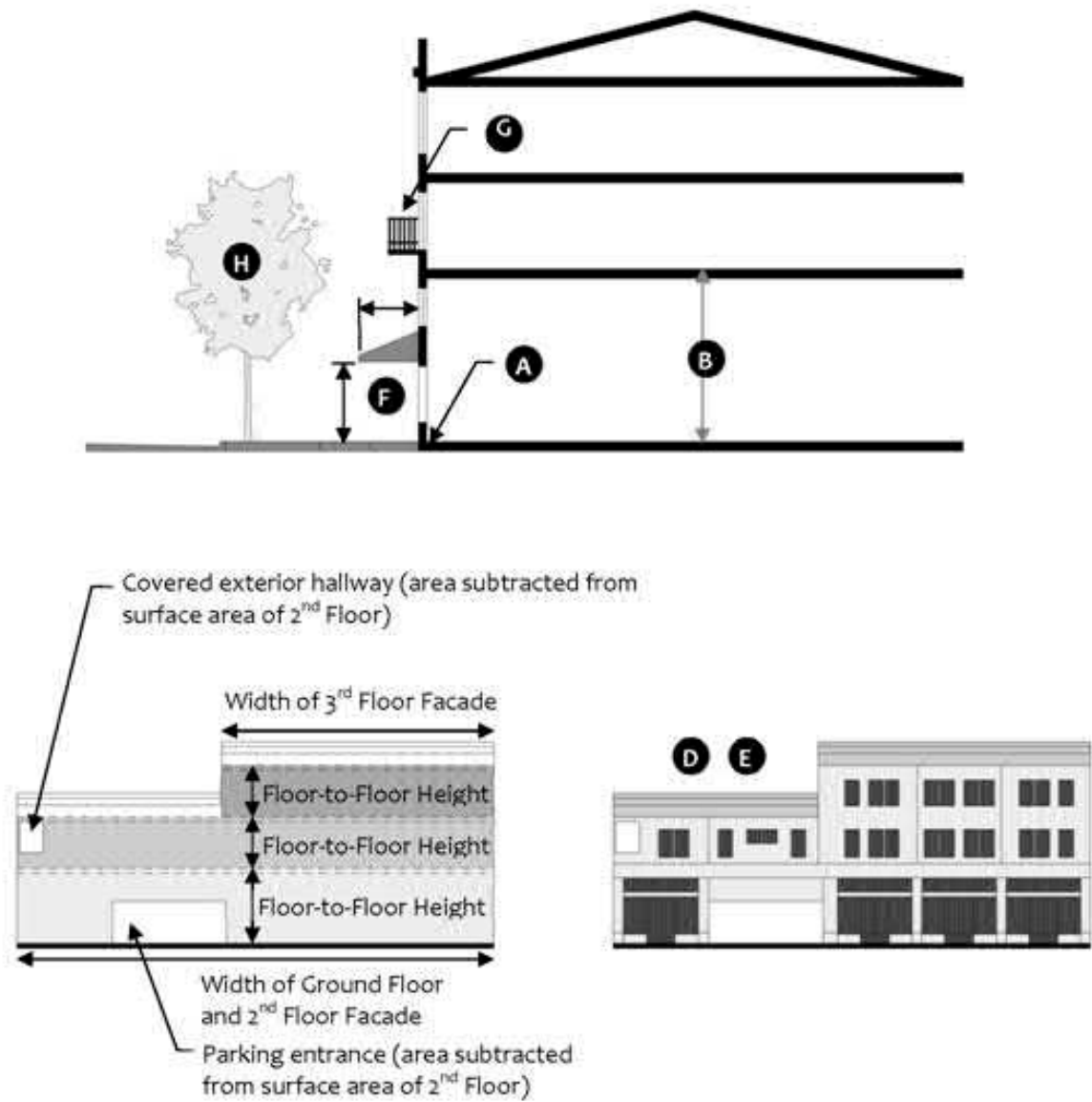
TABLE 4.13 Storefront Standards

FRONTAGE STANDARDS ¹		SEE FIGURE 14.13
A	Elevation of Ground Floor	The ground floor elevation must be located near the elevation of the sidewalk to minimize the need for external steps and external ADA ramps at public entrances.
B	Minimum Ground Floor Ceiling Height	15 feet minimum (floor-to-floor height)
C	Storefront Entrances	All ground floor tenant spaces that have street frontage must have storefront entrances on the facade fronting a street.
	Lobby Entrances	Lobby entrances to upper floor uses must be located on a facade fronting a street.
	Recessed Entrances	Storefront and lobby entrances may be recessed into the facade.
D	Ground Floor Windows	At least 50% of the surface area of the ground floor facade ¹ must be occupied by windows ² .
E	Upper Floor Windows	At least 25% of the surface area of each upper floor facade ¹ must be occupied by windows ² .
F	Awnings and Marquees	Awnings or marquees are required over storefront windows and entrances. Awning and marquees may project up to 6 feet from the facade and extend over the sidewalk provided that at least 8 feet of vertical clearance is provided.
G	Projecting Elements (Balconies, Shade Structures, and Bay Windows)	Projecting elements on upper floors may project 3 feet from the facade and project into the setback.
H	Sidewalk and Setback Treatment	The public sidewalk must be improved with street trees with an average spacing of 30 feet on-center and pedestrian-scaled street lights (no taller than 14 feet). If the front facade is set back from the public sidewalk, the setback must be improved as an extension of the public sidewalk.

Notes:

1. As measured by multiplying the width of the facade by the floor-to-floor height. Opening in the facade (such as entrances to parking facilities or covered outdoor hallways/entrances) must be subtracted from the surface area calculation.
2. All parts of the window (e.g., head, jamb, frame, sash, sill, muntin bars, and panes) that are visible on the elevation drawing must be included as "window" in the calculation. Portions of the window that are not visible on the elevation drawing (such as a window that is blocked by a solid balcony wall) cannot be included in the calculation.

FIGURE 4.15 Storefront Standards

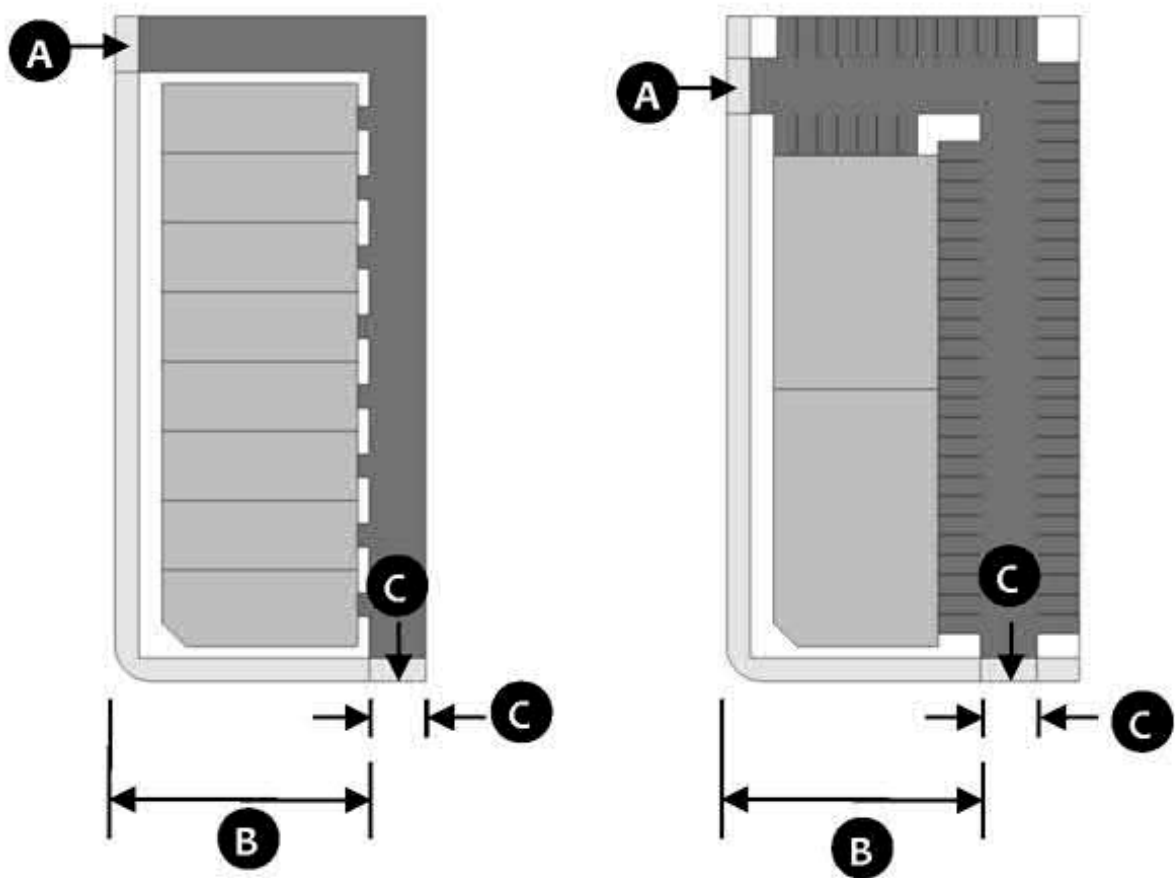


Examples of Storefronts

4.7.8 DRIVEWAY STANDARDS

This section will include standards for the design (width, pavement, fire access requirements) and placement (distance from intersections, line of sight standards) of driveways.

FIGURE 4.16 Driveway Standards



4.7.9 PUBLIC SPACE AMENITIES REQUIRED

Each project proposed within the BAMU Overlay Zone must include a public open space amenity, or some form of physical interface for the pedestrian. Such features may include, without limitation:

- A. **Formal Plazas.** A formal plaza would be a publicly accessible open space which has a design that is influenced by classical urban planning design. A formal plaza would typically include some sort of central water fountain and/or symmetrical landscaping.
- B. **Urban Gardens.** An urban garden can be located on the ground level, or on upper levels of a structure. Urban gardens include ornamental landscaping arranged in raised or at-grade planters or planting areas, potted plants and trees. Many times, there are sculptures or other forms of public art that are included within the urban garden.
- C. **Covered Colonnades.** Colonnades are linear in design and generous in depth. The intent is to provide a comfortably wide, covered pathway that is adjacent to the openings of a building. Sometimes the second floor of a building is utilized to create the “covered” element of the colonnade.
- D. **Sidewalk Dining.** Sidewalk dining may occur wherever a sidewalk space is ample enough to accommodate dining furniture without impeding pedestrian access of the sidewalk. Sidewalk dining may be defined with a railing or planters, or be open and accessible.
- E. **Pedestrian Alleys and Walkways.** A pedestrian alley or walkway is typically a “lane” that does not follow the alignment of a vehicular street, but provides a pedestrian access to either a public space or some other feature within the interior of a development. Pedestrian alleys or walkways must be designed in such a manner so as to be inviting to pedestrians. Therefore, issues such as lighting, security, line of sight, cleanliness and visual appeal are important considerations to a well-designed pedestrian alley or walkway. Sometimes public art, street furniture and access to shops and public spaces are features of pedestrian alleys and walkways.
- F. **Direct Public Transit Connection.** A direct connection to public transportation facilities, or amenities that service public transit facilities such as benches, informational kiosks, walkways, parking facilities or other similar amenities.

4.7.10 SIGNS

- A. **Signage.** See BMC Chapter 17.68 for sign standards.

4.7.11 OPEN SPACE STANDARDS—PUBLICLY-ACCESSIBLE OPEN SPACE

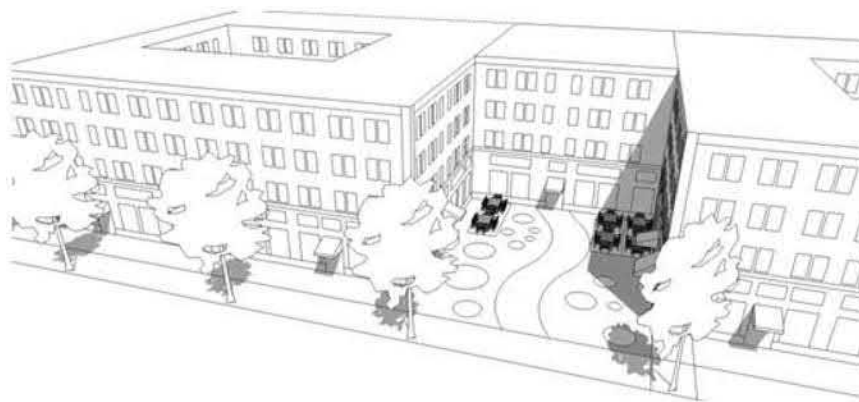
This section provides standards for publicly accessible open space areas in order to ensure a high level of pedestrian connectivity and activity between the public realm and the private realm, as defined in BMC Chapter 17.08 (Definitions).

- A. **Minimum Size.** All new nonresidential development must provide publicly accessible open spaces as a percentage of the total development site area as indicated in section 4.7.6, BAMU Overlay Zone Development Standards.
- B. **Eligible Areas.** Publicly accessible open space areas cannot include parking, driveway, or rear setback areas, but may include front and side setback areas provided that they are integrated into the overall design of the project.

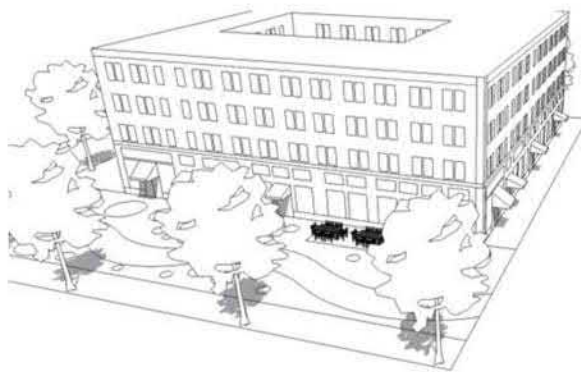
- C. **Ground-Level Installation.** Plazas, courtyards, or other similar publicly accessible open space areas must be installed at ground level and must be incorporated into the design of the development.
- D. **Visibility and Accessibility.** Public open space areas must be visible and accessible from the public rights-of-way to engage the interest of pedestrians and encourage public use and provide necessary measures to ensure sufficient site security and visibility (e.g., planting locations, appurtenances and lighting).
- E. **Landscaping and Hardscapes.** A combination of landscape and hardscape materials must be used in the design of these areas and must include the following components:
 - 1. Hardscape paving may include brick, stone, interlocking concrete pavers, textured concrete, and/or impressed patterned concrete. Hardscape elements may include, without limitation, seating areas, potted plant materials, water features, and public art installations.
 - 2. The balance of the open space areas must be landscaped with turf, shrubs, or groundcover, and trees. All plant materials must be in proportion to the height and mass of the building and must be permanently maintained and irrigated.
- F. **Minimum Height to Width Ratios.** In order to achieve sunlight and air circulation in required publicly accessible open space areas, the following minimum height to width ratios must be provided:
 - 1. Enclosed open space (i.e., open space that is enclosed on four (4) sides, such as a courtyard): 2 to 1 ratio. The required open space must have a width of at least one-half the height of the adjacent building facade (measured perpendicularly from the facade). This requirement applies to all sides of the required open space.
 - 2. Open space that is open on one (1) or more sides: 3 to 1 ratio. The required open space must have a width of at least one-third the height of the adjacent building facade (measured perpendicularly from the facade). This requirement must apply to all sides of the required open space.
- G. **Design Configuration.**
 - 1. Sharing of the required publicly-accessible open space ("quasi-public space") for nonresidential uses and the required common open space for residential uses, indicated in section 4.7.6, BAMU Overlay Zone Development Standards, may be allowed by the applicable review authority when it can be demonstrated that the open space will provide direct benefit to residents of the project and the general public subject to the following limitations:
 - a. Up to thirty (30) percent of the required open space for residential uses in a horizontal mixed-use project may be provided as quasi-public open space within the nonresidential component of the project; or
 - b. Up to fifty (50) percent of the required open space for residential uses in a vertical mixed-use project may be provided as quasi-public open space within the nonresidential component of the project.

- c. The minimum dimension (length and width) of shared common open space areas must be twenty (20) feet. These areas must be located at grade and must be accessible for use by the general public.
 - d. Quasi-public open space areas cannot include outdoor dining areas or other outdoor activity areas for exclusive use by an individual business.
 - e. Quasi-public open space areas are areas located on private property and accessible to the general public. These areas include pedestrian-oriented amenities, including enhanced seating, lighting, paving, landscaping, public art, water features, and other similar features deemed appropriate by the Director.
2. Publicly accessible open space areas must be located and configured as any one of the following (See Figure 4.17, Publicly Accessible Open Space)
- a. **Forecourt.** The publicly accessible open space area is located along a recessed center section of the front facade of the building as illustrated in Figure 4.17A (Publicly Accessible Open Space – Forecourt).
 - b. **Front.** The publicly accessible open space area is located along the street facing frontage of the building as illustrated in Figure 4.17B (Publicly Accessible Open Space – Front).
 - c. **“L” Shaped.** The publicly accessible open space area is located along the front and side of the lot as illustrated in Figure 4.17C (Publicly Accessible Open Space – “L” Shaped).
 - d. **Paseo or Central Courtyard.** The publicly accessible open space area is located on the side of the building or along a center pedestrian paseo or courtyard as illustrated in Figure 4.17D (Publicly Accessible Open Space – Paseo or Central Courtyard).

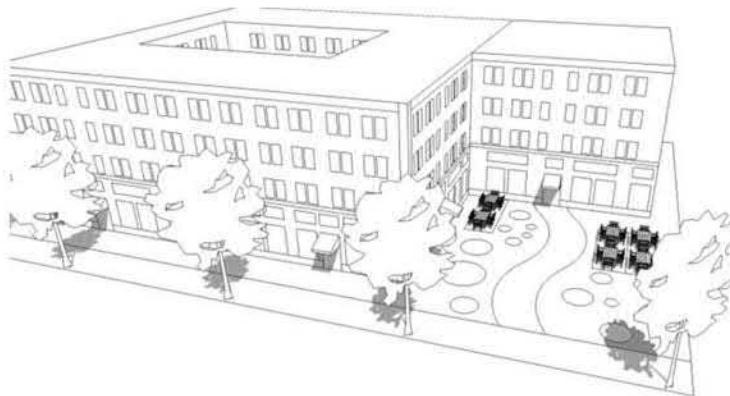
FIGURE 4.17 Publicly Accessible Open Space



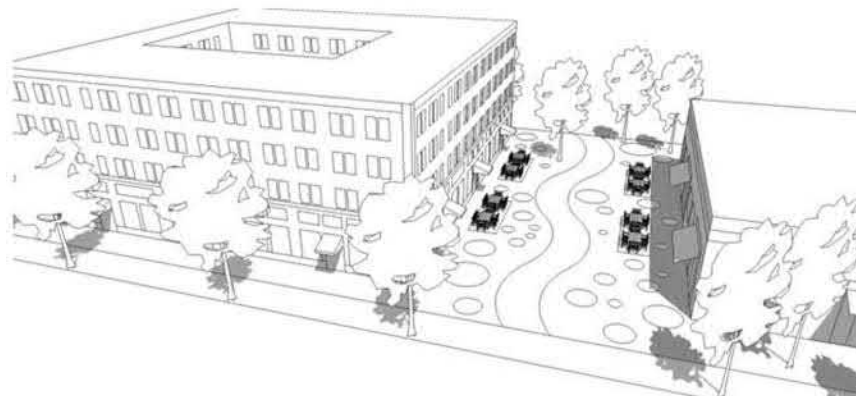
(A) Forecourt



(B) Front



(C) "L" Shaped



(D) Paseo or Central Courtyard

4.8 SPECIFIC USE DEVELOPMENT STANDARDS IN THE BAMU

Live-Work Development.

This section provides operational and compatibility standards for the development of live-work units. These standards are in addition to the standards for live-work development provided previously in this section.

A. Allowed Uses.

1. The nonresidential component of a live-work unit can only be a nonresidential use allowed within the BAMU Overlay Zone, except that certain uses are determined to be not appropriate within a residential environment and are therefore prohibited as provided in subsection B, below.
2. The residential component of a live-work unit can only be a residential use allowed within the BAMU mixed-use overlay zone.

B. Prohibited Uses. A live-work unit cannot be used for any of the following activities or similar activities as determined by the Director:

1. Adult-oriented businesses;
2. Animal care or boarding;
3. Classroom instruction (e.g., art/music lessons, tutoring, and similar uses) involving five (5) or more students at any one (1) time;
4. Commercial food preparation activities;
5. Industrial uses;
6. Vehicle maintenance or repair (e.g., body or mechanical work, including boats and recreational vehicles), vehicle detailing and painting, upholstery;
7. Storage of flammable liquids or hazardous materials beyond that normally associated with a residential use;
8. Medical and dental offices, clinics, and laboratories (not including chiropractors or counselors/psychotherapists);
9. Masseur-masseuse, massage establishments, acupressure spas, day spas, and similar uses;
10. Activities or uses that are not compatible with residential activities or that would clearly conflict with other live-work activities or the character of the surrounding neighborhood as determined by the Director; and
11. Activities or uses that would adversely affect the health or safety of live-work unit residents, because of dust, glare, heat, noise, noxious gases, odor, smoke, traffic, vibration, or other impacts, or that would be hazardous because of materials, processes, products, or wastes.

C. Site Planning and Design Standards.

1. Each live-work unit fronting a public right-of-way must have a pedestrian-oriented frontage that allows views into the interior of the nonresidential areas of the unit.
2. Each live-work unit must have a clearly identified, separate access from other live-work units within the structure or development. Access to individual units must be from common access areas, parking lots, or walkways. Access to each unit must be clearly identified to provide for emergency services.
3. The living space within the live-work unit must be contiguous with the working space, with direct access between the two (2) areas.

D. Operational Standards.

1. No portion of a live-work unit can be separately sold or rented.
2. The owner or developer of a structure containing live-work units must provide written notice to all occupants, tenants, and users that the surrounding area may be subject to higher impacts associated with nonresidential uses (e.g., noise) than exist in more predominantly residential areas. Performance standards for live-work units are those applicable to nonresidential uses allowed in the zone in which the live-work units are located.
3. All activities related to the “work” component of a live-work unit must be conducted within a completely enclosed building.
4. Up to two additional persons who do not reside in the live-work unit may work in the unit.
5. Client and customer visits to live-work units are allowed.
6. Parking for each live-work unit must be provided in compliance with BMC Chapter 17.88 (Off-Street Parking Requirements).
7. A live-work unit cannot be converted to either entirely residential use or entirely nonresidential use.
8. A live-work use may display a window or building-mounted sign up to a maximum of five percent of the building frontage area used for commercial purposes. Signs cannot be illuminated, including neon signs.

Mixed-Use Development.

This section provides operational and compatibility standards for mixed-use development. These standards are in addition to the standards provided previously in this section.

A. Operational Standards.

- B. **Hours of Operation.** Except as otherwise provided by a conditional use permit, outdoor nonresidential uses in mixed-use projects are prohibited from operating between the hours of 10:00 p.m. and 7:00 a.m. These hours may be modified through approval of a Conditional Use Permit in compliance with BMC Chapter 17.96 (Conditional Use Permits and Minor Use Permits).

C. Joint Tenants and Owners Association.

1. A joint tenants and owners association must be formed to ensure the well-being of each tenant and owner in a mixed-use project.
2. The covenants, conditions and restrictions ("CC&Rs") for such an association must be reviewed by the City Attorney and approved by the Director. The association's CC&Rs must include the following:
 - a. Assignment of parking spaces per each use.
 - b. Identification of maintenance responsibilities for landscaping, parking facilities, and recycling and refuse storage facilities.
 - c. Noise notification procedures.
 - d. Relationship between uses regarding association representation.
 - e. Procedures for solving problems that may arise between the different types of uses or residents.

D. Loading and Unloading Activities. Where applicable, the CC&Rs of a mixed-use project must indicate the times when the loading and unloading of goods may occur on the street, provided that in no event must loading or unloading take place after 10:00 p.m. or before 7:00 a.m. on any day of the week.

E. Noise Notification.

1. Residents, whether owners or tenants, of a mixed-use development project must be notified in writing before taking up residence that they will be living in an urban type of environment and that the noise levels may be higher than a typical residential area.
2. The CC&Rs of a mixed-use project must require that the residents acknowledge their receipt of the written noise notification. Their signatures must confirm receipt and understanding of this information.

F. Fences and Walls. In addition to the regulations in Table 17.14.030 (Development Standards for C-G and M-1), fences and walls are subject to the following regulations:

1. **Separation Wall Required.** A masonry separation wall must be constructed on all property lines adjacent to any single-family residential district. Pedestrian access points are encouraged and may be allowed subject to approval of the Director. The separation wall must be six feet in height, as measured from the highest elevation of land contiguous to the wall, except in a required front setback area and in a required exterior side setback area for a corner, reverse corner or key lot, where the wall must be limited to thirty-six inches in height.
2. **Other Fences and Walls.** Fences and walls are allowed in any yard area subject to the following height regulations:
 - a. **Front Yard Area.** In the front yard area, the height is limited to thirty-six inches.
 - b. **Street Side Yard.** In street side yard areas, the height is limited to thirty-six inches.

- c. All Other Areas. In all other areas, the height is limited to six feet, as measured from the side of the fence or wall with the highest grade.
- 3. Location. All perimeter fences and walls must be constructed on the property line unless a different location is permitted by the Director. No parallel wall or fence can be constructed less than five feet from an existing wall or fence, unless approved by the Director.
- 4. Materials.
 - a. Chain link fencing cannot be erected between a primary or accessory structure and a public or private street, except that chain link fencing may be used for security purposes for public utility structures and for temporary fencing needs (construction sites, special events, vacant lots).
 - b. Barbed wire and concertina wire are prohibited, except at public utility structures.
- G. **Landscaping.** Landscaping must comply with BMC § 17.44.070 (Landscaping).
- H. **Screening and Buffering—Mechanical Equipment and Trash Facilities.** Mechanical and air-conditioning equipment must be screened and buffered in compliance with BMC § 17.44.100 (Mechanical Equipment—Compressors, Air Conditioning Units or Similar Machinery) and trash facilities must be screened and buffered in compliance with BMC § 17.44.130 (Trash Facilities).
- I. **Signs.** See BMC Chapter 17.68 for sign standards.
- J. **Sound Mitigation.** Residential dwelling units must be designed to be sound attenuated against present and future project noise. New projects or new nonresidential uses in existing projects must provide an acoustical analysis report, by an acoustical engineer, describing the acoustical design features of the structure required to satisfy the exterior and interior noise standards.
- K. **Design Criteria.**
 - 1. A mixed-use development project must be designed and constructed to
 - a. Be pedestrian in its focus by:
 - b. Providing direct pedestrian linkages to adjacent public sidewalks.
 - c. Creating enhanced pedestrian connections throughout the project between residential and nonresidential uses and parking areas.
 - d. Providing enhanced pedestrian amenities throughout the project, including seating, pedestrian area lighting, special paving, public art, water features, common open space, directories, and similar items to create a pleasant pedestrian experience.
 - e. Incorporating architectural design elements and materials that relate to a pedestrian scale.
 - 2. Locate uses in proximity to one another without large intervening parking lots so that it is convenient for people to walk between the various uses and park their vehicles only once.
 - 3. Create a pedestrian scale and character of development along the street by providing significant wall articulation and varying roof heights, incorporating pedestrian scale elements

(e.g., doors, windows, lighting, landscaping), and locating storefronts and common open space areas (e.g., plaza, courtyard, outdoor dining) near the public sidewalk to contribute to an active street environment.

4. **Consistent Use of Architectural Details and Materials.** Architectural style and use of quality materials must be compatible and consistent throughout an entire mixed-use project. However, differences in architectural details and materials may occur to differentiate between the residential and nonresidential portions of the project. The overall project design and site layout must be one that promotes a strong pedestrian environment and active street frontage. This can be accomplished by incorporating features into the project as outlined in Chapter 7.

5. Features.

- a. **Street Level Features.** Variations in the front building plane must be incorporated through the use of varying building setbacks, variations in wall planes, and the inclusion of pedestrian amenities (e.g., plaza, courtyard, outdoor dining, landscaping). Long expanses of blank walls are prohibited.
- b. **Pedestrian-Oriented Features.** At least 75 percent of the building frontage facing a public street, primary pedestrian way, or parking lot must be devoted to pedestrian-oriented features (e.g., storefronts, pedestrian entrances to nonresidential uses; transparent display windows; landscaping).
- c. **Upper Level Features.** Upper floor balconies, bays, and windows must be provided whenever opportunities exist for these types of features.
- d. **Entrances.** When nonresidential and residential uses are located in a vertical mixed-use structure, separate pedestrian entrances must be provided for each use. The entrances for nonresidential uses must be designed to be visually distinct from the entrances for residential uses. Entrances to individual residential units in a vertical mixed-use project must not be allowed along a street frontage. Instead shared entrances to residential units located above the ground floor must be from lobbies that serve multiple units.
- e. **Neighborhood Interface.** The design of new infill development must be sensitive to the scale and design characteristics of established structures in abutting residential neighborhoods, with the objective of achieving a harmonious transition between the new development and existing neighborhood. Consideration must be given to factors including, without limitation, orientation of architectural features, building articulation, and exterior building treatments.
- f. **Lighting.** Lighting must be incorporated along sidewalks or other pedestrian walkways, plazas, paseos, courtyards, and other common open areas to enhance the pedestrian environment and increase public safety. Lighting for nonresidential uses must be designed, located, and shielded to ensure that they do not adversely impact the residential uses, but must provide sufficient illumination for access and security purposes.

- g. Security. Projects must be designed to minimize security risks to residents and to minimize the opportunities for vandalism and theft. This may be accomplished by:
 - i. Maximizing visibility to common open space areas, internal walkways, and public sidewalks. Use opportunities for natural surveillance to increase visibility.
 - ii. Using walkways, low fences, lighting, signage, and landscaping to clearly guide people and vehicles to and from the proper entrances.
 - iii. Eliminating areas of concealment, hiding places, and dead spaces.
 - iv. Using lighting to improve the visibility of common areas while enhancing the pedestrian environment. Lighting should not be overly bright and should provide a uniform level of light over the subject area to eliminate dark spaces.

Outdoor Dining.

This section provides standards for outdoor dining areas.

- A. **Public Property.** Outdoor dining on public property requires approval of an encroachment permit by the Public Works Director and compliance with the standards of the Public Works Department.
- B. **Private Property.** Outdoor dining on private property must comply with the following standards:
 - 1. Coordinated Design Scheme. The design and appearance of proposed improvements or furniture (e.g., tables, chairs, benches, umbrellas, planters, menu boards) to be placed in an outdoor dining area must present a coordinated theme and be compatible with the appearance and design of the primary structure, as determined by the Director.
 - 2. Hours of Operation. Hours of operation for outdoor dining areas must coincide with those of the associated indoor restaurant.
 - 3. Property Maintenance. The operator must maintain the outdoor dining area(s) in a neat, clean, and orderly condition at all times. This includes all tables, benches, chairs, displays, or other related furniture. An adequate number of trash receptacles must be provided to serve the outdoor dining area.
 - 4. Outdoor Bar Prohibited. A bar designed and/or operated to sell or dispense any alcoholic beverages must not be allowed in the outside dining area.
 - 5. Location. Outdoor dining areas may be allowed to locate in required setback areas but must not encroach into required parking areas. They may be allowed to encroach into a public right-of-way with an approved Encroachment Permit issued by the City Engineer.
 - 6. Noise. Amplified sound (e.g., music, television) cannot be audible beyond the lot line.
- C. **Review Criteria.** When reviewing an application to allow outdoor dining, the reviewing authority must consider the relation of outdoor dining areas to sensitive noise receptors (e.g., hospitals, schools, and residential uses). Mitigation measures must be applied to eliminate potential impacts related to glare, light, loitering, and noise.

4.9 BELLFLOWER VILLAGE OVERLAY ZONE (BVOZ)

4.9.1 PURPOSE AND INTENT

The size and location of the Bellflower Village Overlay Zone creates a unique development opportunity within the City. The project area is located within the downtown area of Bellflower and is fully developed. The area is comprised of commercial, office, and residential uses. The area is also adjacent to fully developed commercial to the north, south and west, and a stable of single-family residential area to the east. Encouraging unique, quality and viable development within the Bellflower Village Overlay Zone is intended in order to promote the orderly recycling of existing land uses within the specific plan boundaries. Because of land values, development standards constraints and other factors that affect the reuse of developed property, it is unlikely that the properties within the boundaries of the overlay zone would recycle without the development and economic incentives provided within the provisions of the Bellflower Village Overlay Zone.

- A. **Purpose.** The Town Center District was created in May 28, 1996, Ordinance 869 with the intent on ensuring quality development and design within the downtown area of the City of Bellflower. Downtown Bellflower is an important area of Bellflower, providing a tangible link to the City's rich historical past, geographic central focal point, recognizable town center, and a centerpiece for renewed civic vitality. However, current land use and development constraints, when combined with the current marketplace and the realities of real estate, provide an atmosphere where conventional and traditional zoning techniques may not accomplish the goals set forth in the Town Center Zoning District. Therefore, land use, development and other regulatory tools need to be created to encourage private sector investment and creativity, as well as ensuring and safeguarding that high design standards and quality construction methods and materials are utilized.
- B. **Intent.** The Bellflower Village Overlay Zone recognizes that there is a geographic portion of downtown Bellflower that has the potential of becoming a creative, vital and visionary neighborhood. Conventional zoning and land use standards will not be successful in promoting this transformation. Therefore, a more creative, flexible and focused land use plan needs to be created. The Bellflower Village Overlay Zone is the land use tool that is proposed to create the incentive for the recycling and reuse of land within the Overlay Zone boundaries.
- C. **Relationship to Other City Plans and Documents.** In May 28, 1998, the City Council adopted Ordinance No. 896 which established the Bellflower Town Center District. The Town Center District is intended to create the catalyst of a design and economic revitalization of the City's traditional downtown district. The City of Bellflower General Plan (Updated 1997) designates the project site as "Town Center," which is essentially a "commercial" land use designation.

The development standards and design guidelines in this Overlay Zone provide an alternative set of development standards and allowable uses beyond those contained in the underlying zoning designation over which the Overlay Zone is applied. In addition, the City of Bellflower Town Center Design Guidelines (Resolution 96-93) apply.

- D. **Content and Components.** The Bellflower Village Overlay Zone will govern future redevelopment and the recycling of land by accomplishing the following:

1. Encourage the recycling of existing developed properties into contemporary, vital and interesting development that will enrich downtown Bellflower.
2. Provide for more flexible regulatory procedure by which the objectives of the City's General Plan and BMC be realized.
3. Encourage creative approaches to the use of land through variation in siting of buildings and the appropriate mixing of commercial land uses and activities.
4. Encourage redevelopment by providing creative and unique economic opportunities to develop and use property.
5. Create a unique and vibrant neighborhood within the downtown area of Bellflower that will encourage further private investment in the City.
6. Enhance the appearance of the community through creative design of buildings, structures, and facilities.
7. Eliminate and prevent the spread of blight.
8. Revitalize, redevelop, and upgrade those parcels within the Overlay Zone area.
9. Strengthen the City's economic base.

This Overlay Zone is a land use policy, regulatory and land use intensity document that establishes policies, development standards, allowable uses, uses allowed with the approval of a conditional use permit, design guidelines to regulate and guide future redesign and redevelopment of the Overlay Zone area. The Overlay Zone establishes the type, location, intensity and character of uses for redevelopment and the recycling of land to occur. The Overlay Zone seeks to shape future development by responding to the existing physical and economic constraints on the site and coordinating future development, parking design and circulation and public facility improvements. Future development and development plans proposed on property within the Overlay Zone is consistent with the provisions within the Overlay Zone.

4.9.2 DESCRIPTION OF BELLFLOWER VILLAGE OVERLAY ZONE AREA

- A. **Location of Overlay Zone.** The Bellflower Village Overlay Zone encompasses an approximate three block area of downtown Bellflower, extending along the eastside of Bellflower Boulevard from Flower Street spanning southerly to Arkansas Street. The amount of land that is within the boundaries of the Overlay Zone is approximately 7.16 acres. This area is further defined by Maple Street and Walnut Street bisecting the project area and dividing the project area into three roughly equal sized blocks. Refer to Exhibit A, Vicinity Map, which presents the regional location of the Overlay Zone. Exhibit B, Location Map, presents the location of the Overlay Zone area.
- B. **Existing Conditions.** The project area is fully developed with existing buildings and land uses. The predominate land use is commercial, but several residential uses exist within the boundaries of the Overlay Zone. The land area within the Overlay Zone is divided into three separate blocks. For the purposes of addressing the existing development within the Overlay Zone, the three

blocks within the Overlay Zone will be referred to as “Block No. 1,” “Block No. 2” and “Block No. 3.” The conditions within each one of these blocks is as follows:

1. **Block No. 1.** This block spans on the eastside of Bellflower Boulevard from Flower Street to Maple Street. This area extends easterly from Bellflower Boulevard, approximately 300 feet. There is a jog, or variation, in the easterly boundary. The buildings and uses within Block No. 1 are as follows:
 - a. **Chase Bank.** (16804 Bellflower Boulevard, APN: 7109-014-018). This property is located on the immediate southeast corner of Bellflower Boulevard and Flower Street. The bank building has existed since the 1960’s and is well maintained.
 - b. **Parking Lot.** (16804 Bellflower Boulevard, APN: 7109-014-017. This parking lot is owned by Chase Bank and is part of the same parcel as the bank building discussed above. The parking lot is separated from the bank building by an alley which parallels Bellflower Boulevard. On the easterly edge of the parking lot is a vacant retail building. The retail building has been vacant for several years and is not architecturally significant.
 - c. **Retail Stores.** (9832 Flower Street, 9844 Flower Street, 9800 Flower Street, and 9900 Flower Street, APNs 7109-014-006, 7109-014-801, 7109-014-800, and 7109-015-800). These properties front onto Flower Street, immediately east of the Chase Bank parking lot. These retail stores are essentially small “mom & pop” style of commercial retail uses. There is no dedicated parking for these stores, and they must rely upon the use of curb, or on-street parking, on the City’s streets.
 - d. **Furniture Store.** (16810 Bellflower Boulevard, APN: 7109-014-014). This store is on the same parcel of land as the Chase Bank building. It is assumed that the furniture store has access to the Chase Bank parking lot.
 - e. **Former Bank of the West.** (16824 Bellflower Boulevard, APN: 7110-014-012). This building is located on the northeast corner of Bellflower Boulevard and Maple Street. The bank building was constructed in the 1960s and is well maintained. This building also has a drive-thru teller feature that is accessible from the rear of the property and exits onto Bellflower Boulevard.
 - f. **Parking Lot.** (9828 Flower Street APN: 7109-014-017. As with the Chase Bank parking lot, this parking lot is also part of the same parcel that the former Bank of the West building sits upon. The parking lot spans the frontage of Maple Street, essentially even with the easterly dimension as the Chase Bank parking lot. However, this parking lot is physically separated from the Chase Bank parking lot by a change of grade elevation which makes internal circulation between the two parking lots impossible as they are presently developed. As with the Chase Bank parking lot, the former Bank of the West parking lot is also separated from the bank building by an alley that runs parallel to Bellflower Boulevard.
2. **Block No. 2.** This block spans the eastside of Bellflower Boulevard, between Maple Street to the north and Walnut Street to the south. This block extends easterly along Maple Street and

Walnut Street approximately 306 feet. The buildings and uses within Block No. 2 are as follows:

- a. **Medic Drugstore and Pharmacy.** (16900 Bellflower Boulevard, APN: 7109-019-001). This property is located on the immediate southeast corner of Bellflower Boulevard and Maple Street. The property is currently improved with a drugstore/pharmacy which was constructed in approximately 1950.
 - b. **Retail Stores—Add's Flooring, etc.** (9812 through 9824 Maple Street, APNs 7109-019-006 and 7109-019-009). This portion of Block No. 2 can be characterized as "light commercial" in development and land use intensity. These properties can be characterized as commercial buildings built with a "zero" setback, with vehicular access behind the buildings. Each of the stores within this portion of Block No. 2 has small "rear-yard loading areas" that also provide surface parking. The retail buildings are one (1) story in height and extend along Maple Street to align with the easterly terminus of the bank parking lots. These properties are separated from the drugstore/pharmacy property by an alley that is parallel to Bellflower Boulevard.
 - c. **Commercial Parking Lot.** (16906 Bellflower Boulevard, APN: 7109-019-003). This parcel was at one time developed with a single-story medical office building with an adjoining surface parking lot. The prior building was demolished and was vacant for several years until recently being changed into a private parking lot.
 - d. **Café Camellia.** (16914 Bellflower Boulevard, APN: 7109-019-005). The "Café Camellia property" is actually a single parcel containing several uses, the Café Camellia restaurant being the most noteworthy. In addition to the Café Camellia restaurant, there is an automobile-related service/retail use that is located within an old 1940's vintage gasoline service station. As with the drugstore/pharmacy property, an alley runs the easterly edge of this property, parallel to Bellflower Boulevard.
 - e. **Mixed-Use Development.** (9815 Walnut Street, APN: 7109-019-007). This property formerly contained a 1955 era 40-unit, motel. The motel property was approximately seventy-one (71) feet of frontage along Walnut Street, but span approximately one hundred thirty-five (135) feet of the north/south alley that parallels Bellflower Boulevard and runs along the easterly edge of the drugstore/pharmacy and Café Camellia properties. Currently, the property is developed with a mixed-use development with six commercial units on the bottom floor and six residential units on the second floor.
 - f. **Apartments.** (9823 Walnut Street, APN: 7109-019-008). This property is currently improved with two (2), two-story apartment buildings. The apartments do not have enclosed or covered parking. However, limited on-site, uncovered surface parking is provided for the apartment tenants. The apartment buildings were constructed in approximately 1958.
3. **Block No. 3.** This is the southerly-most block of the Bellflower Village Overlay Zone. This block is delineated by Walnut Street along the north, Bellflower Boulevard along the west, and Arkansas Street along the south. The approximate easterly dimension is approximately two hundred forty-one (241) feet east of Bellflower Boulevard.

- a. **The Glory Center.** (9812 Walnut Street, APN: 7109-020-020). This property was at one time the location of the local newspaper publisher. The building was upgraded and revitalized in approximately 2003.
 - b. **The Glory Center Parking Lot.** (9816 Walnut Street and 9820 Walnut Street, APN: 7109-020-019 and 7109-020-018). This property is approximately twelve thousand five hundred ninety-three (12,593) square feet in land area, and contains a small office building in addition to the parking lot. This property is separated from The Glory Center building by a private alley that is oriented north/south, parallel to Bellflower Boulevard. The alley in this location has the name of "Rush Street."
 - c. **Vacant Lot/future Parking Lot.** (9824 Walnut Street, APN: 7109-020-901). This property is owned by the City of Bellflower and is currently vacant. The future intent for this property is to develop it as public parking to support the revitalization efforts of the Overlay Zone.
 - d. **Commercial Retail.** (17.12 Bellflower Boulevard, 17.16 Bellflower Boulevard, 17.18 Bellflower Boulevard, and 17.22 Bellflower Boulevard, APNs: 7109-020-021, 7109-020-022, 7109-020-023 and 7109-020-026). These retail uses face onto Bellflower Boulevard, but also have access to the private alley, or "Rush Street," located to the rear of these properties. Independent, "mom & pop" style of businesses are located in this portion of Block No. 3.
 - e. **Miscellaneous Residential and Commercial.** (17.10 Rush Street, 17.14 Rush Street, 17.18 Rush Street, and 9817 Arkansas Street, APNs 7109-020-016, 7109-020-017, 7109-020-015, 7109-020-014, and 7109-020-013 -vacant-). This portion of Block No. 3 contains the most residential. In this area are several single-family residential structures and an apartment building. All of these uses take access from Rush Street, which is located to the west. This area also "curls around" the future City parking lot along Arkansas Street, with a vacant lot that appears at one time provided surface parking for the adjoining commercial buildings along Arkansas Street.
- C. **General Plan and Zoning Designations.** The General Plan designation for the Overlay Zone project area is Town Center. The Zoning designation for this area is also Town Center. The Town Center District and General Plan designations are essentially commercial and specialty office zoning and land use designations. The Town Center District seeks to promote quality, unique and vibrant retail commercial uses.

4.9.3 BELLFLOWER VILLAGE OVERLAY ZONE DESCRIPTION

- A. **Development Concept.** The Overlay Zone concept is an opportunity to utilize an alternative zoning tool for properties that are within the boundaries of the Overlay Zone. The property owner would have the choice of developing property pursuant to the provisions of the underlying zone; or, developing property pursuant to the Bellflower Village Overlay Zone.

The intent of this concept is to provide for a unique and flexible zoning tool that would promote the creative development of property within the boundaries of the Bellflower Village Overlay Zone pursuant to the goals and objectives of the "Mixed-Use" Land Use designation of the

Bellflower General Plan. Essentially, the urban design objectives of this development concept are as follows:

1. **Mixed-Use Theme.** Promote development pursuant to the goals and objectives in the “Mixed-Use” Land Use designation of the Bellflower General Plan;
 2. **Create a Vibrant Commercial Destination.** Create an attractive and unique neighborhood that would attract travelers and commuters who use Bellflower Boulevard as a regional transportation corridor. In addition, the Bellflower Village neighborhood would further the fulfillment of the “Connection of Plazas” concept adopted in the Downtown Bellflower Revitalization Vision Strategy (January 2002).
 3. **Protect the Adjacent Residential Neighborhood.** Through innovative and thoughtful design techniques, maintain and preserve the integrity of the existing single-family residential neighborhood that is immediately east of the Bellflower Village Overlay Zone.
 4. **Village Design Theme.** Allow for the use of varying architectural design themes, but require that each design is thoughtfully blended and is compatible with adjoining and adjacent development. The urban design goal is to foster a vibrant, unique and exciting village atmosphere that creates a “sense of place”; irrespective of the mix and proportion of commercial and residential uses that will eventually be developed and occupy the Bellflower Village Overlay Zone district.
 5. **Incremental Development.** Base the approval of individual and incremental development projects on a single concept plan that provides defined ranges of flexibility to accommodate market changes.
 6. **Public Spaces.** Provide for public spaces that serve both the commercial visitor as well as the local resident. Public spaces would further the goals of the “Connection of Plazas” concept adopted in the Downtown Bellflower Revitalization Vision Strategy (January 2002).
 7. **Land Use Flexibility.** Maintain the land use and development options so that as individual properties become available for development, that there be a choice between commercial, residential or mixed use, as provided in current market conditions. Although “live/work” and “mixed use” development is not a strict requirement for development within the Bellflower Village Overlay Zone, it is strongly encouraged that new develop integrate these types of development techniques into any proposed project. The overall design goal is to create pedestrian walk-ability within the boundaries of the Bellflower Village Overlay Zone and to contribute to the unique “landmark” place-making capabilities of this overlay zoning district.
- B. **Conceptual Development Plan.** The overlay zone concept is intended to allow for the individual development of existing parcels, while including certain common features that serve to unify the entire overlay zone district. These common features allow for the potential development to act in a complimentary fashion that reinforces and benefits each development. Therefore, these common features act in a manner which will result in a greater level of land use effectiveness and efficiency.

The existing parcel and ownership configuration within the boundaries of the Bellflower Overlay Zone precludes the option for a single unified development effort. Therefore, a single parcel, or small collection of parcels, must be allowed to be developed on an incremental basis, but with regulatory flexibility to make accommodations for significant development standards, such as guest parking. This regulatory flexibility would economically encourage the private sector to invest and redevelop existing properties within the boundaries of the overlay zone, but also address the fundamental needs to provide for those development standards, such as guest parking.

1. **Future Land Uses and Density.** Within the Bellflower Village Overlay Zone area, the land use development pattern is predominately commercial. However, there is a small scattering of residential uses. There is also an existing north/south alley that runs parallel to Bellflower Boulevard and separates parcels with Bellflower Boulevard street frontage from the remainder of property within the overlay zone district. Projects that choose to utilize the development standards in the Bellflower Village Overlay Zone may intensify the development potential of the individual parcel of land, beyond what the underlying zoning development standards would allow. The “intensification” of development potential is intended to create an economic incentive for the existing properties that are within the boundaries of the overlay zone to redevelop through privately funded development efforts. Such intensification may be in the form of greater building height, the ability to incorporate residential uses into the project, or the ability to transfer some off-street parking obligations to another location.
2. **Access and Circulation.**
 - a. Because there are numerous individually developed parcels within the boundaries of the Bellflower Village Overlay Zone area, there are multiple driveway aprons that provide vehicular access from five public streets that are adjacent to, or within, the boundaries of the Bellflower Village Overlay Zone area. To the extent practicable, developments on individual parcels must reduce the number of driveway aprons that access public streets. This may be accomplished by use of shared driveway access points or removal of driveway aprons.
 - b. There are three points of entry into the interior of the Bellflower Village Overlay Zone. These are the intersections of Bellflower Boulevard and Maple Street, Bellflower Boulevard and Walnut Street, and Bellflower Boulevard and Arkansas Street. Furthermore, access into the single-family residential neighborhood to the east is accessible from Maple Street, Walnut Street and Arkansas Street.
3. **Parking.**
 - a. The Bellflower Village Overlay Zone creates two satellite parking lots that can be used to assume some or all of the required off-street parking for new development within the overlay zone district. Each development that proposes to transfer some, or all, of their required off-street parking obligation to one of the two satellite parking lots will be required to demonstrate the feasibility of transferring their parking obligation, as well as to provide financial compensation that will provide for the long-term maintenance of the satellite parking lots.

- b. Residential projects are required to provide on-site parking for each residential unit. Guest parking requirements, however, may be transferred to one of the satellite parking lots upon Director approval. Commercial uses may propose to transfer all on-site parking obligations to one of the satellite parking lots subject to federal and state handicap parking requirements and loading areas.
 - c. The parking requirement for each use will be those that are established in the BMC. Each development that proposes to utilize the satellite parking lots to meet some, or all, of their on-site parking obligation must enter into a parking agreement with the City in a form approved by the City Attorney.
4. **Public Spaces.** Developing public spaces is very important for the overall success of the Bellflower Village Overlay Zone. All such public places must adhere to the “Connection of Plazas” design concept which promotes development of pedestrian-oriented plazas or open spaces within downtown Bellflower.
- a. Each project proposed within the Bellflower Village Overlay Zone must include a public open space amenity, or some form of physical interface for the pedestrian. Such features may include, without limitation:
 - i. **Formal Plazas.** A formal plaza would be a publicly accessible open space which has a design that is influenced by classical urban planning design. A formal plaza would typically include some sort of central water fountain and/or symmetrical landscaping.
 - ii. **Urban Gardens.** An urban garden can be located on the ground level, or on upper levels of a structure. Urban gardens include ornamental landscaping arranged in raised or at-grade planters or planting areas, potted plants and trees. Many times, there are sculptures or other forms of public art that are included within the urban garden.
 - iii. **Covered Colonnades.** Colonnades are linear in design and generous in depth. The intent is to provide a comfortably wide, covered pathway that is adjacent to the openings of a building. Sometimes the second floor of a building is utilized to create the “covered” element of the colonnade.
 - iv. **Sidewalk Dining.** Sidewalk dining may occur wherever a sidewalk space is ample enough to accommodate dining furniture without impeding pedestrian access of the sidewalk. Sidewalk dining may be defined with a railing or planters, or be open and accessible.
 - v. **Pedestrian Alleys and Walkways.** A pedestrian alley or walkway is typically a “lane” that does not follow the alignment of a vehicular street, but provides a pedestrian access to either a public space or some other feature within the interior of a development. Pedestrian alleys or walkways must be designed in such a manner so as to be inviting to pedestrians. Therefore, issues such as lighting, security, line of sight, cleanliness and visual appeal are important considerations to a well-designed

pedestrian alley or walkway. Sometimes public art, street furniture and access to shops and public spaces are features of pedestrian alleys and walkways.

5. **Architectural Treatments and Features.** The Bellflower Village Overlay Zone does not specify a designated architectural style or theme. Instead, the Bellflower Town Center District Design Guidelines apply.
6. **Infrastructure Improvements and Public Services.**
 - b. **Water Service.** Water service for the Bellflower Village Overlay Zone area is provided by the Bellflower Somerset Mutual Water Company (BSMW). BSMW's supplies are derived from two sources, including groundwater from local wells and surface water from the Metropolitan Water District of Southern California (MWDSC). MWDSC's water is attained from the Colorado River and the State Water Project in Northern California. The BSMW distribution system consists of one pressure zone, which ranges from 52 to 72 pounds per square inch (psi).
 - i. The Bellflower Village Overlay Zone area is currently fully developed, and has full and complete access to water infrastructure. Projected water demand for development within the Bellflower Village Overlay Zone is approximately 0.13 gallons per day (gpd) per square foot for a retail store; 0.57 gpd per square foot for restaurants; 0.49 gpd per square foot for offices; and a four-person family uses approximately 255 gpd for residential uses.
 - ii. The Bellflower Village Overlay Zone is served by an eight-inch waterline that runs in a north/south direction that is in Bellflower Boulevard. In addition, there is a six-inch waterline in Flower Street, and six-inch waterlines in Maple Street, Walnut Street and Arkansas Street.
 - iii. New development within the overlay zone district would be required to connect to the existing water purveyor. New connections to the existing underground water conveyance piping may require upgrading, the installation of new valves and related equipment and would be evaluated on a project-by-project basis in order to comply with minimum Los Angeles County Fire Department Fire Flow Criteria.
7. **Wastewater Improvements.** The Bellflower Village Overlay Zone is located within the Sanitation District of Los Angeles County. Wastewater that would be generated as a result of development within the overlay zone boundaries would flow into local sewer lines, which are owned and operated by the City of Bellflower, and eventually feed into the regional county Sanitation District sewer lines. The recommended peak flow of wastewater generation factors for commercial and restaurant uses are 300 gallons per day per 1,000 square feet of commercial area, and 150 gallons per day per seat for restaurants. Non-residential uses generate approximately 1.18 pounds of waste day. Residential uses generate approximately 2.49 pounds of waste per day.
 - c. **Drainage Improvements.** All regional storm drain facilities within the City are constructed and maintained by the County of Los Angeles Department of Public Works.

Local stormwater management facilities, such as storm drains within local streets and roads, are the responsibility of the City of Bellflower.

- i. Stormwater flow from the Bellflower Village Overlay Zone discharges into local storm drains operated and maintained by the City of Bellflower Public Works Department. These drains convey stormwater runoff to the San Gabriel River Channel. The San Gabriel River Channel is east of the City, and is designed to contain a 100-year flood. The San Gabriel River is maintained by the U.S. Army Corps of Engineers and the Los Angeles County Department of Public Works. The construction of flood control improvements along the San Gabriel River in 1947 effectively eliminated flooding within the City.
 - ii. On-site stormwater runoff would be designed to convey away from existing catch basins located within the public street system adjacent to, and within, the Bellflower Village Overlay Zone district. The development of on-site storm drainage infrastructure will be required, pursuant to the preparation of a Drainage Plan and a Stormwater Mitigation Plan.
- d. **Police Protection Services.** The Los Angeles County Sheriff's Department (LASD) provide police protection and law enforcement services for the City of Bellflower, including the area within the boundaries of the Bellflower Village Overlay Zone. The station with first response duties to the overlay zone district would be the Lakewood Sheriff's Station, located at 5130 Clark Avenue in the City of Lakewood. This station is located approximately 2.93 miles south of the Bellflower Village Overlay Zone. The Lakewood Station has a staff of one captain, six lieutenants, 12 sergeants, 150 deputies, and 30 civilian officers. In addition to the Lakewood Station, the City of Bellflower operates a substation that is located at 16615 Bellflower Boulevard, in the City of Bellflower. The Bellflower Substation has a staff of one lieutenant, three sergeants, 43 deputies, and seven civilian officers. Currently, the City utilizes an officer-to-population ratio of seven and one-half to 10,000 to determine service demand.
- e. **Fire Protection Services.** Fire protection and emergency services are provided by the County of Los Angeles Consolidated Fire Protection Department (County Fire Department). Current and future development would receive all first response services from Fire Station No. 23, located at 9548 E. Flower Street, approximately 0.29 miles to the northwest. Fire Station No. 98, which is located at 9814 Maplewood Avenue would provide backup personnel and equipment. All new development would be required to demonstrate that adequate fire flow in the range of 2,500 gallons per minute (gpm) at a minimum residual pressure of 20 psi, for a minimum duration of two hours, is available to meet Los Angeles County Fire Department Regulation 8 Fire Flow Criteria.

4.9.4 ALLOWABLE USES

The following uses are allowable either as a permitted use, a use that is permitted with the approval of a conditional use permit, or a prohibited use. Any use that is not listed as a permitted use, or a conditionally permitted use, and is not specifically listed as prohibited, may be allowed pursuant to the provisions of this section. It is the intent of this overlay zone is to allow a mixture of land uses

that are complimentary to each other, compatible with residential uses within the boundaries of the overlay zone and the adjoining single-family residential neighborhood to the east, and to create a vibrant and exciting village atmosphere. The range of uses is specifically intended to attract new investment into downtown Bellflower, specifically, the Bellflower Village Overlay Zone.

The following uses are provided in a matrix format.

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
A.	Commercial Uses			
1.	General Commercial			
	Adult businesses			X
	Ambulance service			X
	Animal care			
	Animal/pet sales			X
	Animal boarding & day care			X
	Animal grooming, indoor only		X	
	Animal medicine/veterinary		X	
	Animal training			X
	Animal/pet supplies, indoor only	X		
	Arcades (see video game arcades)			
	Art galleries/museums ¹		X	
	Athletic, health & fitness clubs			X
	Automobile related sales & service			
	Automobile repair & service			X
	Auto customization & service			X
	Auto parts & accessories			X
	Automobile sales, new & used			X
	Auto stereo/alarm installation			X
	Automobile carwash			X
	Banks and savings & loans	X		
	Automated teller machines	X		
	Drive thru teller			X
	Barbershops	X		

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY	PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
Beauty shops and nail salons	X		
Billiards/darts			
Billiard parlors – primary use			X
Billiards – accessory, 2 tables max	X		
Billiards – accessory, more than 2		X	
Darts – accessory only	X		
Bridal shops	X		
Catering services		X	
Carpet, flooring & tile, retail only	X		
Check cashing and cash advance			X
Child day care			X
Copy centers, reproduction centers	X		
Dance studios			X
Drive thru – component to retail			X
Dry cleaners	X		
DVD & video rental (no adult businesses)	X		
Fortunetelling			X
Game arcades		X	
Interior decorating showrooms	X		
Laundromats			X
Locksmiths	X		
Mailboxes, mailroom, postal stores	X		
Martial arts studios			X
Masseur-masseuse, massage establishments, acupuncture spas, day spas and similar uses			X
Mortuary ¹		X	
Optician, optometrists, optical shops	X		
Outdoor vendor carts		X	
Pawn shops			X
Photography studios	X		

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
	Picture frames – sales/service	X		
	Recycling facilities, stand alone			X
	Sporting goods	X		
	Storage, wholesale or retail			X
	Tattoo shops/body piercing			X
	Tire stores, new or used			X
	Tuxedo shops, sales and rental	X		
	Upholstery shops			X
2.	Specialty Commercial			
	Alcohol			
	Cocktail lounge, bar, pubs, etc.		X	
	On-site, accessory to food uses		ACUP ²	
	Off-site sale, liquor, beer & wine		X	
	On-site, accessory all other uses		X	
	Antique shops	X		
	Apparel and clothing stores	X		
	Appliance stores – large, retail only	X		
	Appliance stores – small, retail only	X		
	Art stores/artist supplies	X		
	Bakeries, retail only	X		
	Bicycle shops	X		
	Bookstores, new & used	X		
	Camera & photography, retail	X		
	Candy & confectioneries	X		
	Computer games/accessories - sales	X		
	Convenience stores/mini mart	X		
	Costume shops, sales & rental	X		
	Drug & pharmacy	X		
	Dry goods	X		
	Electronics/computers - retail	X		

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
Food uses – retail (non-drive thrus)				
Coffeehouse/tea		X		
Delicatessens		X		
Food court/fast food		X		
Grocery, fresh produce/meat		X		
Health/organic/vitamins		X		
Ice cream, yogurt, smoothies		X		
Restaurants/cafes		X		
Fabric/linens		X		
Florists/floral		X		
Furniture/home goods		X		
Gift shops/stationery/greeting cards		X		
Gun sales – accessory use		X		
Hardware/home improvement				
Less than 3,000 sq. ft. floor area		X		
3,001 sq. ft. floor area & greater			X	
Outdoor sales area			X	
Hobby & crafts sales		X		
Hotel & motels				X
Internet/computer access				
Internet café – primary activity			X	
Internet – accessory, 3 or less		X		
Internet – accessory, 4 or more			X	
Internet access only – wireless		X		
Internet gaming			X	
Jewelry/gems/coins sales		X		
Leather goods/luggage		X		
Liquor stores (see Alcohol)				
Medical supplies, retail only		X		
Microbrewery–onsite (see Alcohol)				

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
	Music			
	Instrument sales – new or used	X		
	Instrument repair – accessory	X		
	Lessons – accessory	X		
	Lessons – primary		X	
	Music supplies	X		
	Recorded music – new or used	X		
	Recording studio		X	
	Sound equipment sales	X		
	Office supplies	X		
	Paint & wallcovering – retail	X		
	Party supply stores	X		
	Patio/bbq furniture/equipment	X		
	Shoe stores, including repair	X		
	Smoke/cigar shop		X	
	Electronic cigarette and/or electronic liquid shop		X	
	Surplus stores (army/navy)	X		
	Thrift store			X
	Toy store	X		
	Trophy shop	X		
	Watch & clock sales/repair	X		
3.	Commercial Office/Administrative			
	Administrative/professional offices			
	Accountant, tax preparer	X		
	Administrative offices, general	X		
	Architecture/designer	X		
	Attorneys/legal services	X		
	Computer programming/software	X		
	Consulting offices	X		
	Engineer	X		

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
	Financial services	X		
	Insurance	X		
	Real estate	X		
	Travel agencies/ticket agencies	X		
	Medical offices			
	Chiropractic	X		
	Dental	X		
	General and specialty medical	X		
B.	Residential			
1.	Live/work (no ground floor residential)	X		
2.	Lofts		X	
3.	Mixed use (no ground floor residential)	X		
4.	Multiple-family			
	Attached		X	
	Detached		X	
5.	Single-family			X
C.	Other Uses/Miscellaneous			
	Banquet halls/public assembly		X	
	Religious facilities ¹		X	
	Fraternal halls/clubs		X	
	Parking lots/parking structures	X		
	Public/private utility/communication			X
	Private storage/mini storage			X
	Swap meet, indoor & outdoor			X

TABLE 4.14 BVOZ Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	PROHIBITED USE
D.	Uses not listed			
	<p>In order to ensure that the Bellflower Village Overlay Zone permits all similar uses, if a use is not specifically listed as a permitted, per-mitted with a conditional use permit, or pro-hibited, then the Director must make a de-termination if the proposed use is similar to a use listed in the above matrix.</p> <p>The determination of the Director must be based upon the evaluation of the characteristics of the proposed use with the goals, objectives and policies of the General Plan, as well as with the goals and objectives of the Bellflower Village Overlay Zone. The Director must make a written report of his/her findings to the Planning Commission.</p> <p>The determination of the Director may be appealed pursuant to the provisions of Chapter 17.112 of the Bellflower Municipal Code.</p>	X	X	X

Note:

1. Art galleries, museums, mortuaries, and religious facilities may be allowed with a Minor Conditional Use Permit if the total gross floor area proposed is less than 5,000 square feet.
2. ACUP = Administrative Conditional Use Permit

4.9.5 DEVELOPMENT STANDARDS

The Bellflower Village Overlay Zone contains special development standards that pertain specifically to the properties that are within the boundaries of the Bellflower Village Overlay Zone district. These special development standards are allowed to deviate from the City's conventional zoning standards in some categories. Where noted, the City's conventional zoning standards apply and remain in full force.

The development standards set forth for residential, commercial, office and mixed-use development are intended to provide for flexibility in site design, as well as methods to integrate commercial and residential uses within boundaries of the Bellflower Village Overlay Zone. These development standards are the minimum standards for development. The Bellflower Village Overlay Zone allows for a mixture of commercial, office and residential (attached or "live/work").

The architecture and site planning for any future development will be reviewed and approved pursuant to the following development standards.

TABLE 4.15 BVOZ Development Standards Table

DEVELOPMENT CRITERIA		DEVELOPMENT STANDARD	SPECIAL PROVISIONS AND COMMENTS
A.	Lot Area Standards.		
	The minimum lot area required to develop on any lot or parcel	5,000 square feet	
B.	Required Yards Standards.		
	Setback from the public right-of-way or street	5 feet	
	Setback from an interior property line	5 feet	
C.	Building Heights/Allowable Stories.		
	The maximum building height for any building or structure	3 stories or 45 feet	Nonhabitable, projections exceeding 45 feet, if architecturally integrated into the building design.
	Structures greater than 3 stories or 45 feet, but in no case more than 4 stories may be allowed with a C.U.P.	4 stories	Pursuant to the approval of a C.U.P.
D.	Relationship to Public/Private Streets and Alleys.		
	Relationship to public streets		All building facades and elevations must have architectural treatments incorporating a minimum of at least 3 different building materials.

TABLE 4.15 BVOZ Development Standards Table

DEVELOPMENT CRITERIA		DEVELOPMENT STANDARD	SPECIAL PROVISIONS AND COMMENTS
	Relationship to private streets and alleys		Building facades and elevations facing private streets and alleys must have full architectural treatment, with an emphasis on pedestrian scale and access.
E. Required Parking.			
	Commercial uses	The provisions of BMC Chapter 17.88 apply, except a "parking space" may not be less than 9'-0" in width by 18'-0" in length	
	Alternative off-street parking requirement		Required off-street parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council.
	Residential uses	Minimum of 2 covered spaces per unit	Each residential unit must have a minimum of 2 covered or enclosed, on-site spaces per unit.
	Guest parking	The provisions of BMC Chapter 17.88 apply, except a "parking space" may not be less than 9'-0" in width by 18'-0" in length	Required guest parking may be transferred to designated off-site parking locations pursuant to the provisions of a DDA or similar agreement approved by the City Council.
F.	Fences, Walls, Landscaping Standards		
	The provisions of BMC Chapter 17.48 apply	A Master Landscape and/or Hardscape plan is required. The Project review Committee may	

TABLE 4.15 BVOZ Development Standards Table

DEVELOPMENT CRITERIA		DEVELOPMENT STANDARD	SPECIAL PROVISIONS AND COMMENTS
		adjust these standards on a project-by-project basis, if it can be demonstrated that the adjustments to the development standards will result in a superior project, superior design and superior site design.	
G.	Lighting Standards	The provisions of BMC Chapter 17.48 apply	
H.	Mechanical Equipment/Trash Service	The provisions of BMC Chapter 17.48 apply	
I.	Sign Standards		
	Design, height, materials, illumination, maintenance, etc., of signs	Determined by the Master Sign Plan in accordance with BMC Chapter 17.68	
	Required Master Sign Plan		The Project review Committee must approve any Master Sign Plan. See BMC Chapter 17.68.

4.9.6 ATTACHMENTS.

FIGURE 4.18 BVOZ Vicinity Map

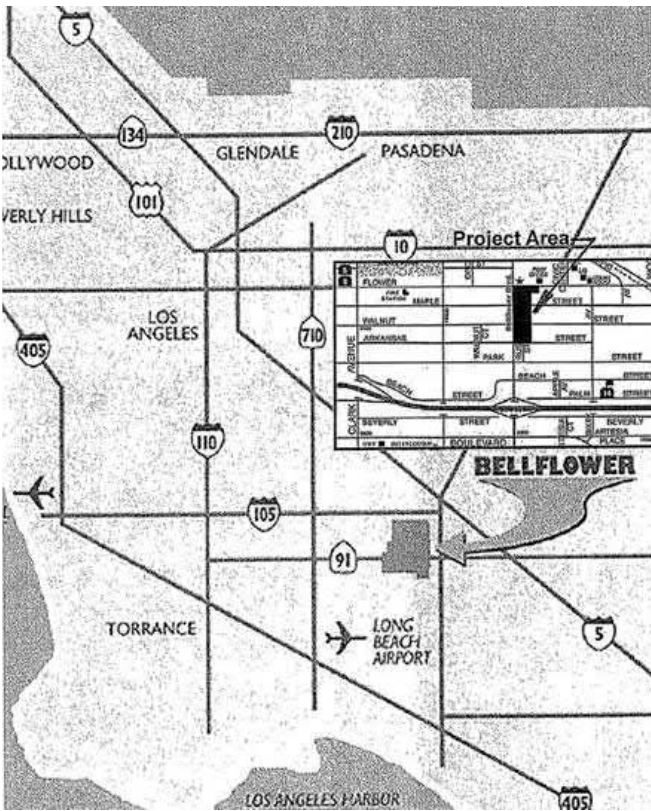
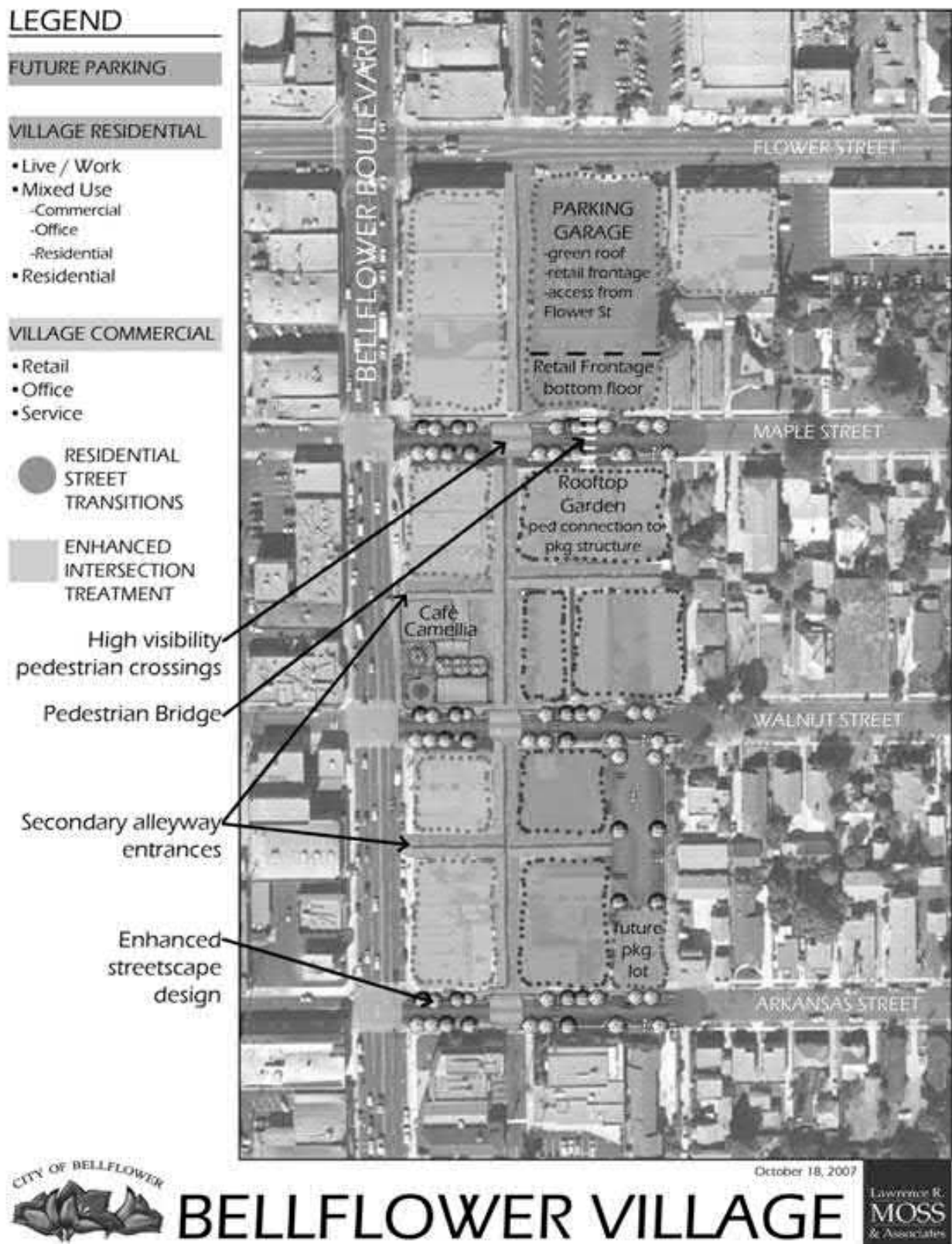


FIGURE 4.19 BVOZ Overlay Zone Boundaries



FIGURE 4.20 BVOZ Conceptual Illustration

4.10 BELLFLOWER VILLAGE OVERLAY ZONE-NORTH (BVOZ-N)

4.10.1 PURPOSE AND INTENT

The size and location of the Bellflower Village Overlay Zone—North (this Overlay Zone) creates a unique development opportunity within the City. The project site is located within the downtown area of Bellflower and is fully developed. The site is also adjacent to fully developed commercial to the north, south and west, and a mixture of land uses to the east, comprising of commercial, multiple-family residential and public uses. Because of land values, development standards constraints and other factors that affect the reuse of developed property, it is unlikely the properties within the boundaries of the overlay zone would recycle within a reasonable timeframe without the development and economic incentives provided within the provisions of the Bellflower Village Overlay Zone—North.

- A. **Purpose and Intent.** The Town Center District was created in May 13, 1996, with the intent on ensuring quality development and design within the downtown area of the City of Bellflower. Downtown Bellflower is an important area of Bellflower, providing a tangible link to the City's rich historical past, geographic central focal point, a recognizable town center and a hope for renewed civic vitality. However, current land use and development constraints, when combined with the current marketplace and the realities of real estate, provide an atmosphere where conventional and traditional zoning techniques may not accomplish the goals set forth in the Town Center Zoning District within a reasonable timeframe. Therefore, land use, development and other regulatory tools need to be created to encourage timelier private sector investment and creativity, as well as ensuring and safeguarding high design standards and quality construction methods and materials are utilized.

The Bellflower Village Overlay Zone—North recognizes there is a geographic portion of downtown Bellflower that has the potential of becoming a creative, vital, pedestrian-friendly and visionary neighborhood. Existing conventional zoning and land use standards have not been successful in promoting that transformation. Therefore, a more creative, flexible and focused land use plan needs to be created. The Bellflower Village Overlay Zone—North is the land use tool that is proposed to create that incentive for the recycling and reuse of land within the Overlay Zone boundaries.

- B. **Relationship to Other City Plans and Documents.** In May 28, 1996, the City adopted Ordinance No. 869 (BMC Chapter 17.48), which established the Bellflower Town Center District. It was the intent of the Town Center District to create the catalyst of a design and economic revitalization of the City's traditional downtown district. The City of Bellflower General Plan (Updated 1997) designates the project site as "Town Center," which is essentially a "commercial" land use designation.

In May 12, 2008, the City adopted Ordinance No. 1161, which established the Bellflower Village Overlay Zone. That is the City's first Overlay Zone; its boundaries are on the east side of Bellflower Boulevard, from Flower Street at the north and Arkansas Street at the south; and it extends easterly approximately 300 feet. The Bellflower Village Overlay Zone encourages the creative reuse of existing properties into mixed use residential/commercial development, conventional commercial development, public open spaces and other innovative development concepts.

The development standards and design guidelines in this Overlay Zone provide an alternative set of development standards and allowable uses beyond those contained in the underlying zoning designation over which this Overlay Zone is applied. Where specifically identified, the provisions of this Overlay Zone may be used instead of the underlying zoning designation. If this Overlay Zone is silent in regards to certain development standards, design guidelines or review procedures, then those standards and guidelines contained in the BMC apply.

- C. **Goals.** The Bellflower Village Overlay Zone—North will guide future redevelopment and the recycling of land by accomplishing the following:
1. Encourage the recycling of existing developed properties into contemporary, vital and interesting development that will enrich downtown Bellflower;
 2. Provide for a more flexible regulatory procedure by which the objectives of the City's General Plan and BMC can be realized;
 3. Encourage creative approaches to the use of land through variation in siting of buildings and the appropriate mixing of commercial land uses and activities;
 4. Encourage the recycling of property by providing creative and unique economic opportunities to develop and use property;
 5. Create a unique and vibrant neighborhood within the downtown area of Bellflower that will encourage further private investment in the City;
 6. Enhance the appearance of the community through creative design of buildings, structures, and facilities;
 7. Eliminate and prevent the spread of blight;
 8. Revitalize, redevelop, and upgrade those parcels within the Bellflower Village Overlay Zone—North area;
 9. Strengthen the City's economic base.

This Overlay Zone is a land use policy, regulatory and land use intensity document that establishes policies, development standards, allowable uses, uses allowed with the approval of a conditional use permit and, design guidelines to regulate and guide future redesign and redevelopment of this Overlay Zone area. This Overlay Zone establishes the type, location, intensity and character of uses for redevelopment and the recycling of land to occur. This Overlay Zone seeks to shape future development by responding to the existing physical and economic constraints on the site and coordinating future development, parking design and circulation and public facility improvements. Future development and development plans proposed on property within this Overlay Zone is consistent with the provisions within this Overlay Zone.

4.10.2 DESCRIPTION OF BELLFLOWER VILLAGE OVERLAY ZONE—NORTH

- A. **Location of Overlay Zone.** The Bellflower Village Overlay Zone—North encompasses an approximate four block area of downtown Bellflower, extending along both sides of Bellflower Boulevard from Flower Street on the south to the MTA right-of-way to the north. The amount of

land that is within the boundaries of the Overlay Zone is approximately 1,066,556 square feet, or 24.5 acres. That area is further defined by multiple streets bisecting the overlay area on an east/west axis and dividing it into a series of smaller geographic areas defined by the existing City street pattern. Furthermore, the easterly-most boundary of this Overlay Zone is Civic Center Drive, where the Bellflower Civic Center is located. Refer to Figure 4.21 and 4.22 for a Vicinity Map, which presents the regional location of this Overlay Zone and. Location Map, which presents the location of this Overlay Zone area.

B. **Existing Conditions.** The overlay area is fully developed with existing buildings and land uses. The predominate land use is commercial, with only one residential use existing within the boundaries of this Overlay Zone. The land area within this Overlay Zone is divided into four separate geographic areas. For the purposes of addressing the existing development within this Overlay Zone, there are four planning areas within this Overlay Zone referred to as “Area No. 1,” “Area No. 2,” “Area No. 3,” and “Area No. 4.” The conditions within each one of those areas are as follows:

1. Area No. 1 (Figure 4.23). That area spans both sides of Bellflower Boulevard from the MTA right-of-way on the north to Oak Street on the south. That area extends on an east/west axis from the westerly boundary of the City’s public parking lot on the west, to the apex that is formed where Oak Street and the MTA right-of-way meet. The buildings and uses within Area No. 1 are as follows:
 - a. Commercial building – 16337 Bellflower Blvd. (APN: 7106-019-023). That property is the northerly-most property within the Overlay Zone and is shaped like a triangle. This property is bounded by Pacific Ave to the north, Mayne St. on the south and Bellflower Blvd. to the east. This commercial building has a parking lot.
 - b. Commercial buildings – 9742 Mayne St.; 16411—16435 Bellflower Blvd. and 9751 Oak St. (APN: 7106-020-900; 7106-020-040; 7106-020-039; 7106-020-002; 7106-020-036; 7106-020-005; and 7106-020-032). Those properties are bounded by Mayne Street to the north, Oak Street to the south and Bellflower Boulevard to the east. The City’s public parking lot is to the west, adjacent to single-family residential property. That block is characterized by commercial structures facing Bellflower Boulevard with parking lot access to the rear, or west-facing facades. There are multiple properties/buildings comprising this block.
 - c. Commercial buildings and public uses – 16400—16436 Bellflower Blvd.; and 9825 and 9837 Oak St. (APN: 7109-010-906; 7109-010-905; 7109-010-022; 7109-010-021; 7109-010-020; 7109-010-902; 7109-010-904; and 7109-010-907). Those properties are located on the east side of Bellflower Blvd., forming a triangle with Oak St. as the base extending easterly until it intersects with the MTA right-of-way, which forms the hypotenuse. That block has several City -owned properties, such as the public parking lots, the small transit center. In addition, the restored Pacific Electric Train Depot is immediately to the north of that block. Facing Bellflower Blvd. are several commercial structures with access to Bellflower Blvd. and the public parking lots to the east.

2. Area No. 2 (see Figure 4.24). This area is located entirely on the west-side of Bellflower Blvd. spanning three blocks; extending from Oak St. to the north to Flower St. to the south. The City public parking lots are located along the westerly portion of this area, which abuts predominantly single-family residential properties farther to the west. In addition, this area has several significant public amenities, such as the Friendship Square plaza and the adjoining “Pirate Park” playground, which are located within the portion of Belmont St. that intersects Bellflower Blvd. to the east and extends westerly to the approximate terminus of the public parking lots. Also, within this area is the “Laurel St.” public improvements that are adjacent to the intersection with Bellflower Blvd. The buildings and uses within Area No. 2 are as follows:
 - a. Commercial buildings – 9731 Belmont St. and 16503—16537 Bellflower Blvd. (APN: 7106-023-904; 7106-023-001; 7106-023-002; 7106-023-905; 7106-023-004; 7106-023-005; and 7106-023-006). Those properties are located between Oak St. to the north and Friendship Square/Pirate Park to the south (Belmont St. right-of-way). Public parking is located to the west. The commercial properties and buildings have access onto Bellflower Blvd. and to the public parking lot to the west.
 - b. Commercial buildings – 16601—16641 Bellflower Blvd. (APN: 7106-024-902; 7106-024-904; 7106-024-002; 7106-024-003; 7106-024-004; 7106-024-005; and 7106-024-032). Those properties are located between Friendship Square/Pirate Park (Belmont St. right-of-way) to the north and Laurel St. to the south. Bellflower Blvd. abuts these properties to the east and the City’s public parking lots are located to the west. It should be noted that the portion of Laurel St. that abuts that block has been enhanced with pedestrian features to create wider public sidewalks and other pedestrian amenities.
 - c. Commercial buildings – 16701—16715 Bellflower Blvd. and 9721—9751 Flower St. (APN: 7106-027-903; 7106-027-001; 7106-027-027; 7106-027-003; 7106-027-004; 7106-027-019; 7106-027-020; 7106-027-021; 7106-027-022; and 7106-027-023). That block extends from Laurel St. to the north to Flower St. to the south. Bellflower Blvd. forms the easterly boundary of this block. Commercial properties facing onto Flower St. extend westerly for approximately four hundred thirty (430) feet from Bellflower Blvd. A City public parking lot is located to the north of the commercial properties facing onto Flower St. and west of the commercial properties facing onto Bellflower Blvd.
3. Area No. 3 (see Figure 4.25). This area is located on the east side of Bellflower Blvd. between Oak St. to the north and Belmont St. to the south. This area extends easterly from Bellflower Blvd. to include the properties on the east side of Adenmoor Ave. This area includes several notable public amenities, the Town Center Civic Plaza is located in the approximate center of this area, and at the northeast corner of Bellflower Blvd. and Belmont St., is the Town Center Gateway Plaza. The City public parking lot is located to the east of the commercial buildings that face onto Bellflower Blvd., and the Town Center Civic Plaza. In addition, City public parking is located at the southeast corner of Oak St. and Adenmoor Ave. and Belmont St. and Adenmoor Ave. The buildings and uses within Area No. 3 are as follows:
 - a. Commercial buildings – 16500—16552 Bellflower Blvd. (APN: 7109-010-910; 7109-010-017. 7109-010-002; 7109-010-003; 7109-010-031; 7109-010-007; 7109-010-008; 7109-

010-009; and 7109-010-909). Those properties are located on the east side of Bellflower Blvd. between Oak St. on the north and Belmont St. to the south. That block is predominately developed with commercial structures facing onto Bellflower Blvd., with access to the alley immediately to the east and the public parking lot beyond. In addition, that block contains the Town Center Gateway Plaza located at the northeast corner of the Bellflower Blvd. and Belmont St. intersection.

- b. Commercial buildings, public parking and Town Center Civic Plaza – 9826 Oak St.; and 9819—9833 Belmont St. (APN: 7109-010-903; 7109-010-801; 7109-010-802; 7109-010-900; 7109-010-901; 7109-010-011; 7109-010-908; and 7109-010-013). Those properties are located east of Bellflower Blvd.; between Oak St. to the north, Belmont St. to the south and Adenmoor Ave. to the east. Those properties include commercial buildings facing onto Belmont St. to the south with their northerly facade facing onto the City's Town Center Civic Plaza. The City public parking lot is located along the westerly portion of that block.
- c. Commercial building and public parking – 16518 Adenmoor Ave. and 9847 Belmont St. (APN: 7109-011-801; 7109-011-017. 7109-011-002; and 7109-011-909). Those properties are located along the east side of Adenmoor Ave., between Oak St. to the north and Belmont St. to the south. The MTA right-of-way also abuts a portion of that block to the northeast. That block comprises predominately of public parking and a commercial building.

- 4. Area No. 4 (see Figure 4.26). This area is located on the east side of Bellflower Blvd. between Belmont St. to the north and Flower St. to the south. This area extends easterly along Belmont St. approximately two hundred fifteen (215) feet and along Flower St. approximately four hundred fifty (450) feet. This area contains commercial buildings and public and private parking lots. The buildings and uses within Area No. 4 are as follows:

- a. Commercial buildings – 16620—16730 Bellflower Blvd. (APN: 7109-013-003; 7109-013-004; 7109-013-005; 7109-013-006; 7109-013-007; 7109-013-008; 7109-013-009; 7109-013-002; 7109-013-001; 7109-013-011; 7109-013-010; 7109-013-012; 7109-013-013; and 7109-013-800). Those properties are located on the east side of Bellflower Blvd. between Belmont St. on the north and Flower St. to the south. That block is predominately developed with commercial structures facing onto Bellflower Blvd., with access to the alley immediately to the east and the public and private parking lots beyond.
- b. Commercial buildings and public and private parking – 9812 and 9820 Belmont St.; and 9815 and 9835 Flower St. (APN: 7109-013-015; 7109-013-016; 7109-013-037; 7109-013-036; 7109-013-801; and 7109-013-900). Those properties are located to the east of Bellflower Blvd. facing commercial properties between Belmont St. to the north and Flower St. to the south. That block extends along Belmont St. approximately two hundred one (201) feet and along Flower St. approximately four hundred fifty (450) feet, to form an irregular shape along the easterly edge.

- C. General Plan and Zoning Designations. The General Plan designation for this Overlay Zone area is Mixed-Use. The Zoning designation for this Overlay Zone area is predominately Town Center.

There are several parcels within the boundaries of the Bellflower Village Overlay Zone—North that have a Commercial Zoning designation. However, the Town Center District is essentially a commercial and specialty office zoning designation. The Town Center District seeks to promote quality, unique and vibrant retail commercial uses.

4.10.3 DEVELOPMENT CONCEPT OF BELLFLOWER VILLAGE OVERLAY ZONE—NORTH

- A. Development Concept. This Overlay Zone concept is an opportunity to utilize an alternative zoning tool for those properties that are within the boundaries of the Overlay Zone. The property owner would have the choice of developing property pursuant to the provisions of the underlying zone or developing property pursuant to the Bellflower Village Overlay Zone—North.
- B. The intent of this concept is to provide for a unique and flexible zoning tool that would promote the creative development of property within the boundaries of the Bellflower Village Overlay Zone—North pursuant to the goals and objectives of the “Mixed-Use” Land Use designation of the Bellflower General Plan. Essentially, the urban design objectives of this development concept are as follows:
 1. Mixed-Use Theme. Promote development pursuant to the goals and objectives in the “Mixed-Use” Land Use designation of the Bellflower General Plan;
 2. Create a Vibrant Commercial Destination. Create an attractive and unique neighborhood that would attract travelers and commuters who use Bellflower Boulevard as a regional transportation corridor. In addition, the Bellflower Village neighborhood would further the fulfillment of the “Connection of Plazas” concept adopted in the Downtown Bellflower Revitalization Vision Strategy (January 2003).
 3. Protect the Adjacent Residential Neighborhoods. Through innovative and thoughtful design techniques, maintain and preserve the integrity of the existing single-family residential neighborhood that is immediately west of the Bellflower Village Overlay Zone—North, and the multiple-family residential neighborhood that is immediately east of the Bellflower Village Overlay Zone—North.
 4. Village Design Theme. Allow for the use of varying architectural design themes, but require that each design is thoughtfully blended and is compatible with adjoining and adjacent development. The urban design goal is to foster a vibrant, unique and exciting, pedestrian-friendly village atmosphere that creates a “sense of place,” irrespective of the mix and proportion of commercial and residential uses that will eventually be developed and occupy the Bellflower Village Overlay Zone—North district.
 5. Incremental Development. Base the approval of individual and incremental development projects on a single concept plan that provides defined ranges of flexibility to accommodate market changes.
 6. Public Spaces. Provide for public spaces that serve both the commercial visitor as well as the local resident. Public spaces would further the goals of the “Connection of Plazas” concept adopted in the Downtown Bellflower Revitalization Vision Strategy (January 2003).

7. **Land Use Flexibility.** Maintain the land use and development options so that as individual properties become available for development, there are choices between commercial, residential or mixed use, as provided in current market conditions. Although “live/work” and “mixed use” development is not a strict requirement for development within this Overlay Zone, it is strongly encouraged that new develop integrate these types of development techniques into any proposed project. The overall design goal is to create pedestrian walk-ability within the boundaries of this Overlay Zone and to contribute to the unique “landmark” place-making capabilities of this Overlay Zone.
- C. **Conceptual Development Plan.** This Overlay Zone development plan proposes to use the unique development and land use pattern that currently exists within the boundaries of this Overlay Zone. This Overlay Zone concept is intended to allow for the individual development of existing parcels that would eventually result in the development of the Bellflower Village land use concept. However, there are certain common features that serve to unify this entire Overlay Zone. Those common features allow for the potential development to act in a complimentary fashion that reinforces and benefits each development. Therefore, these common features act in a manner which will result in a greater level of land use effectiveness and efficiency.
 - D. The existing parcel and ownership configuration within the boundaries of the Bellflower Village Overlay Zone—North precludes the option for a single unified redevelopment effort. Therefore, the development scheme must allow for a single parcel or small collection of parcels, to development on an incremental basis, but with regulatory flexibility to make accommodations for significant development standards, such as guest parking. This regulatory flexibility would economically encourage the private sector to invest and redevelop existing properties within the boundaries of this Overlay Zone, but also address the fundamental needs to provide for those development standards, such as guest parking.
 - E. However, one asset the Bellflower Village Overlay Zone—North area possesses that is less abundant in other portions of the downtown area of the City, are considerable public parking lots. Those public parking lots are typically located behind the Bellflower Boulevard-facing commercial buildings.
1. **Future Land Uses and Density.** The Bellflower Village Overlay Zone—North comprises an area within the City of eight (8) city-blocks, running north to south and east to west. On an east to west axis, the overlay zone span is approximately nine hundred sixty-two (962) feet, if measured along Flower Street spanning an area on both sides of Bellflower Boulevard. Within this area, the land use development pattern is predominately commercial. There are also two (2) existing north/south alleys that run parallel to Bellflower Boulevard, and separate those parcels with Bellflower Boulevard street-frontage from the remainder of property within the overlay zone district. Project applicants who choose to utilize the development standards in the Bellflower Village Overlay Zone—North may intensify the development potential of the individual parcel of land, beyond what the underlying zoning development standards would allow. The “intensification” of development potential is intended to create an economic incentive for the existing properties that are within the boundaries of this Overlay Zone to redevelop through privately funded development efforts. Such intensification may be in the

form of greater building height, the ability to incorporate residential uses into the project, or the ability to transfer some off-street parking obligations to another location.

2. **Access and Circulation.** Bellflower Village Overlay Zone – North is accessible primarily from Bellflower Boulevard. Bellflower Village Overlay Zone – North area is also accessible from Flower Street to the south. Mayne Street, Oak Street, Belmont Street and Laurel Street provide access to the interior of the Bellflower Village Overlay Zone – North area. The north/south running alleys intersects with Flower Street on the south, and Mayne Street on the north (on the west side) and the MTA right-of-way (on the east side). This alley provides access to most of the properties within the Overlay Zone. There are four points of entry into the interior of the Bellflower Village Overlay Zone—North. Those are the intersections of Bellflower Boulevard and Mayne Street, Bellflower Boulevard and Oak Street, Bellflower Boulevard and Belmont Street, and Bellflower Boulevard and Laurel Street. Furthermore, access into the single-family residential neighborhood to the west is accessible from Mayne Street, Oak Street, Belmont Street and Laurel Avenue. Residential uses on the east-side of the Bellflower Village are less numerous and tend to be multiple-family residential properties. The location where the Bellflower Village Overlay Zone—North abuts the aforementioned residential neighborhoods are transition points and as such, appropriate design considerations should be incorporated into any new development that abuts those transition points.
3. **Parking.** The Bellflower Village Overlay Zone—North has a series of public parking lots that can be used to assume some or all of the required off-street parking for new development within the Overlay Zone District. Some commercial properties are within the public parking district boundaries, and therefore, are able to utilize the public parking lots for their on-site parking requirements for commercial uses. However, some properties within the existing public parking district may be candidates for development intensification pursuant to the Bellflower Village Overlay Zone—North standards. Thus, each development that proposes to transfer some, or all, of their resulting required off-street parking obligation to an existing public parking lot are required to demonstrate the feasibility of transferring their parking obligation.

Unless otherwise approved through a development agreement with the City, residential projects will be required to provide on-site parking for each residential unit; however, the guest parking requirements may be a candidate for transfer to the adjoining public parking lots. Commercial uses that are not in the public parking district may propose to transfer up to their entire on-site parking obligation to adjoining parking lots. However, they will be encouraged to maintain handicapped accessible parking on their property, as well as loading areas

The on-site parking requirement for each use will be those that are established in the BMC. Each development that proposes to utilize the public parking lots to meet some, or all, of their on-site parking obligation will need to enter into an agreement with the City, which will be approved on a case-by-case basis with access and proximately to the parking lots being one consideration.

4. **Public Spaces.** The development of easily accessible, and visually appealing, public spaces are very important for the overall success of the Bellflower Village Overlay Zone—North.

Within the Bellflower Village Overlay Zone—North, there are several locations of plazas and pedestrian areas. In addition, new development and redevelopment of existing properties will be encouraged to design public open space features into the development design. The intent of this public space feature is twofold. The first is to create a visually significant design element that has a prominent location in order to attract the attention of passing commuters using Bellflower Boulevard. The second reason is to provide pedestrian amenities for visitors of downtown Bellflower, which will further enhance the visitation experience. For the development incentives provided by the Bellflower Village Overlay Zone—North to be available, a project proposed within this Overlay Zone must include a public open space amenity, or some form of physical interface for the pedestrian. Such features may include, but not be limited to:

- a. **Formal Plazas.** A formal plaza would be a publicly accessible open space which has a design that is influenced by classical urban planning design. A formal plaza would typically include some sort of central water fountain and/or symmetrical landscaping.
 - b. **Urban Gardens.** An urban garden can be located on the ground level, or on upper levels of a structure. Urban gardens include ornamental landscaping arranged in raised or at-grade planters or planting areas, potted plants and trees.
 - c. **Covered Colonnades.** Colonnades are linear in design and generous in depth. The intent is to provide a comfortably wide, covered pathway that is adjacent to the openings of a building. Sometimes the second floor of a building is utilized to create the “covered” element of the colonnade.
 - d. **Sidewalk Dining.** Sidewalk dining may occur wherever a sidewalk space is ample enough to accommodate dining furniture without impeding pedestrian access of the sidewalk. Sidewalk dining may be defined with a railing or planters, or be open and accessible.
 - e. **Pedestrian Alleys and Walkways.** A pedestrian alley or walkway is typically a “lane” that does not follow the alignment of a vehicular street, but provides a pedestrian access to either a public space or some other feature within the interior of a development. Pedestrian alleys or walkways must be designed in such a manner so as to be inviting to pedestrians. Therefore, issues such as lighting, security, line of sight, cleanliness and visual appeal are important considerations to a well-designed pedestrian alley or walkway. Sometimes public art, street furniture and access to shops and public spaces are features of pedestrian alleys and walkways.
5. **Architectural Treatments and Features.** The Bellflower Village Overlay Zone—North does not specify a designated architectural style or theme. Instead, the Bellflower Town Center District Design Guidelines apply. However, it is important whatever architectural theme is chosen for a development, that the architectural design be sensitive to the “pedestrian scale” of the ground level building elevation.
 6. **Projections into the Public Right-of-Way.** Minor and incidental projections into or over the public right-of-way may be allowed by encroachment permit. Such minor and incidental projections into or over the public right-of-way may include the following:

- a. Window and door awnings;
- b. Decorative and/or ornamental lighting;
- c. Architectural features and enhancements;
- d. Projecting blade signs and/or hanging “pedestrian-oriented” signs;
- e. Ornamental railings, planters or similar features in conjunction with outside dining; and
- f. Public art.

4.10.4 ALLOWABLE USES

The following uses are: (A) allowable either as a permitted use; or (B) a use that is permitted with the approval of a conditional use permit; (C) a use that is permitted with the approval of an Administrative Conditional Use Permit; or (D) a prohibited use. Any use that is not listed as a permitted use, or a conditionally permitted use, or an administrative conditionally permitted use and is not specifically listed as prohibited, may be allowed pursuant to the provisions of this section. It is the intent of this Overlay Zone to allow a mixture of land uses that are complimentary to each other, compatible with residential uses within the boundaries of the overlay zone and the adjoining residential neighborhoods to the west and east, and to create a vibrant and exciting village atmosphere. The range of uses is specifically intended to attract new investment into downtown Bellflower, specifically, the Bellflower Village Overlay Zone—North.

The following uses are provided in a matrix format.

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
A.	Commercial Uses.				
1.	General Commercial				
	Adult businesses				X
	Ambulance service				X
	Animal care				
	Animal/pet sales				X
	Animal boarding & day care				X
	Animal grooming, indoor only		X		
	Animal medicine/veterinary		X		
	Animal training				X
	Animal/pet supplies, indoor only	X			
	Arcades (see Game arcades)				

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY	PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
Art galleries/museums		X		
Art galleries/museums with total gross floor area of less than 5,000 sq. ft.			X	
Athletic, health and fitness clubs				X
Accessory use for residential tenants developed within a comprehensive mixed-use project	X			
Automobile related sales & service				
Automobile repair & service				X
Auto customization & service				X
Auto parts & accessories				X
Automobile sales, new & used				X
Auto stereo/alarm installation				X
Automobile carwash				X
Banks and savings & loans	X			
Automated teller machines	X			
Drive thru teller				X
Barbershops	X			
Beauty shops and nail salons	X			
Billiards/darts				
Billiard parlors – primary use				X
Billiards – accessory, 2 tables max	X			
Billiards – accessory, more than 2		X		
Darts – accessory only	X			
Bridal shops	X			
Catering services		X		
Carpet, flooring & tile, retail only	X			
Check cashing and cash advance				X
Child day care, 7 or more children		X		
Copy centers, reproduction centers	X			
Dance studios				X

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY	PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
Drive thru – component to retail				X
Dry cleaners	X			
DVD & video rental (no adult businesses)	X			
Fortunetelling				X
Game arcades		X		
Interior decorating showrooms	X			
Laundromats				X
Locksmiths	X			
Mailboxes, mailroom, postal stores	X			
Martial arts studios				X
Masseur-masseuse, massage establishments, acupressure spas, day spas and similar uses				X
Mortuary		X		
Mortuaries with total gross floor area of less than 5,000 sq. ft.			X	
Optician, optometrists, optical shops	X			
Outdoor vendor carts		X		
Pawn shops				X
Photography studios	X			
Picture frames – sales/service	X			
Recycling facilities, stand alone				X
Sporting goods	X			
Storage, wholesale or retail				X
Tattoo shops/body piercing				X
Tire stores, new or used				X
Tuxedo shops, sales and rental	X			
Upholstery shops				X

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
2.	Specialty Commercial				
	Alcohol				
	Cocktail lounge, bar, pubs, etc.		X		
	On-site, accessory to food uses			X	
	Off-site sale, liquor, beer & wine		X		
	On-site, accessory all other uses			X	
	On-site brewery or winery in conjunction with other permitted activity		X		
	On-site tasting rooms			X	
	Antique shops	X			
	Apparel and clothing stores	X			
	Appliance stores – large, retail only	X			
	Appliance stores – small, retail only	X			
	Art stores/artist supplies	X			
	Bakeries, retail only	X			
	Bicycle shops	X			
	Bookstores, new & used	X			
	Camera & photography, retail	X			
	Candy & confectioneries	X			
	Comic books – new & used	X			
	Computer games/accessories – sales	X			
	Convenience stores/mini mart	X			
	Costume shops, sales & rental	X			
	Drug & pharmacy	X			
	Dry goods	X			
	Electronics/computers – retail	X			
	Food uses – retail (non-drive thrus)				
	Coffeehouse/tea	X			
	Delicatessens	X			
	Food court/fast food	X			

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
	Grocery, fresh produce/meat	X			
	Health/organic/vitamins	X			
	Ice cream, yogurt, smoothies	X			
	Restaurants/café	X			
	Fabric/linens	X			
	Florists/floral	X			
	Furniture/home goods	X			
	Gift shops/stationery/greeting cards	X			
	Gun sales – accessory use	X			
	Hardware/home improvement				
	Less than 3,000 sq. ft. floor area	X			
	3,001 sq. ft. floor area & greater		X		
	Outdoor sales area		X		
	Hobby & crafts sales	X			
	Hotel & motels				X
	Internet/computer access				
	Internet café – primary activity		X		
	Internet – accessory, 3 or less	X			
	Internet – accessory, 4 or more		X		
	Internet access only – wireless	X			
	Internet gaming		X		
	Jewelry/gems/coins sales	X			
	Leather goods/luggage	X			
	Liquor stores (see Alcohol)				
	Medical supplies, retail only	X			
	Microbrewery–on-site (see Alcohol)				
	Music				
	Instrument sales – new or used	X			
	Instrument repair – accessory	X			
	Lessons – accessory	X			

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
	Lessons – primary		X		
	Music supplies	X			
	Recorded music – new or used	X			
	Recording studio		X		
	Sound equipment sales	X			
	Office supplies	X			
	Paint & wallcovering – retail	X			
	Party supply stores	X			
	Patio/bbq furniture/equipment	X			
	Shoe stores, including repair	X			
	Smoke/cigar shop		X		
	Electronic cigarette and/or electronic liquid shop		X		
	Surplus stores (army/navy)	X			
	Thrift store				X
	Toy store	X			
	Trophy shop	X			
	Watch & clock sales/repair	X			
3. Commercial Office/Administrative					
	Administrative/professional offices				
	Accountant, tax preparer	X			
	Administrative offices, general	X			
	Architecture/designer	X			
	Attorneys/legal services	X			
	Computer programming/software	X			
	Consulting offices	X			
	Engineer	X			
	Financial services	X			
	Insurance	X			
	Real estate	X			

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
	Travel agencies/ticket agencies	X			
	Medical offices				
	Chiropractic	X			
	Dental	X			
	General and specialty medical	X			
	Psychiatric/psychological	X			
B.	Residential				
	Live/work (no ground floor residential)	X			
	Lofts		X		
	Mixed use (no ground floor residential)	X			
	Multiple-family				
	Attached		X w/out sufficient points	X with sufficient points	
	Detached		X w/out sufficient points	X with sufficient points	
	Single-family				X
C.	Other Uses/Miscellaneous				
	Government Facilities	X			
	Public assembly uses, such as banquet halls, religious institutions, fraternal halls/clubs and similar public assembly uses	X			
	Public assembly uses with a total gross floor area of less than 5,000 sq. ft.			X	
	Parking lots/parking structures	X			
	Public open space/amenities	X			
	Public/private utility/communication		X		
	Private storage/mini storage				X
	Swap meet, indoor & outdoor				X

TABLE 4.16 BVOZ-N Allowable Uses

USE CATEGORY		PERMITTED USE	REQUIRES A CUP	REQUIRES AN ACUP	PROHIBITED USE
D.	Uses Not Listed				

In order to ensure that the Bellflower Village Overlay Zone—North Area permits all similar uses, if a use is not specifically listed as a permitted, permitted with a conditional use permit, or prohibited, then the Director must make a determination if the proposed use is similar to a use listed in the above matrix.

The determination of the Director must be based upon the evaluation of the characteristics of the proposed use with the goals, objectives and policies of the General Plan, as well as with the goals and objectives of the Bellflower Village Overlay Zone. The Director must make a written report of his/her findings to the Planning Commission.

The determination of the Director may be appealed pursuant to the provisions of Chapter 17.112 of the Bellflower Municipal Code.

4.10.5 DEVELOPMENT STANDARDS

The Bellflower Village Overlay Zone—North contains special development standards that pertain specifically to the properties that are within the boundaries of the Bellflower Village Overlay Zone—North. Those special development standards are allowed to deviate from the City’s conventional zoning standards in some categories. Where noted, the City’s conventional zoning standards apply and remain in full force.

The development standards set forth for residential, commercial, office and mixed-use development are intended to provide for flexibility in site design, as well as methods to integrate commercial and residential uses within boundaries of the Bellflower Village Overlay Zone—North. Those development standards are the minimum standards for development. The Bellflower Village Overlay Zone—North allows for a mixture of commercial, office and residential (attached or “live/work”).

The architecture and site planning for any future development will be reviewed and approved pursuant to the following development standards.

TABLE 4.17 BVOZ-N Development Standards

DEVELOPMENT CRITERIA		DEVELOPMENT STANDARD	SPECIAL PROVISIONS AND COMMENTS
A.	Lot Area Standards.		
	The minimum lot area required to develop on any lot or parcel	5,000 square feet	
B.	Required Yards Standards.		
	Setback from the public right-of-way or street	None	
	Setback from an interior property line	None	
C.	Building Heights/Allowable Stories.		
	The maximum building height for any building or structure	4 stories or 55 feet	Nonhabitable, projections exceeding 55 feet, if architecturally integrated into the building design
	Structures greater than 4 stories or 55 feet, but in no case more than 5 stories may be allowed with a CUP	5 stories	Pursuant to the approval of a CUP
D.	Relationship to Public/Private Streets and Alleys.		
	Relationship to public streets		All building facades and elevations must have architectural treatments incorporating a minimum of at least 3 different building materials

TABLE 4.17 BVOZ-N Development Standards

DEVELOPMENT CRITERIA		DEVELOPMENT STANDARD	SPECIAL PROVISIONS AND COMMENTS
	Relationship to private streets and alleys		Building facades and elevations facing private streets and alleys must have full architectural treatment, with an emphasis on pedestrian scale and access
	Encroachment on or into the public right-of-way		Architectural, building features, signage and similar features may project on or into the public right-of-way, pursuant to the approval of an Encroachment Permit and a right-of-way legal instrument. Minimum horizontal and vertical clearances must be maintained as set forth by the Building Official and/or City Engineer
E.	Required Parking.		
	Commercial uses	The provisions of BMC Chapter 17.48 and/or 17.88 apply, as applicable	
	Alternative off-street parking requirement		Required off-street parking may be transferred to designated off-site parking locations pursuant to an agreement approved by the City Council
	Residential uses	Minimum of 2 covered spaces per unit	Each residential unit must have a minimum of 2 covered or enclosed, on-site spaces per unit
	Guest parking	The provisions of BMC Chapter 17.48 and/or 17.88 apply, as applicable	Required guest parking may be transferred to designated off-site parking locations pursuant to an agreement approved by the City Council
F.	Fences, Walls, Landscaping Standards.	The provisions of BMC Chapter 17.48 apply	Special provisions: Refer to BMC Chapter 17.48

TABLE 4.17 BVOZ-N Development Standards

DEVELOPMENT CRITERIA		DEVELOPMENT STANDARD	SPECIAL PROVISIONS AND COMMENTS
G.	Lighting Standards.	The provisions of BMC Chapter 17.48 apply	
H.	Mechanical Equipment/Trash Service.	The provisions of BMC Chapter 17.48 apply	
I.	Sign Standards.		
	Design, height, materials, illumination, maintenance, etc., of signs	Determined by the Master Sign Plan in accordance with BMC Chapter 17.68 Chapter	
	Required Master Sign Plan		The provisions of the BMC Chapter 17.68 apply. The Project review Committee must approve any Master Sign Plan.

4.10.6 ADMINISTRATIVE CONDITIONAL USE PERMIT (ACUP) IMPLEMENTATION

- A. Implementation. All development projects requiring or accumulating sufficient points to allow an administrative conditional use permit requires review and approval by the Director.
- B. Director's Review. The Director must review the proposal in accordance with Chapter 7.
- C. Required Findings. The Director can approve the application, plans, provided the following findings are made:
 - 1. The use/development is consistent with the provisions of this Overlay Zone.
 - 2. The use/development is unique and different from any other use within the Overlay Zone.
 - 3. The use/development is complimentary and compatible with other projects within this Overlay Zone.
 - 4. The overall project is of high quality and innovative in order to create a "signature" use/development for the City of Bellflower.
- D. Appeal Provisions. The determination of the Director may be appealed to the Planning Commission, pursuant to the provisions of the BMC.

4.10.7 EXHIBITS (MAP AND ILLUSTRATIONS)

FIGURE 4.21 BVOZ-N Vicinity Map

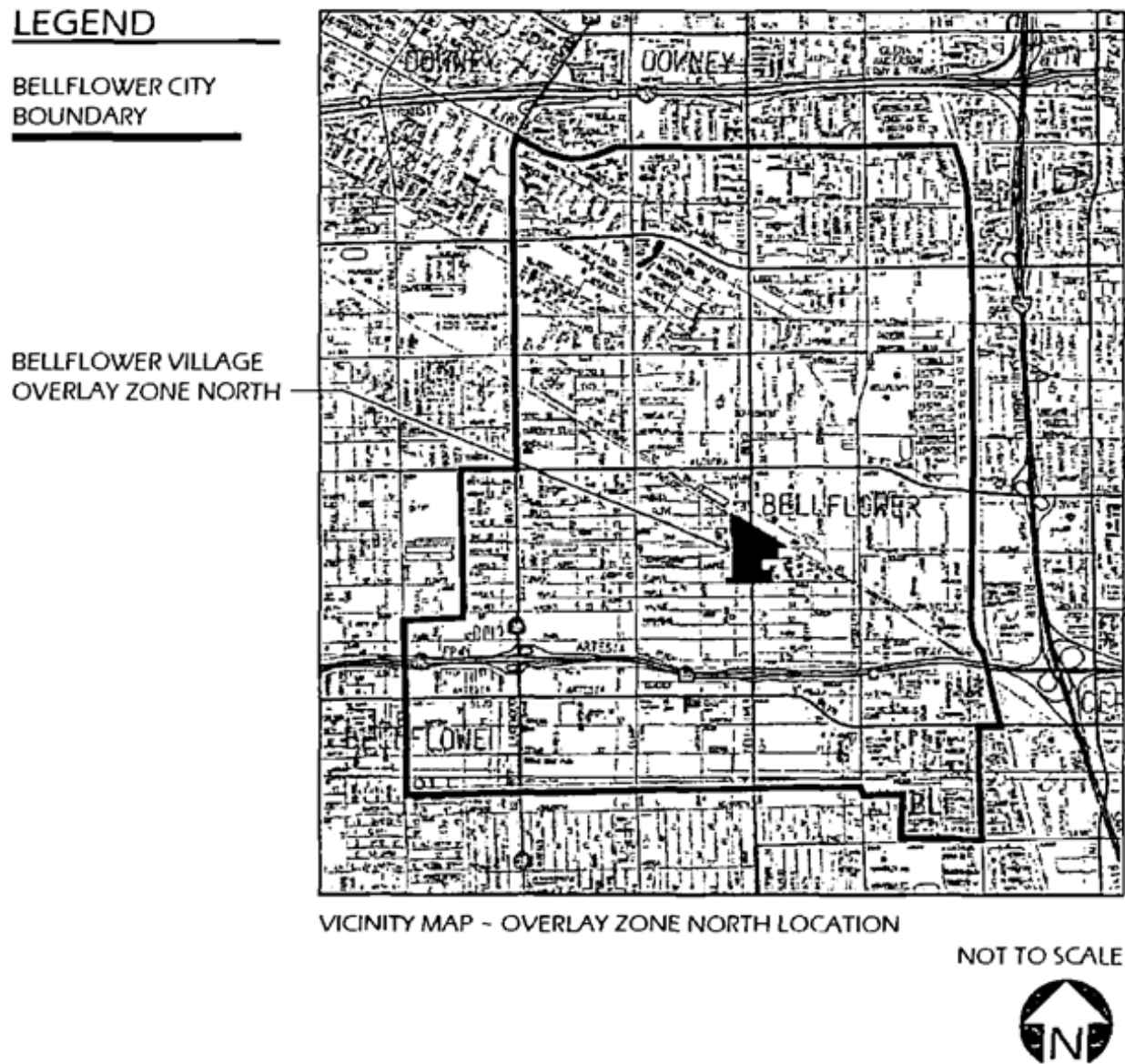


FIGURE 4.22 BVOZ-N Overlay Zone Boundaries

LEGEND

OVERLAY ZONE
NORTH BOUNDARY

AREA BOUNDARIES

FUTURE PARKING

MIXED USE

- Residential
- Commercial
- Retail
- Office
- Service



OVERLAY ZONE NORTH AND CORRESPONDING AREAS

NOT TO SCALE



FIGURE 4.23 BVOZ-N Area No. 1

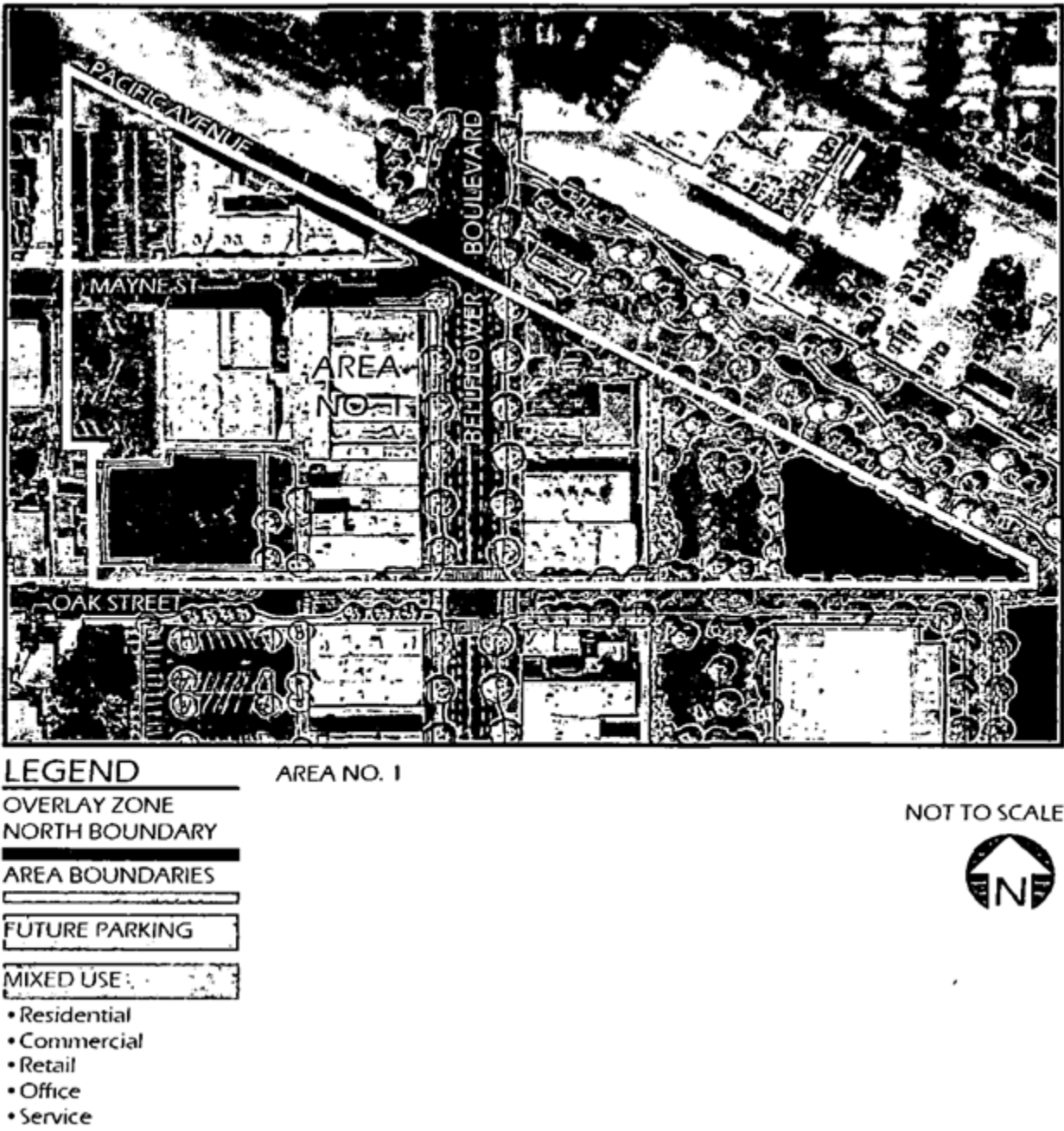


FIGURE 4.24 BVOZ-N Area No. 2

LEGEND

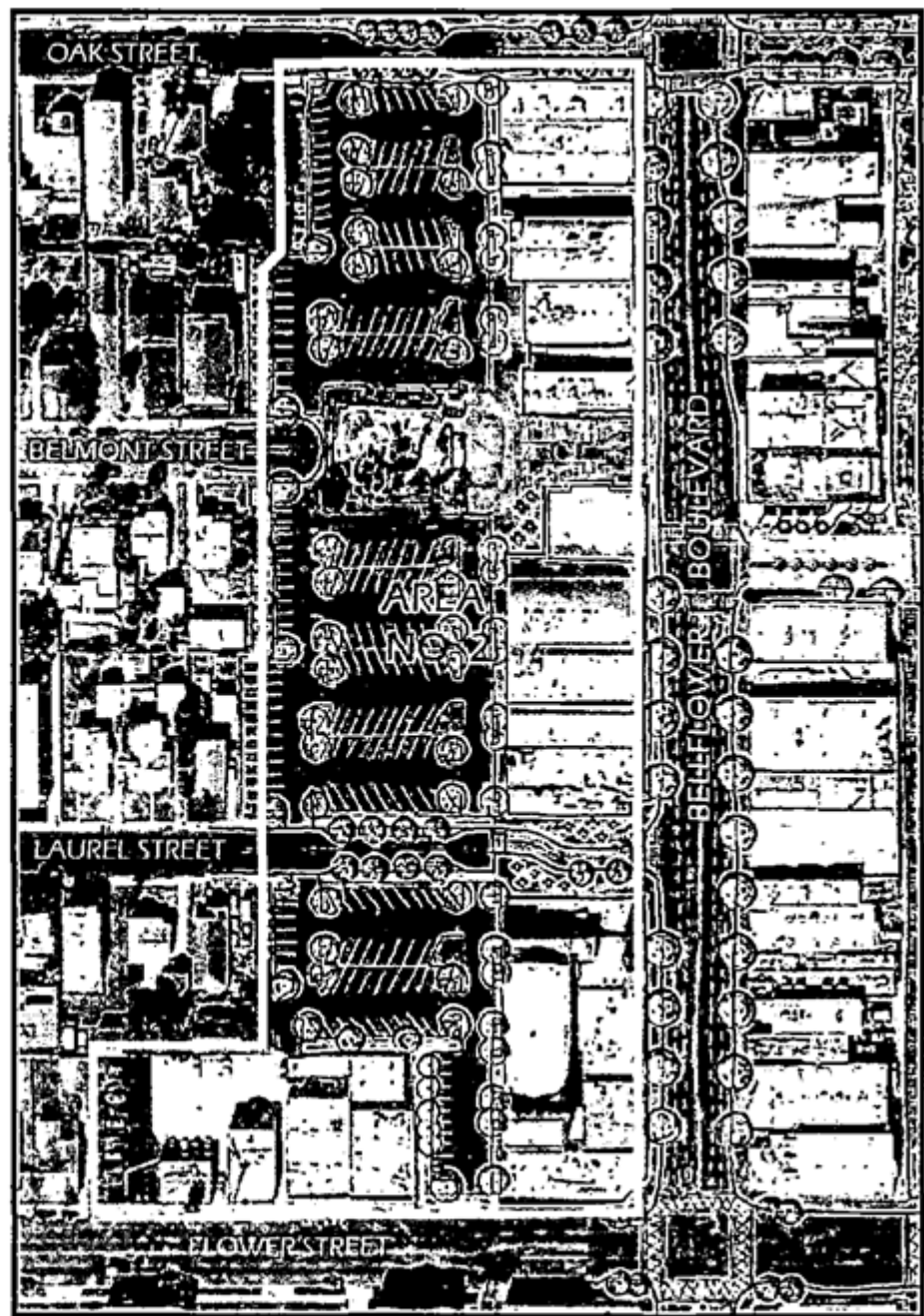
OVERLAY ZONE
NORTH BOUNDARY

AREA BOUNDARIES

FUTURE PARKING

MIXED USE

- Residential
- Commercial
- Retail
- Office
- Service



AREA NO. 2

NOT TO SCALE



FIGURE 4.25 **BVOZ-N Area No. 3**

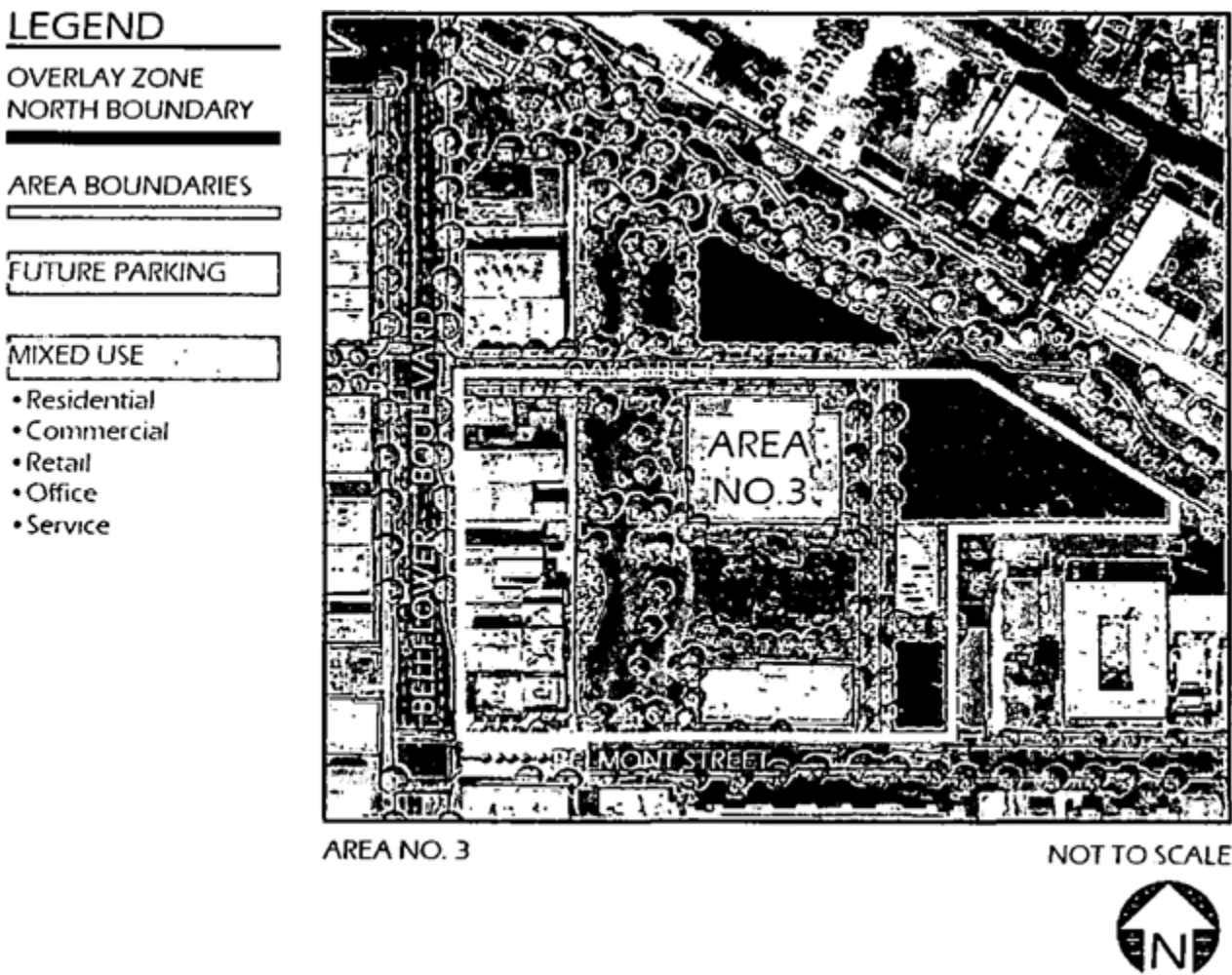
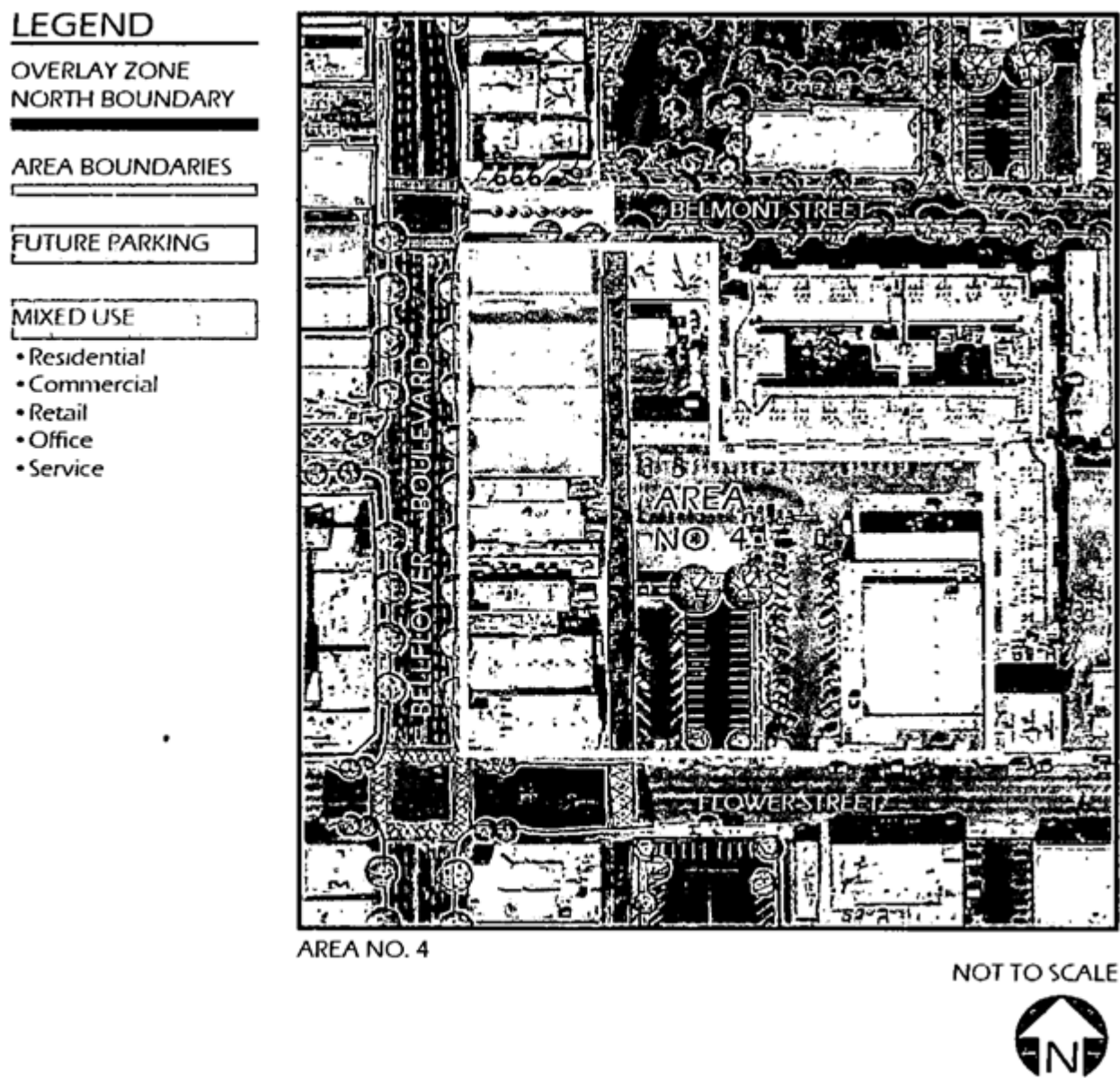
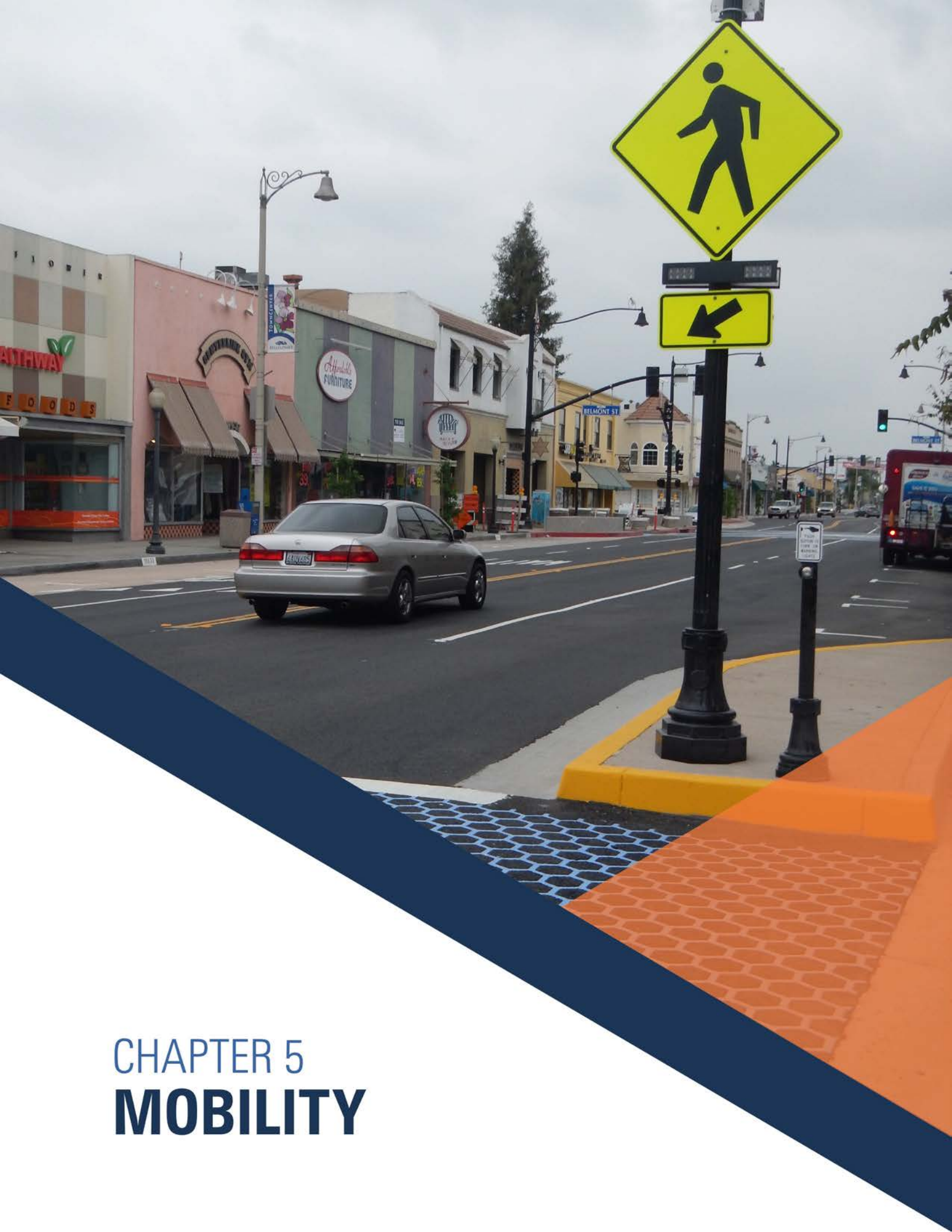


FIGURE 4.26 **BVOZ-N Area No. 4**







CHAPTER 5

MOBILITY

5. MOBILITY

This chapter describes the circulation and other improvements to public streets and open space areas needed to support transit-oriented development (TOD) within the Specific Plan area. A key component of the Specific Plan is targeted improvements to the current circulation network in order to transform it into a network that places a higher priority on the principles of transit accessibility, complete streets, and multi-modal design. The construction of a future light rail station along Bellflower Boulevard requires an assessment of pedestrian, bicycle, bus, and vehicle accessibility to and from the station area. The strategies set forth in this document are intended to provide a framework that supports both motorized and non-motorized modes of transportation together in an integrated system.

5.1 OBJECTIVES AND POLICIES

The following objectives and policies set the framework for the mobility and public realm strategy of Bellflower. They serve as guidelines and provide direction for future decision-making and development activities. The objectives and policies identified in this section were derived from input received from community members, stakeholders, and staff during the community engagement process and serve as tools to help the City reach their goals as outlined in the vision. The following lists the major mobility objectives and policies for the Downtown Bellflower Station Area Specific Plan:

- **OBJECTIVE 1:** Provide and maintain a comprehensive circulation system that improves accessibility to transit, and the safe and efficient movement of all users of the roadway.
 - » **Policy 1.1:** Implement complete streets designs that contribute to a multi-modal transportation system.
 - » **Policy 1.2:** Ensure that roadway improvements allow for easier, safer, and more efficient transit operations, as well as improved passenger safety and accessibility.
 - » **Policy 1.3** Consult with local transit operators to provide attractive and convenient bus stops, including shade/weather protection, seats, transit information, and bus shelters where appropriate.

- » **Policy 1.4:** Consider the interactions between bus and bicyclists and design bus stops that will help minimize conflicts.
- » **Policy 1.5:** Maintain existing on street parking when feasible.
- **OBJECTIVE 2:** Provide safe, well connected, and accessible bikeway and pedestrian network.
 - » **Policy 2.1:** Establish a connected pedestrian and bicycle network that links the Downtown Bellflower Transit Station, the Regional Bus Station, the local bus station, residential neighborhoods, local schools, and retail corridors.
 - » **Policy 2.2:** Complete bicycle infrastructure improvements that respond to the recommendations from the Bellflower-Paramount Bike & Trail Master Plan. Any recommended improvements require City approval prior to consideration.
 - » **Policy 2.3:** Establish and maintain attractive and functional sidewalks that maximize accessibility, enhance the pedestrian environment, and foster social interaction.
 - » **Policy 2.4:** Incorporate pedestrian amenities into the area around the future Bellflower Boulevard Transit station such as kiosks, benches, tables, and landscaping.
 - » **Policy 2.5:** Design bicycle and pedestrian infrastructure in accordance with federal, state, and local design standards, including ADA accessibility standards.
- **OBJECTIVE 3:** Provide and maintain attractive mobility corridors that promote livability and sustainability.
 - » **Policy 3.1:** Extend the streetscape improvements on Bellflower Boulevard (between Flower and Mayne Streets).
 - » **Policy 3.2** Implement streetscape features such as, street trees, landscaping, and wayfinding signage to create safer and attractive corridors particularly along Alondra Boulevard and Flower Street. Real-time signage and smart wayfinding must be promoted when feasible.
 - » **Policy 3.3** Integrate pedestrian amenities, such as benches and public art to transform the streetscape and create public space.

- » **Policy 3.4:** Provide gateway signage at Bellflower Boulevard and Alondra Boulevard to direct people to the transit station and downtown area.
- **OBJECTIVE 4:** Promote efficient use of parking resources.
 - » **Policy 4.1:** Establish guidelines and standards to optimize parking supply that is consistent with related City requirements.
 - » **Policy 4.2:** Encourage and allow shared parking for new development in lieu of the provision of off-street parking spaces in a manner consistent with related City requirements.
 - » **Policy 4.3:** Maintain on-street parking where feasible. Careful consideration will be given before removing on-street parking.
- **OBJECTIVE 5:** Integrate the future transit station into the urban fabric and multi-modal transportation networks.
 - » **Policy 5.1:** Coordinate with Metro on the location and construction of the future station to ensure that access and design are cohesive with the City-owned parking structure adjacent to the tracks.
 - » **Policy 5.2:** Coordinate with Metro on a joint development project at the future transit station site to enable residential, retail, and service commercial uses.
 - » **Policy 5.3:** Coordinate with Metro on the creation of a public outdoor area that serves transit users. Support the creation of space for a variety of amenities.
 - » **Policy 5.4:** Coordinate with local transit operators to provide a well-designed interchange between bus service and the future Downtown Bellflower Transit Station.
 - » **Policy 5.5:** Coordinate with Metro to incorporate existing and future pedestrian and bicycle connections to the future transit station. Enhance access with new and upgraded crosswalks.

5.2 FIRST-LAST MILE STRATEGY

In 2014, the Los Angeles Metropolitan Transportation Authority (Metro) approved its First Last Mile Strategic Plan, which identifies design strategies to improve active transportation access and connections to public transit. The Downtown Bellflower Station Area Specific Plan recognizes that station access is a key element in successful TOD station area planning

and has identified strategies that focus on improving accessibility during the first and last miles of a transit rider's journey. These strategies include streetscape improvements, bicycle and pedestrian infrastructure improvements, as well as signage and wayfinding improvements.

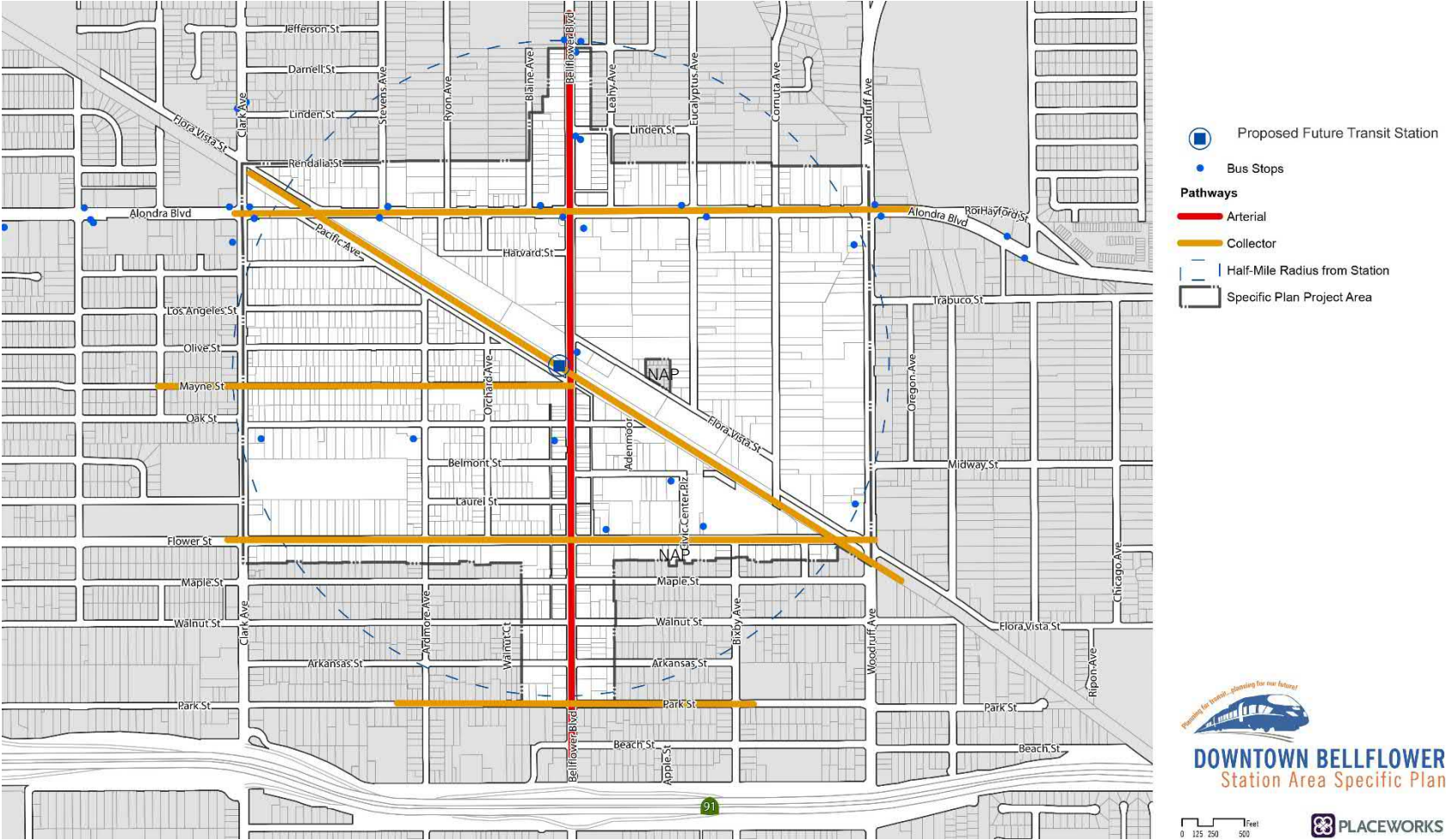
5.2.1 PATHWAYS

The Metro Pathways concept established in Metro's First and Last Mile Strategic Plan includes a hierarchy of pathways that extend to and from a transit station and is designed to support active modes of transportation. These pathways take into consideration the existing street network, key destinations, feeder transit services, existing and planned infrastructure, existing bike and pedestrian volumes, and surrounding land uses in order to design an active transportation network that improves transit station access. The recommended pathway system (Figure 5.1 Pathways) may not correlate with the City's traditional street classification shown in Figure 5.2 Street Network. The Metro Pathways concept is comprised of two types of pathways, pathway arterials and pathway collectors. Pathway arterials serve as the main branches of the network, while pathway collectors serve as feeder routes. How the pathways function around the Bellflower Station may be different than their official street designation as either arterial or collector. For example, while Alondra Boulevard may be a major arterial as defined by Bellflower's circulation element, it functions as a collector in relation to the station. Figure 5.1 illustrates the proposed Metro Pathway network surrounding the Downtown Bellflower Transit Station and includes pathway arterials, pathway collectors, existing bus stops, and key destinations.

Pathway Arterials

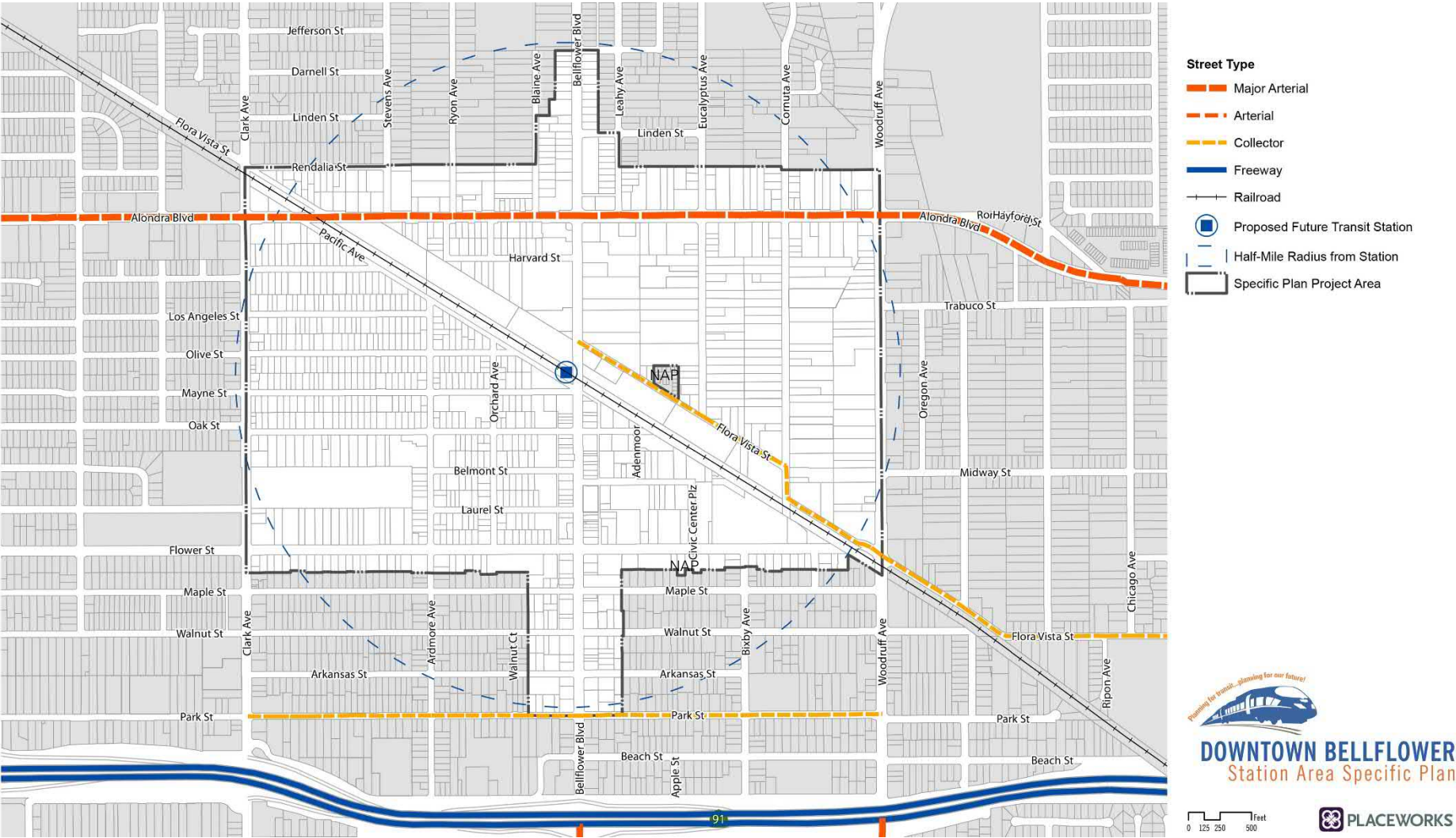
Pathway arterials are primary routes that extend from stations and support maximum activity for active transportation users. They are designed to accommodate high levels of active transportation and a broad range of users. They typically include design treatments such as signal and crossing improvements, wayfinding signage, and plug-in components (e.g., bike sharing). Potential design treatments are dependent on existing conditions and City priorities. Implementation will require consideration by the City of Bellflower's Public Works Department.

FIGURE 5.1 Pathways



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FIGURE 5.2 Street Network



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Pathway arterials should typically radiate out from a station portal in at least four directions and extend out at a minimum of one-half mile from the station to an upper limit of three miles from the station. Pathway arterials should also integrate the regional bikeway network at opportune points beyond the one-half mile access shed. As illustrated in Figure 5.1, the pathway arterials connecting the Downtown Bellflower Transit Station to the community include Bellflower Boulevard and Mayne Street. Because of the station's limited access points, ingress and egress to the site will be especially important.

Pathway Collectors

Pathway collectors are routes within the station area that feed into pathway arterials, support crossing movements, and enhance general station area permeability. They work to reduce travel distances for non-motorized users by focusing on the provision of efficient access to pathway arterials. Pathway collectors include streets and routes located within the one-half mile access shed of a transit station and streets that feed into the main branch lines or pathway arterials. They typically include design considerations that improve intersections and crossings. As illustrated in Figure 5.1, pathway collectors surrounding the Downtown Bellflower Transit Station includes Alondra Boulevard to the north, Mayne Street to the west, Flower Street, Park Street, and the bike trail to the south.

5.3 STREET NETWORK

The Downtown Bellflower Station Area Specific Plan provides guidance for the design of a comprehensive and context-sensitive street network to connect the Bellflower community. The Specific Plan Area is characterized by a grid network, with a break in the pattern occurring where the railroad right-of-way crosses at a diagonal from north-west to south-east. Much of the street network within the Specific Plan area will remain the same in order to support new development and growth within the area. Some of the streetscape improvements implemented along Bellflower Boulevard will be extended from Mayne Street to Alondra Boulevard. There are additional proposed improvements along key arterials within the Specific Plan area. These improvements are intended to transform the existing auto-oriented streetscape into a more sustainable, multi-modal design. The Specific Plan's roadway and circulation network plans are described below and can be seen in Figure 5.2. The Specific Plan focuses on Bellflower Boulevard, Alondra

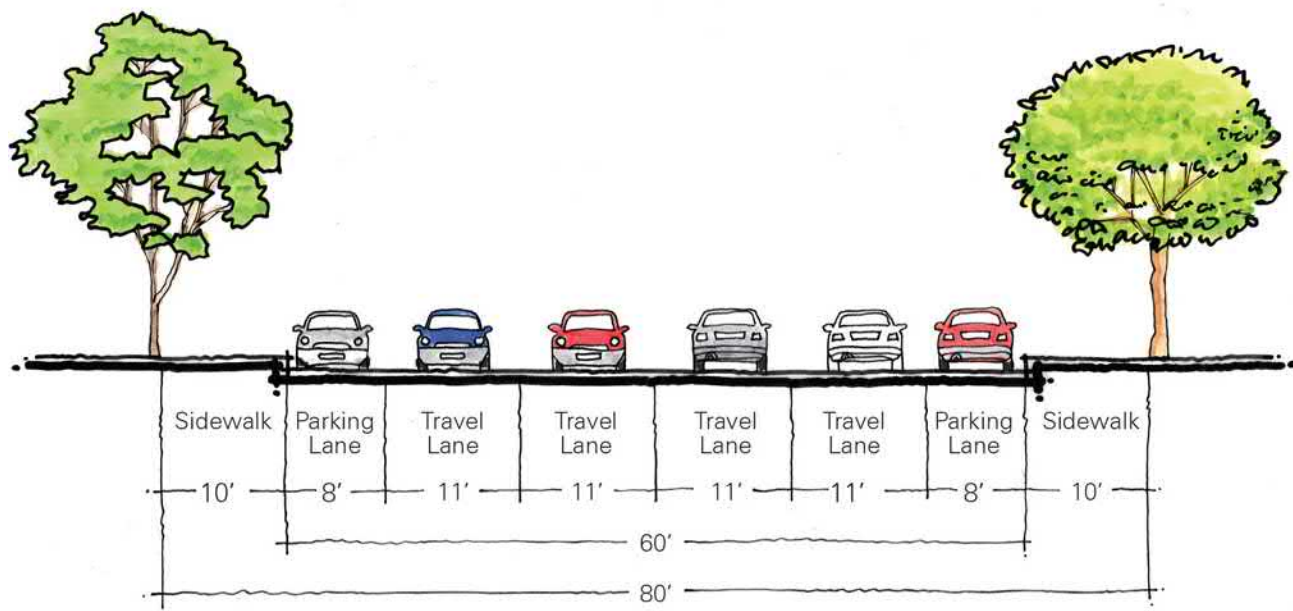
Boulevard, Flower Street, and the bike trail because of their importance in relation to the Bellflower Station.

5.3.1 BELLFLOWER BOULEVARD

Existing Conditions

Bellflower Boulevard is classified as a secondary arterial roadway within the specific plan area by the General Plan Circulation Element as it distributes traffic to smaller geographic areas, connects drivers to higher volume arterials such as Alondra Boulevard, and accommodates trips of moderate length. However, in relation to the Bellflower Station it acts as an arterial and serves as the primary access route to and from the Bellflower Station and for people wishing to visit Downtown Bellflower. The existing boulevard is illustrated in Figure 5.3.

FIGURE 5.3 Bellflower Boulevard (Existing)



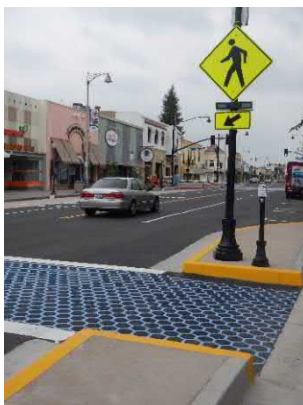
Bellflower Boulevard connects to Alondra Boulevard to the north and SR-91 to the south. It is a north-south, four-lane undivided roadway with a painted double yellow centerline, within the Specific Plan boundary. Bellflower Boulevard is well-traveled and has two lanes of travel in both directions, on-street parking, with left turn lanes at Alondra Boulevard, and no bicycle lanes. Within the Specific Plan area, Bellflower Boulevard has eight traffic signals, two pedestrian-dedicated flashing sign crossings, and one scramble intersection, meaning that all traffic signals turn

red at the same time and pedestrians can move freely through the intersection (including diagonally) during the crossing period. Two bus routes travel along Bellflower Boulevard (see the Transit Network section for more information). Bellflower Boulevard is a completed Los Angeles County Traffic Signal Synchronization Program route.

Proposed Improvements

Improvements to Bellflower Boulevard are planned within the existing right-of-way. They include extending the street improvements between Mayne Street and Alondra Boulevard, as well as potential turn-outs for bus and rideshare services associated with the transit station.

Since the Downtown Revitalization Vision Strategy was prepared in 2003, the City has been working on land use and design strategies to help downtown Bellflower develop its own unique sense of place. As part of these efforts significant improvements has been made along Bellflower Boulevard between Flower and the train tracks. Including new decorative traffic signal equipment, widening roads to provide exclusive turn lanes, new sidewalks and curb ramps, and decorative crosswalks. The City's vision is that these improvements would extend along Bellflower Boulevard north to Alondra Boulevard and south to the SR-91.



Extending the improvements that have already been started along Bellflower Boulevard will help create a cohesive corridor. Wider sidewalks with pedestrian amenities, such as street trees, landscaping, and lighting should be considered to enhance safety and the overall pedestrian environment.

5.3.2 ALONDRA BOULEVARD

Existing Conditions

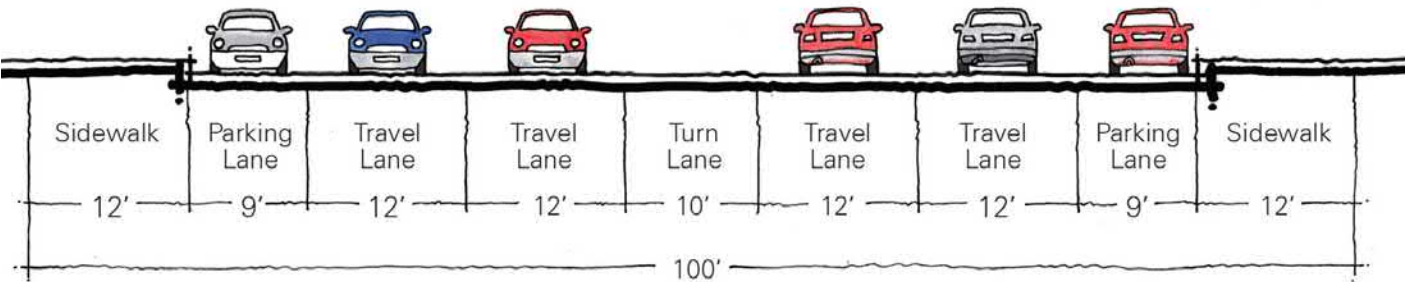
Alondra Boulevard is a key east-west, auto-centric corridor with wide travel lanes, minimal street trees, and no bicycle lanes. It is classified as a major arterial roadway in the General Plan. It provides a high degree of mobility and access to the surrounding community. It is a main corridor for local traffic while being used as a thoroughfare for through traffic. It acts as a collector in relation to the future Bellflower Station.

Alondra Boulevard provides connectivity from the Specific Plan study area to I-605 to the east and I-710 to the west. This is a heavily traveled roadway with two lanes of travel in each direction, one turning lane and on-street parking.

Alondra Boulevard, in the project vicinity, is an east-west, four-lane divided roadway with a continuous left-turn lane. The posted speed limit on Alondra Boulevard is 40 miles per hour.

Within the Specific Plan area, there are seven traffic lights, complete with crosswalks to promote pedestrian safety. Alondra Boulevard also serves as a relatively busy corridor for public transportation and provides bus stops for four different bus routes within the Specific Plan area. Alondra Boulevard is a completed Los Angeles County Traffic Signal Synchronization Program route.

FIGURE 5.4 Alondra Boulevard (Existing)





Proposed Improvements

Improvements to Alondra Boulevard would take place within the existing right-of-way. The streetscape improvements to Alondra Boulevard should build upon those already made on Bellflower Boulevard with a focus on improvements to the pedestrian realm through added street trees, curb extensions, and bus shelters. Adding additional street trees and working within the existing right-of-way to add more pedestrian amenities and landscaping will help give Alondra Boulevard and the adjacent businesses a stronger identity and sense of place.

Proposed improvements along Alondra Boulevard include:

- » Curb extensions at Alondra Boulevard at Eucalyptus Avenue.
- » Traffic signal replacement associated with curb extensions.
- » New bus shelters

See Figure 5.10 for recommended street trees and Section 5.4 for bikeway recommendations.

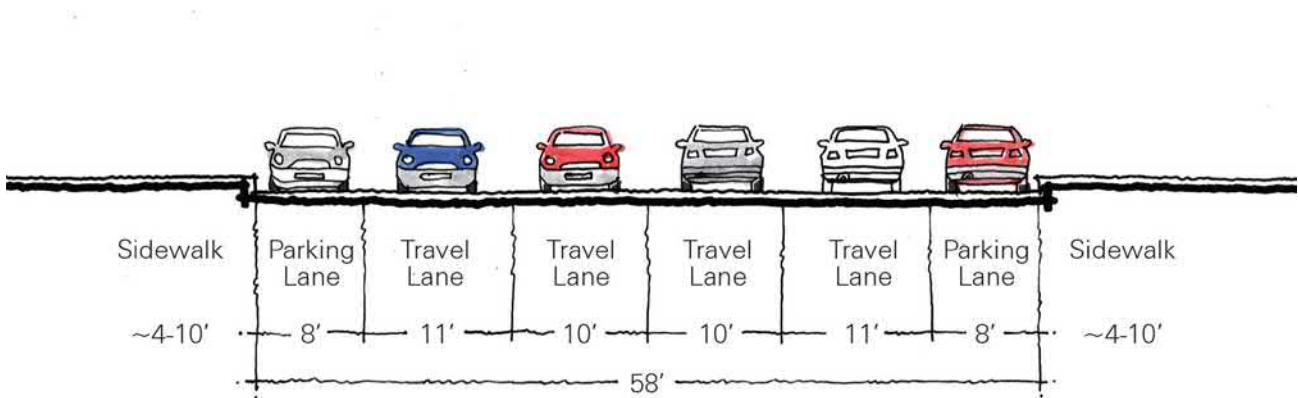
5.3.3 FLOWER STREET

Existing Conditions

Flower Street, in the project vicinity, is an east-west, four-lane undivided roadway with a painted double yellow centerline. On-street parking is permitted on Flower Street.

Flower Street is classified as a Secondary Arterial roadway and serves to funnel traffic from local roads to the arterial network, particularly on the south side of the specific plan area. Flower Street intersects with Bellflower Boulevard, Woodruff Avenue, and Clark Avenue, and provides connections to the surrounding freeways and interstates. Although not as heavily traveled as Bellflower Boulevard and Alondra Boulevard, Flower Street has two travel lanes in both directions, on-street parking, and serves only one bus route.

FIGURE 5.5 Flower Street (Existing)



Proposed Section

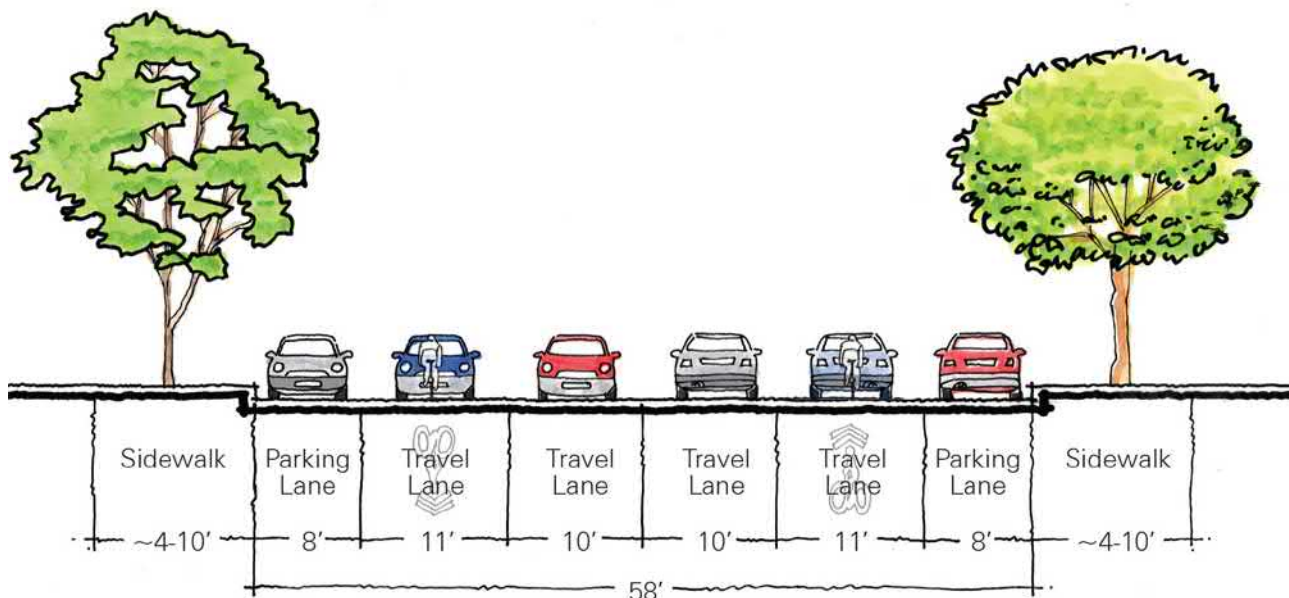
Flower Street was recently re-paved and all intersections brought into ADA compliance. As such, changes along Flower Street are proposed to be more minimal but may include the addition of more street trees and the eventual undergrounding of overhead utilities.

As a secondary arterial, Flower Street is envisioned as part of the expanding bicycle network within Bellflower. The proposed street sections for Flower Street is in response to the Bellflower-Paramount Bike & Trail Master Plan. Although the Bicycle Master Plan proposes a Class II Bicycle Lane, this would require the removal of needed on-street parking or travel lanes. Therefore, it is proposed that Flower Street be designated a Class III Bicycle Route with shared lane markings along Flower Street from the Pacific Electric Rail Road right-of-way to the west City Boundary.

The proposed alternative, seen in Figure 5.6, would make Flower Street a designated bicycle route. Bicycles would share the existing road with vehicles, so no lane width alterations are needed. The shared-use lane is proposed along the length of Flower Street along the outside travel lane. This improvement would allow cyclists and commuters to connect into the multi-use trail that runs diagonally through Bellflower.

See Figure 5.10 for recommended street trees and Section 5.4 for bikeway recommendations.

FIGURE 5.6 Flower Street (Proposed)



5.4 BICYCLE NETWORK

Bicycling provides a sustainable solution for traveling the first and last mile to and from a transit station. While transit and bicycling are complementary modes, issues of infrastructure connectivity often impact a person's decision to bike the first and last mile of their journey from transit. The following section describes improvements to the bicycle network to promote bicycling within the Specific Plan area.

This section describes the existing conditions of the bicycle network within the Specific Plan area including its relation to the Bellflower-Paramount Bike & Trail Master Plan.

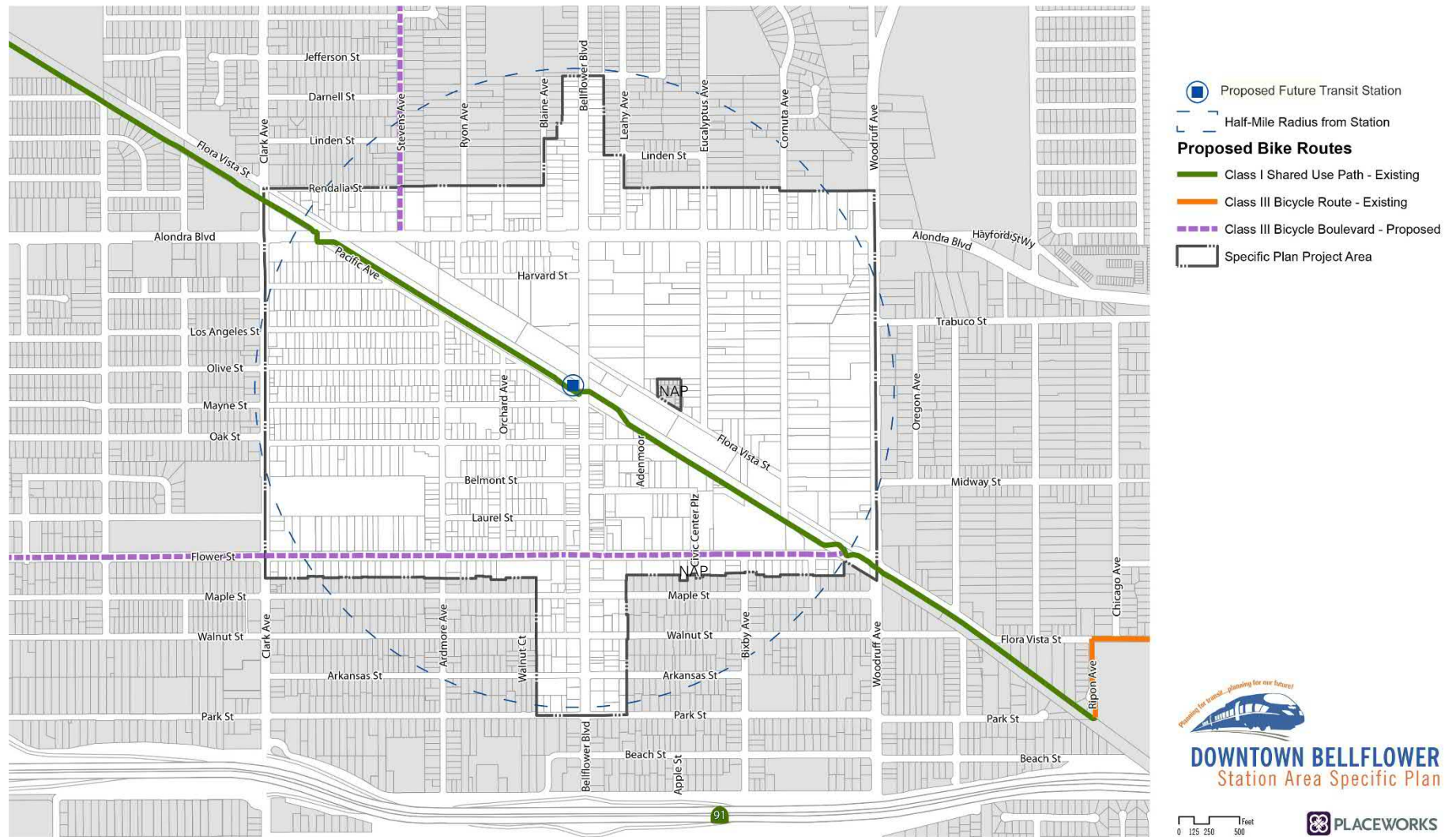
5.4.1 BICYCLE NETWORK

Figure 5.7 illustrates the level of bicycle accessibility within a half-mile radius of the Downtown Bellflower transit station. Using the station as a starting point, all possible bicycle routes were mapped based on the street grid and then consolidated into a larger catchment area or bike shed. The proposed metro line extension will be constructed on a right-of-way that is currently owned by the Pacific Electric Rail Road and partially occupied by a walking and bicycling trail (described in Section 5.4.3). This means that the existing path will need to be realigned in order to coexist with the rail line.

The existing bicycle network within the Specific Plan area consists of approximately 2.5 miles of existing bikeway infrastructure. The Specific Plan proposes to add another 2.5 miles of bikeways to the existing network as illustrated in Figure 5.7. Flower Street was selected based on the opportunities it presented to improve connectivity to the future transit station and to the regional bikeway network of neighboring cities.

Additional opportunities to expand Bellflower's bicycle network should make sure to tie into the existing network and should maintain compatibility with the Bellflower-Paramount Bike & Trail Master Plan.

FIGURE 5.7 Bicycle Network



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5.4.2 BICYCLE FACILITY TYPES

Bikeways are facilities that are designated primarily for bicycle travel. They are generally divided into the following five classes per the Bellflower-Paramount Bike & Trail Master Plan:

- » **Class I (Bike Path):** Provides an exclusive right-of-way completely separated from the street used by motor vehicles. Designated for the exclusive use of bicycles and pedestrians with crossflow traffic minimized.
- » **Class II (Bike Lane):** Provides a restricted on-street right-of-way. Designated for the exclusive or semi-exclusive use of bicycles with through travel by motor vehicles or pedestrians prohibited, but with crossflows by pedestrians and motorists permitted. Vehicle parking can be allowed to the right of a bike lane if sufficient right-of-way width exists.
- » **Class III (Bike Route):** Provides for shared use with motor vehicles and is on-street. Designated by signs or permanent markings.
- » **Class III (Bike Boulevard):** Provides for shared use with motor vehicles and is on-street. Designated by signs or permanent markings. It is further enhanced with traffic calming measures, bicycle wayfinding signage, and crossing improvements.
- » **Class IV (Separated Bikeway or Cycle Track):** Provides bikeways located in or adjacent to a roadway and separated from traffic by a physical barrier such as bollards, on-street parking, or planters. This design allows an exclusive right-of-way for bicycle travel.

5.4.3 WALKING AND BIKE TRAIL

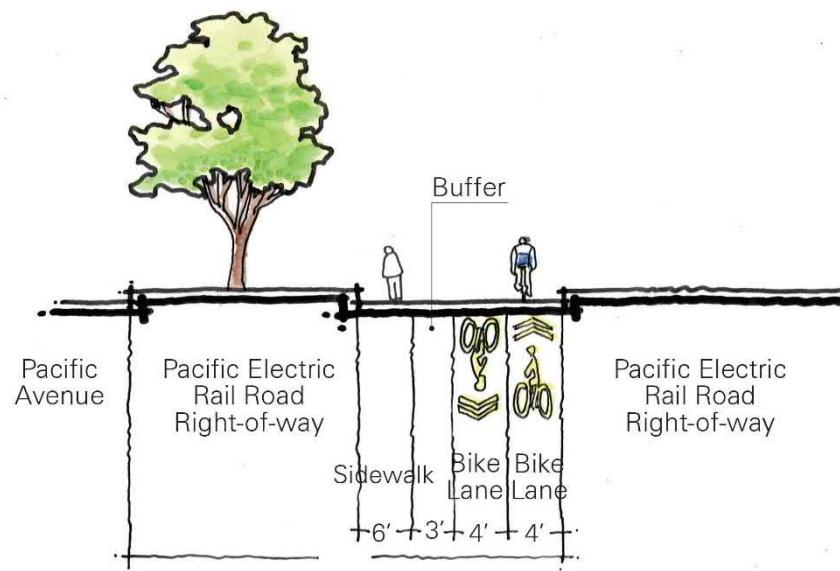
Existing Conditions

The existing bike trail is a regional amenity and the backbone of Bellflower's bicycle network. It runs diagonally through the city along the railroad right-of-way from the San Gabriel River and the City of Cerritos to the east to the City of Paramount to the west. Because the railroad tracks form a natural barrier dividing the city, this trail is crucial to helping cyclists navigate the City. The current bike path includes a 4' bike lane in either direction with a 2' buffer and 6' pedestrian sidewalk.

Vision

The existing bike trail along the Pacific Electric Rail Road right-of-way is crucial to Bellflower's vision to plan for high-quality transit and a transit-supportive environment and the success of the bicycle network. The bicycle network is designed to intersect with the bike trail and connects in at several locations including the intersection of Flower Street and Mayne Street. If this bike trail were to be removed, it would turn the existing network into a series of bicycle segments—with large gaps between existing segments and no way to bridge the gap. Therefore, it is recommended that when the metro line is extended that it includes sufficient right-of-way that multi-use trails can be maintained for the length of the existing bike trail. An example of this approach is the new Santa Monica Extension, which includes a parallel multi-use trail for much of the route.

FIGURE 5.8 Walking and Bike Trail (Existing)



Additional street trees should be added along this pathway to enhance areas with more shading. Street trees will keep the path shaded and make it more welcoming and comfortable.

5.5 PEDESTRIAN ENVIRONMENT

The quality of the pedestrian environment often impacts a person's decision to walk or drive to an area. Adequate pedestrian infrastructure, together with appropriate land use designations, can help to activate corridors and promote pedestrian activity. This section discusses some of the existing opportunities to improve the pedestrian environment within Bellflower and to reinforce the area as a TOD district.

Sidewalks present inventive opportunities to transform streets into public spaces. In order to create a vibrant walking environment that encourages pedestrian activity, sidewalks need to be safe, connected, comfortable, accessible, and attractive. The sidewalk improvements made along Bellflower Boulevard are an example of a well-designed pedestrian environment.

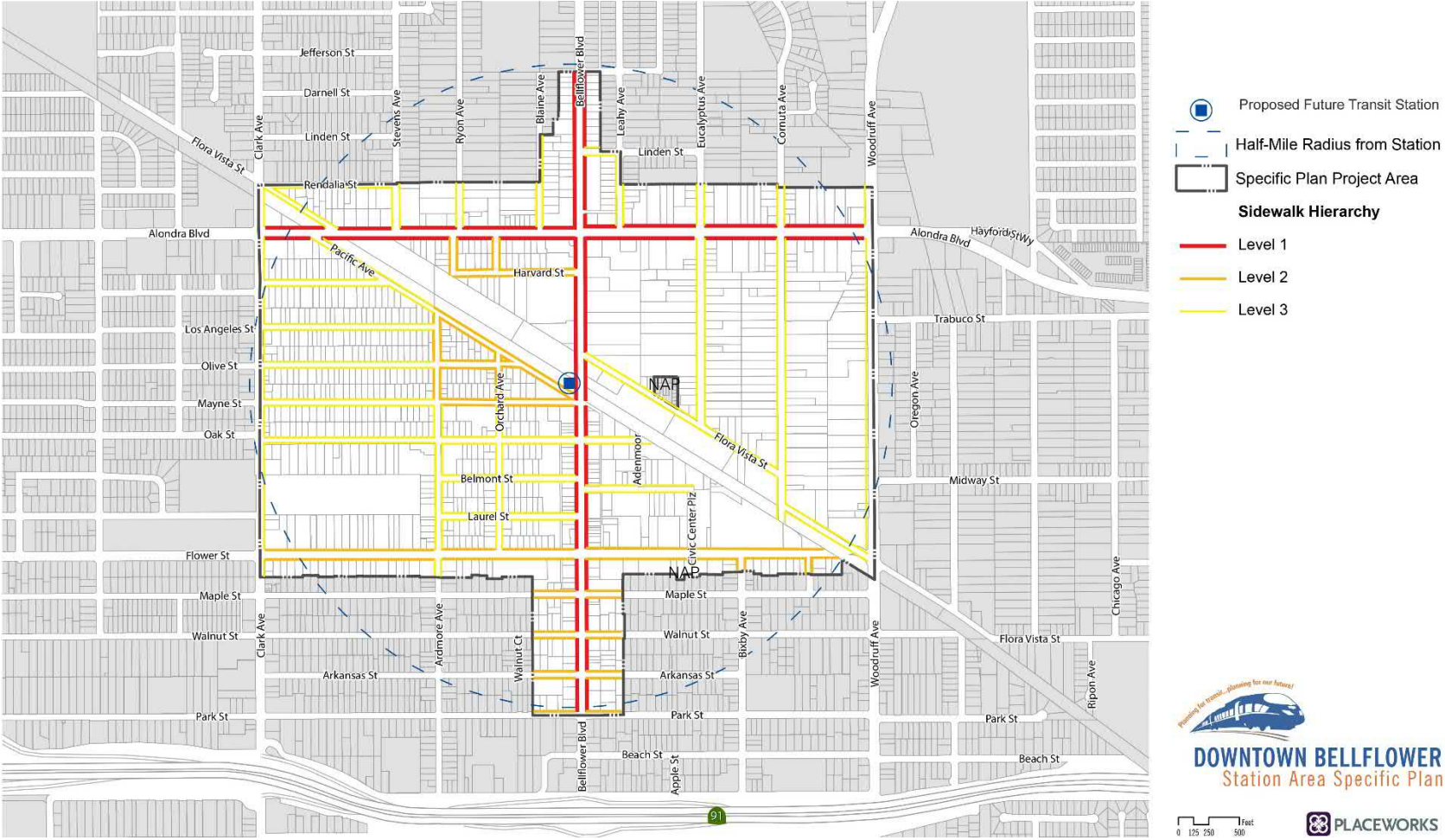
Although sidewalks exist along major streets within the Specific Plan area, most sidewalks lack landscaping and shade and do not support high levels of pedestrian activity. The Specific Plan proposes a sidewalk hierarchy to establish a physical framework for sidewalk design. The sidewalk hierarchy shown in Figure 5.9 is intended to facilitate the most appropriate allocation of space that encourages people to walk as a part of their everyday routine.

The area immediately surrounding the proposed station location has a Walk Score of 79 out of 100, reflecting a very walkable area where most errands can be accomplished on foot. Walk Scores are determined by awarding points based on the distance from a particular origin to amenities in a variety of categories. Areas with amenities within a 5-minute walk are given maximum points. Population density and road metrics such as block length and intersection density are analyzed when determining Walk Scores.

Due to the large block size on the east side of the study area, there is limited pedestrian connectivity between blocks. Barriers are present along the Bellflower bike trails where private residential fencing prevents residents of a large mobile home community and senior citizen housing development from easily accessing the multi-purpose trail.



FIGURE 5.9 Sidewalk Hierarchy



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5.5.1 SIDEWALK HIERARCHY

The sidewalk hierarchy (Figure 5.9) is designed to support varying levels of pedestrian activity. It provides more space and wider sidewalks in areas with higher pedestrian activity and where greater needs for street furniture and additional pedestrian amenities are needed and narrower sidewalks where such needs do not exist. Sidewalks must serve a variety of competing uses. For example, the sidewalk must provide a buffer between pedestrians and the street, a buffer between pedestrians and store entrances, and serve as a thoroughfare and an amenity zone for items such as street furniture and street trees.

The sidewalk hierarchy is composed of three levels: Level 1 sidewalks, which are the widest sidewalks, should be located along pathway arterials to accommodate high levels of pedestrian activity. Level 2 sidewalks should be located along pathway collectors to accommodate moderate levels of pedestrian activity, while Level 3 sidewalks should be reserved for low-density residential streets. The various levels of the sidewalk hierarchy are defined as follows:

- » **Level 1:** Sidewalks are the widest sidewalks within the hierarchy and should have a minimum width of ten feet when possible in order to support high pedestrian volumes and to accommodate the street trees, benches, outdoor seating, and other amenities. Level 1 sidewalks should be located along pathway arterials and areas with higher density, mixed-use, or commercial land use development, and near the future transit station. Space allocation for Level 1 sidewalks should favor the pedestrian and store frontage to provide adequate passing space between pedestrians and to accommodate for store-front amenities such as outdoor seating within commercial or mixed-use zones.
- » **Level 2:** Sidewalks are slightly narrower in width than Level 1 sidewalks and should have a minimum width of seven feet in order to accommodate moderate levels of pedestrian activity and to accommodate some pedestrian amenities. Level 2 sidewalks should be located along pathway collectors. Space allocation for Level 2 sidewalks should favor the pedestrian and street furniture.
- » **Level 3:** Sidewalks are the narrowest sidewalks within the hierarchy and should be located along low-density residential streets that do not carry high volumes of traffic or pedestrian activity. They should have a minimum width of five feet in order to meet American Disability Act (ADA)

standards and be accessible for all pedestrians regardless of ability. Due to its minimal width, space allocation of Level 3 sidewalks should favor clear lines of travel for pedestrians.

Figure 5.9 illustrates the suggested locations of the various sidewalk levels. The Specific Plan, however, recognizes that sidewalk design and construction often occur under constrained conditions, such as narrow right-of-ways, utilities, grading, and topography. These are all key factors to consider when designing sidewalk improvements.

5.6 STREET TREES

Street trees serve a variety of urban design functions such as acting as a pedestrian buffer, accentuating spaces, creating a sense of enclosure, improving air quality, reduction of heat island effect by providing shade and filtered light, and improving visual aesthetics along a corridor. Street trees should be incorporated whenever possible, especially along pathway arterials. All street trees should be planted in accordance with established City planting standards. Planting a variety of tree species ensures greater resilience and continuity when facing unforeseen events such as diseases and tree pests, and adverse weather and climate.

There are some trees that have been planted along front property lines that help to provide shade for pedestrians. There are ninety-six street trees on Bellflower Boulevard from Linden Street to the 91 -Freeway and forty-four street trees on Mayne Street between Bellflower Boulevard and Clark Avenue.

The streetscape design recommendations are focused on the following roadways: Bellflower Boulevard, Alondra Boulevard, Flower Street, and Mayne Street. Special care should be taken when planting trees in areas with overhead utility lines.

In general, a consistent, well-design landscape plan for the streets can provide shade, introduce seasonal color, define the street edge, and invite pedestrian activity. The following street tree recommendations (Figure 5.10) are based on Bellflower's Master Street Tree Master Plan with some additions.

FIGURE 5.10 **Street Trees**



Pink Trumpet Tree



Elegant Water Gum



Golden Rain Tree



Australian Willow



Golden Trumpet Tree



Desert Willow



New Zealand Christmas Tree



Chinese Pistache



Tipu Tree (Requires 6' Parkways)



Street trees will be arranged along the sidewalk or landscape easements at intervals appropriate with street scale and canopy cover in order to provide a sense of rhythm and movement within the streetscape.

- » Flowering trees and trees with seasonal color have been selected to provide a dynamic color palette. The use of color is key for creating an active landscape which denotes the passing of time and creates visual interest.
- » The plant palette incorporates deciduous plant material to provide shade canopies during the warm season while allowing penetration of sunlight during the cooler months to provide a more comfortable and inviting atmosphere.
- » Incorporate suspended pavement system(s) improving soil volume to support larger tree species development and improve plant health, promoting higher plant success rates. Additionally, these systems provide storm water quality management through capture, evapotranspiration and storage.
- » The placement of trees and planted parkways will provide greater separation for pedestrians from the vehicle traffic, promoting the use of sidewalks by creating a more comfortable experience for pedestrians.
- » The plant palette avoids trees with invasive root structures and trees that drop excessive amounts of leaf and seed pod debris.

5.7 ENTRIES & MONUMENTATION

Key intersections and gateways require greater attention to detail due to their prominent locations and sensitive relationship to the public realm. Special wayfinding signage is often used to mark the entrance or direction to a particular destination. In addition, the treatment of buildings and the public realm at key locations create a landmark and establish a unique sense of identity.

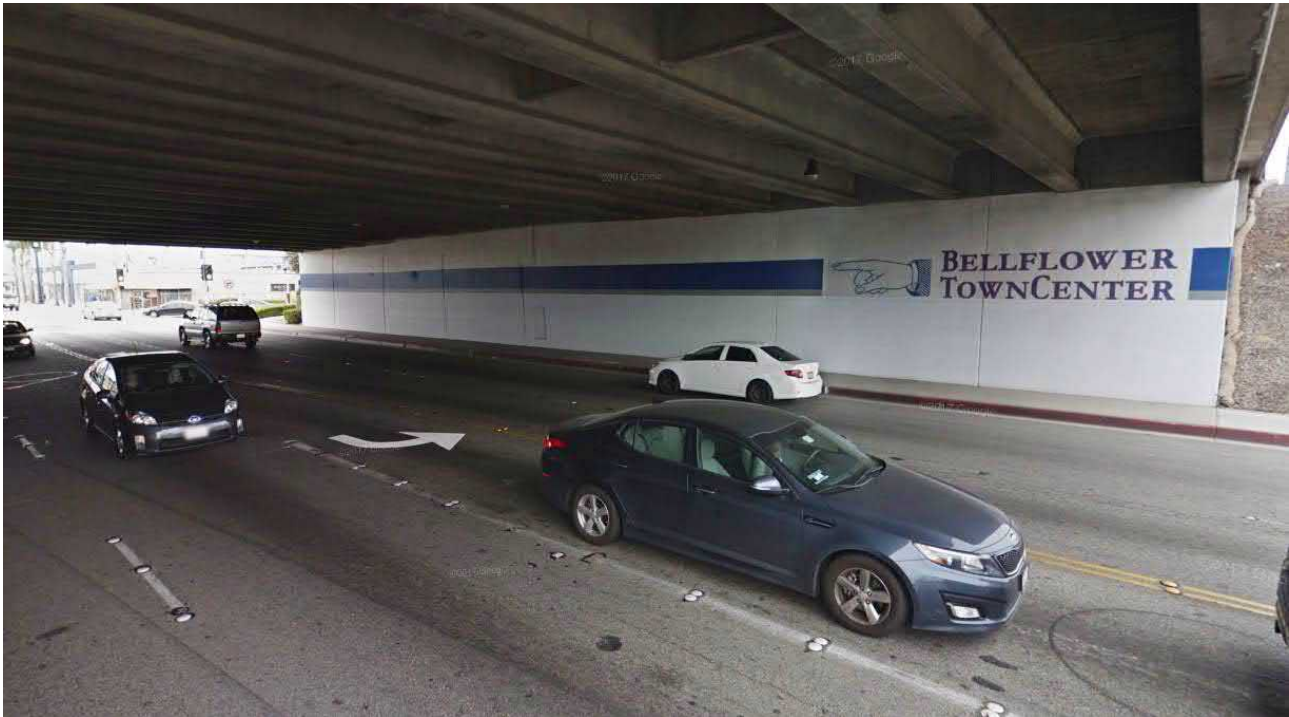
Key opportunity areas exist along major corridors and entrance points in the Specific Plan area including:

- » Existing wayfinding mural under SR-91
- » Existing arched gateway over Laurel Street
- » Future gateway feature at the southwest corner of Alondra Boulevard and Bellflower Boulevard
- » Future landmark feature in the plaza of the future transit station
- » Future special monumentation for the new Fire Museum

5.8 GREEN STREETS

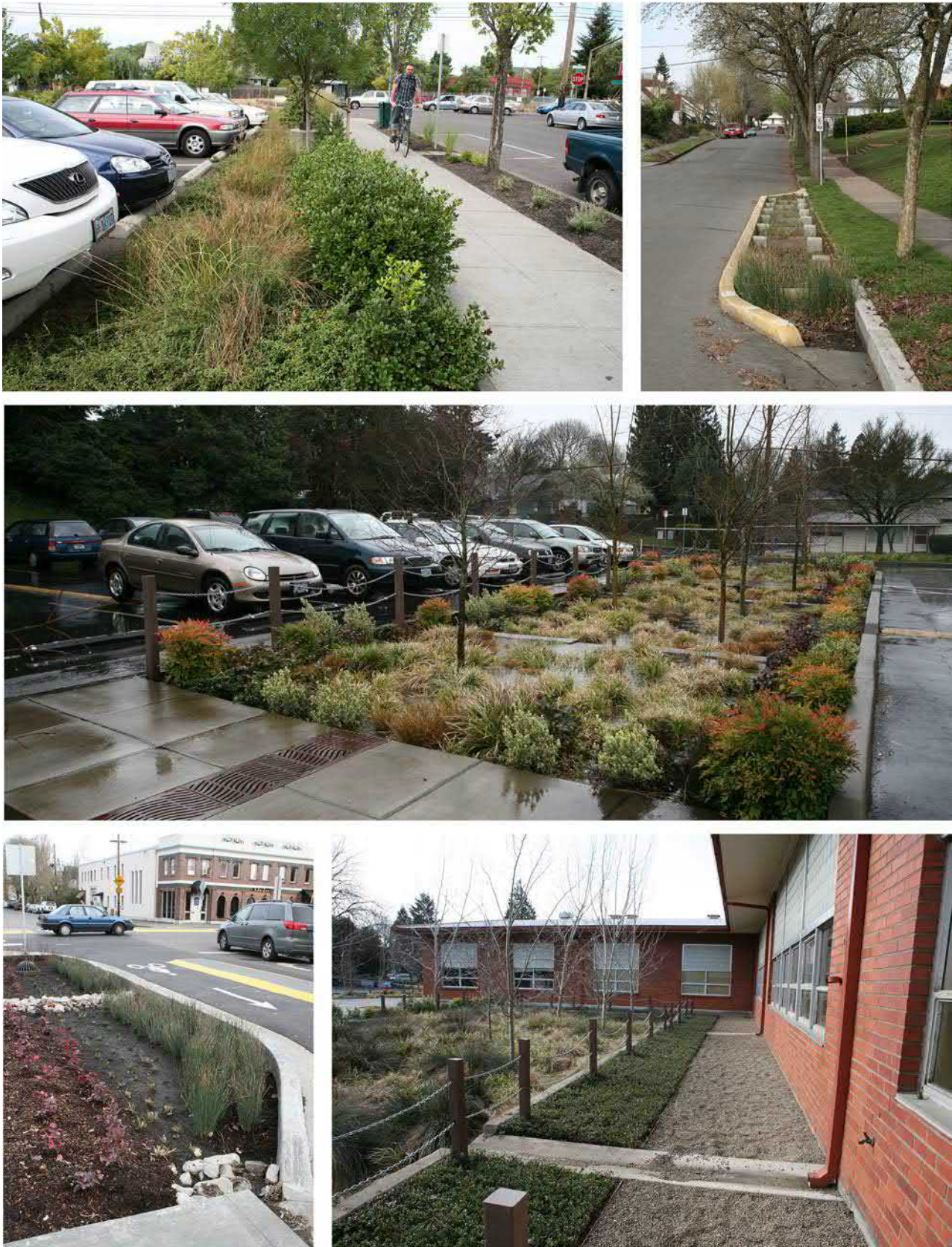
The Downtown Bellflower Station Area Specific Plan encourages the incorporation of sustainable landscape practices to achieve a desired visual aesthetic which includes healthy growing conditions, plant sizing, environmental sustainability in addition to contributing to the long-term development of Bellflower. The following are guidelines for undertaking green street improvements:

- » The use of rain gardens, open tree grates, green roofs and pockets of open space to slow stormwater flow rates, allow natural percolation of runoff and reduce the heat island effect is encouraged.
- » Use of permeable pavement to capture and treat stormwater runoff must be used when appropriate. Examples of permeable paving include porous asphalt, reinforced grass, semi-imperious concrete paving blocks, and reinforced gravel with grass.
- » Stormwater retention system such as rain barrels are also encouraged in order to provide for non-potable uses such as irrigation.



- » Irrigation controllers should be programmed to operate late night and early morning to reduce evaporation.
- » Plants with similar water requirements should be grouped together to utilize hydro zoning techniques. Trees should be pruned annually to maintain appearance, growth and to help develop scaffold branches to prevent toppling and storm or wind damage to specimens.
- » Vegetation in a state of decline or dead plant material should be replaced. Hardscape and landscaped planters should be kept clear of weeds, leaf litter, garbage, and other debris.
- » The governing documents of a common interest development or property owner associations cannot prohibit or include conditions that have the effect of prohibiting the use of low-water use plants or restricting compliance with a local water-efficient landscape ordinance or water conservation measure.
- » Implementation of infiltration best management practices must be strategically placed as not to cause adverse impacts to the proposed building structures. Infiltration within close proximity to foundations and footings is highly discouraged. Where high density and large building footprints occur, the locations of infiltration should be limited to select areas such as landscaping, parking lots, courtyards, plazas, and streets. Opportunities for infiltration include incorporation of underground detention systems with drywells, bioretention planters typically located within pedestrian zone landscape areas and permeable pavement within parking areas of streets and sidewalks.
- » Opportunities for biotreatment features such as storm water planters and bioretention facilities are optimally located within traditional landscape areas adjacent to hardscape or buildings. Biotreatment planters can also be located around the buildings to provide treatment of roof runoff.

FIGURE 5.11 **Examples of Biotreatment Features**



5.9 TRANSIT CIRCULATION

The Specific Plan area has access to a network of public transportation options. Seven bus routes, operated by Metro, the City of Bellflower, and the Cities of Long Beach and Norwalk traverse through the Specific Plan area. The closest existing rail stations (located on the Metro Green Line) are the Lakewood Boulevard and Norwalk stations, 3 and 4 miles, respectively, north of the center of the Specific Plan area. There are no direct bus routes connecting residents of the Specific Plan area to the existing Green Line stations. The Metro Blue Line is approximately eight miles west of the center of the Specific Plan area, and the Metro 127 bus line provides connections to the Compton Station and greater Metro rail system.

5.9.1 BUS NETWORK

Existing Conditions

Buses are an amenity that connect residential districts, employment centers, and transit hubs while providing an alternative to single-occupancy driving. The quality and frequency of service often effects an individual's decision to ride transit. Bus stop amenities also help improve the bus-riding experience; these include shade elements, seating, vegetation, and real-time bus arrival information. Additionally, adequate pedestrian infrastructure at and within one-half mile or more of each stop is important to create a connection to local business and homes. The existing pedestrian hierarchy can be seen in Figure 5.9.

Within the Bellflower TOD Specific Plan area are seven bus lines that can be seen in Figure 5.12. Bellflower Bus operates two local bus services: The North Route and the South Route. These operate at one-half hour headways, the time between bus arrivals. These routes provide access to local amenities including Downtown Bellflower, the historic train depot, and Simms Park.

LA Metro operates two bus lines within the specific plan area. The 127 comes approximately every hour and provides access to Compton. The 128 runs at approximately forty-minute headways during peak periods and one hour off-peak. Long Beach Bus operates the 91, 92, and 93 bus routes in the area that run at twenty-minute headways and connect downtown Bellflower with Long Beach. Norwalk Bus operates Route 1 in Bellflower connecting residents to Norwalk and Rio Hondo College.

An on-board ridership survey undertaken by city staff indicated that 71% of Bellflower Bus riders were choice riders, meaning that they could have made the trip by another means. This number of

riders and percentage of choice riders is likely to change with the opening of the Bellflower Station.

Proposed Improvements

To provide convenient and frequent bus service connection to the future Downtown Bellflower Transit Station, buses will be rerouted to provide direct access at the station site. Transit will also be integrated into the future parking and multi-modal structure to the south of the station site. This will enhance the connection between the surrounding neighborhoods and the station and increase the appeal for riders who may choose to take the bus and train. One proposed improvement is a north-south serving rapid bus service along Bellflower Boulevard.

Future studies will be undertaken to determine route connections, the location of new stops, and the frequency of bus service to serve the future riders and provide connections to local amenities.

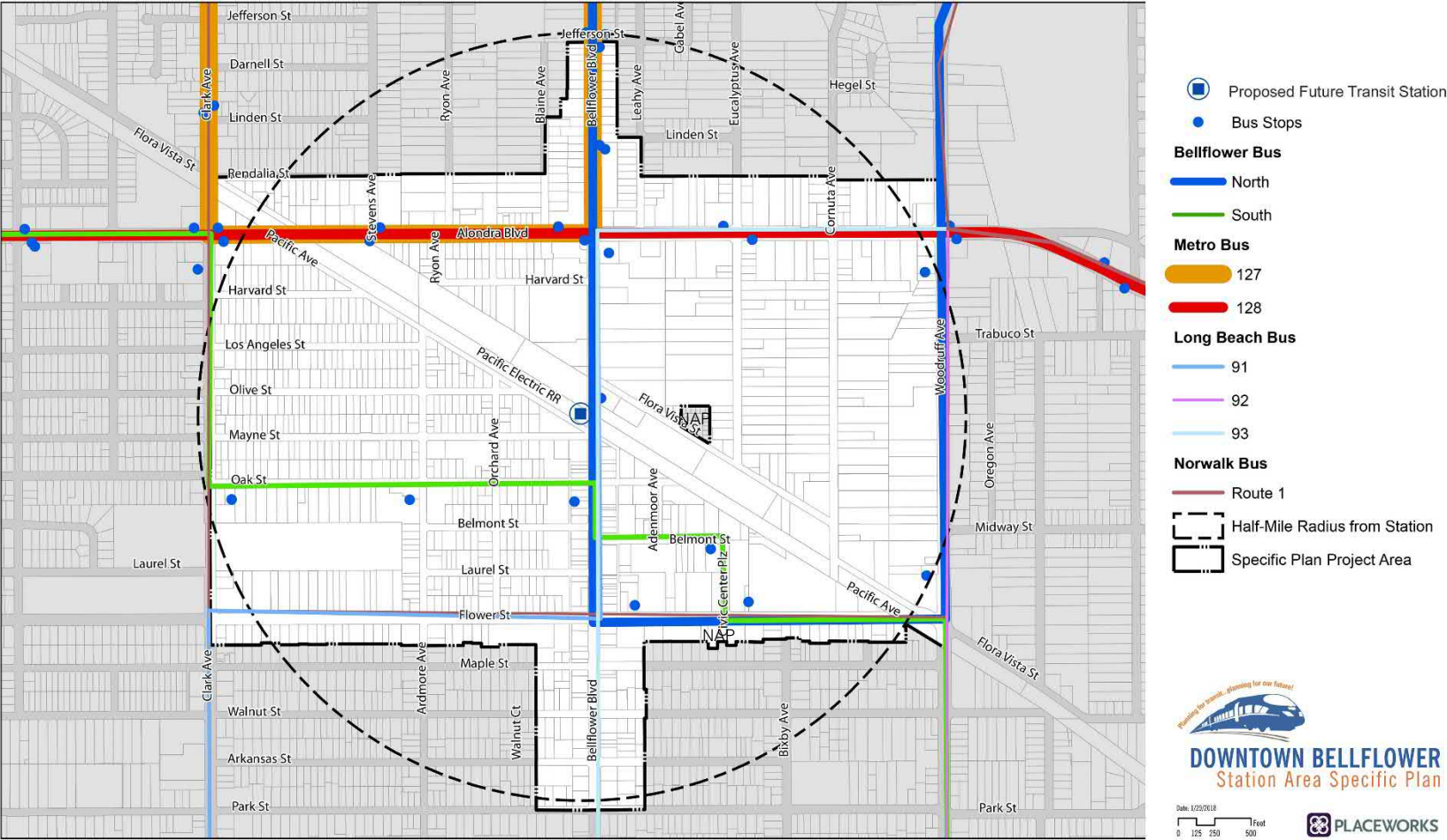
5.9.2 WEST SANTA ANA BRANCH LINE

The Bellflower Station provides an opportunity to unite the City's goals for the downtown area with the benefits of a local connection to the future West Santa Ana Branch Transit Corridor and Metro's extensive transit network. This light-rail line will connect Bellflower and surrounding cities to Downtown Los Angeles to the north and Artesia to the south, and ultimately, to stations in Orange County. The West Santa Ana Branch Transit Corridor was included in a list of projects accelerated by the 2028 Los Angeles Olympics and is anticipated to connect Downtown Los Angeles and Downtown Bellflower with high-quality transit in 2027.

Bellflower's proximity to this transit line will provide opportunities to attract new development, encourage multimodal transportation, improve access to major employment centers, and reintroduce Downtown Bellflower as a thriving, experience-oriented district for residents and visitors.

Although the opening of the transit station is years away, the market is ready to start embracing future transit and to revitalize Bellflower's charming, walkable downtown. By introducing a transit station in the downtown core, Bellflower will create a setting for new high-quality residences and a range of new commercial and entertainment activity that will support and sustain a thriving downtown environment.

FIGURE 5.12 Bus Routes



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5.9.3 WEST SANTA ANA BRANCH CORRIDOR

The West Santa Ana Branch Transit Corridor is the southern 20-mile portion of the full West Santa Ana Branch Transit Corridor, running from The City of Artesia to Union Station in Downtown Los Angeles that can be seen in Figure 5.13.

The goals of the West Santa Ana Branch Transit Corridor include:

- » Economic development/connecting people to jobs.
- » Maximizing neighborhood and station connectivity.
- » Establishing multimodal transit hubs at station locations.
- » Enhancing opportunities for public spaces and commerce.
- » Designing surrounding streets for people and active transportation.
- » Creating affordable and accessible housing—50 percent of region’s housing growth to be built in transit-oriented development.
- » Managing parking effectively.

The Pacific Electric Right-of-way/West Santa Ana Branch Corridor (PE ROW/WSAB) Alternative Analysis Report studied a 34-mile by 8-mile corridor along the former Pacific Electric Railway, now owned by Metro and Orange County Transportation Authority (OCTA). Potential station locations were studied to identify the potential influence of a transit system on land use patterns and urban design. The West Santa Ana Branch Corridor is expected to:

- » Carry up to 70,000 daily boardings by 2040.
- » Cost an estimated \$4.3-4.6 billion (in 2015) in capital costs.
- » Create 59,000 living wage jobs during construction.
- » Generate \$6.68 billion in economic growth over 15-20 years.
- » Result in a significant reduction in the region’s carbon footprint.

The PE ROW/WSAB Alternative Analysis Report determined a single feasible alignment along the former Pacific Electric right-of-way (ROW) for the southern portion of the line that travels from the City of Huntington Park to the City of Artesia, which is currently used as Class I a bike trail in Bellflower.

FIGURE 5.13 **West Santa Ana Branch Transit Corridor**



5.9.4 DOWNTOWN BELLFLOWER TRANSIT STATION

A station along the West Santa Ana Branch Transit Corridor is proposed at Bellflower Boulevard and the Pacific Electric ROW, at the northern edge of Downtown Bellflower. The Downtown Bellflower Station is consistent with the City's longstanding transit-oriented development vision, policies, and plans for Downtown. In addition, it was an historic stop on the Pacific Electric West Santa Ana Branch and is part of the City's transportation heritage, now honored by the restored Pacific Electric Depot.

Of the fifteen proposed station locations on the Eco-Rapid line (see Figure 5.13), the Downtown Bellflower Station was one of six selected as a prototype location that would receive an additional level of research. The six prototype stations each possess community settings and opportunities that best represent study corridor opportunities as a whole.

Station Design Concepts

The city commissioned studies to explore the future station location and relationship to the surrounding site. One study explored two locations for the Bellflower Station. The first station option was located above Bellflower Boulevard with entrances on both the east and west of the roadway. A second station option was located to the west of Bellflower Boulevard with a single main entrance.

Both design options promoted the improvement of pedestrian infrastructure, the integration of the bike path and bus stops, connection to the future parking and multi-modal structure to the south of the station site, and the incorporation of open space and shopping amenities. These design elements activate the station's relationship with the vibrant Downtown Bellflower, which provides an amenity-rich streetscape to the south, and future amenities to the north recommended by this plan.

The conceptual station and site design was based on Metro standards and guidelines and the station's relationship to the surrounding site. Standards and guidelines considered include LACMTA Metro Rail Design Criteria – Section 6 - Architectural Revision 9 (2015), LACMTA Metro Rail Architectural Directive (2010), Federal Transit Administration: Americans with Disabilities Act: Guidance (FTA AADAG), and National Fire Protection Association (NFPA) 130: Standard for Fixed Guideway

Transit and Passenger Rail. These provided guidelines on station and platform sizing, station accessibility, design and material criteria, life safety, and circulation and access elements.

The visualization below (Figure 5.14) is a conceptual view looking south of the future Bellflower Station above Bellflower Boulevard and associated-transit oriented development. The station acts as a gateway to Downtown Bellflower just south of the station. The site acts as a link in the public open space network while providing amenities to commuters and residents. This visualization demonstrates the opportunities to integrate the station into the local area along with enhanced pedestrian and open space amenities that unite the station with the transit-oriented goals of the city. Detailed assessment of pedestrian amenities is required to determine operational feasibility and effect on transportation.

Plans of the plaza and platform levels can be seen in Figures 5.15 through and 5.18.

FIGURE 5.14 Bellflower Station Conceptual Design: View Looking South



FIGURE 5.15 Bellflower Station Conceptual Design
Option 1: Plaza Level Plan

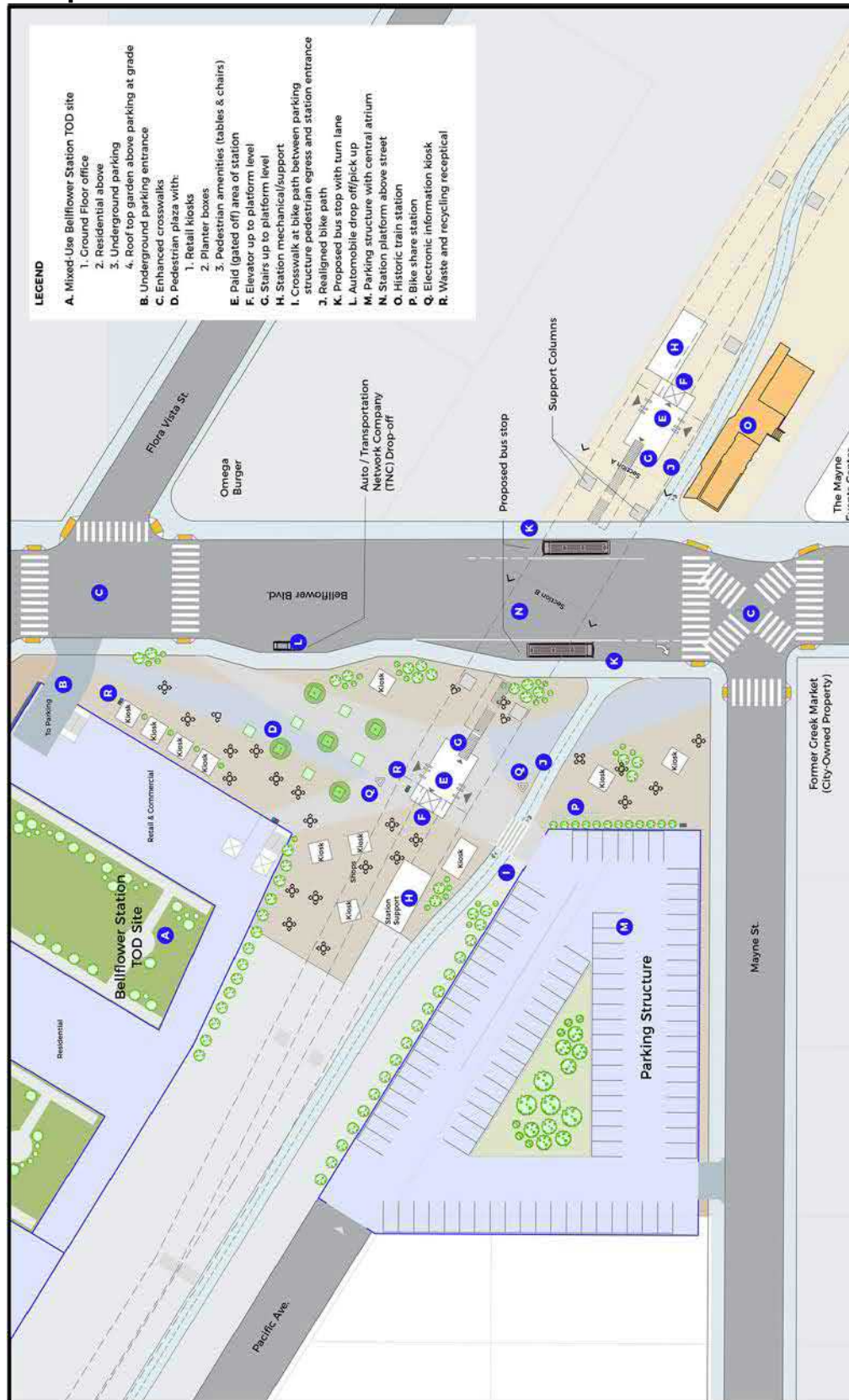


FIGURE 5.16 Bellflower Station Conceptual Design
Option 1: Platform Level Plan

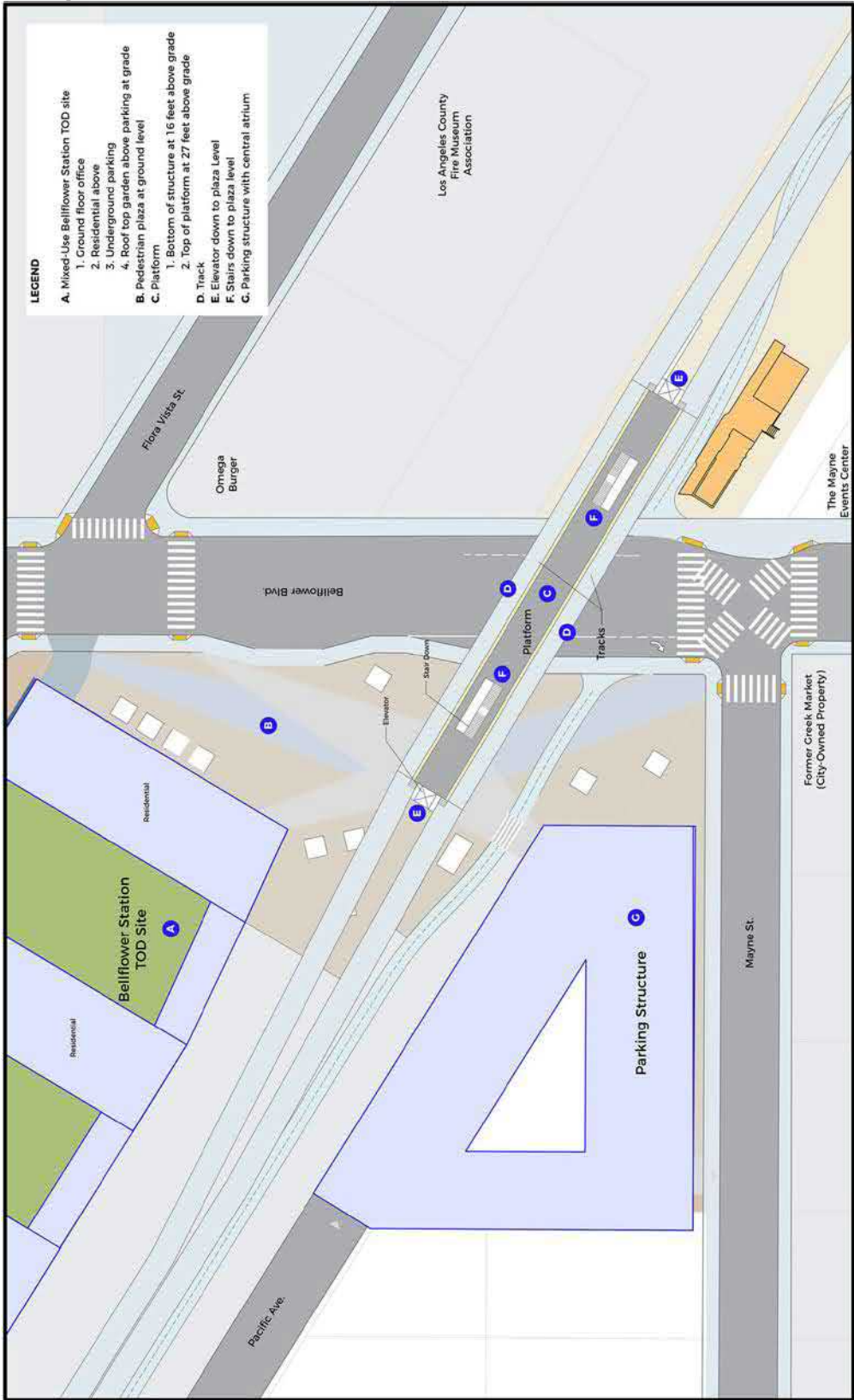


FIGURE 5.17 Bellflower Station Conceptual Design
Option 2: Plaza Level Plan

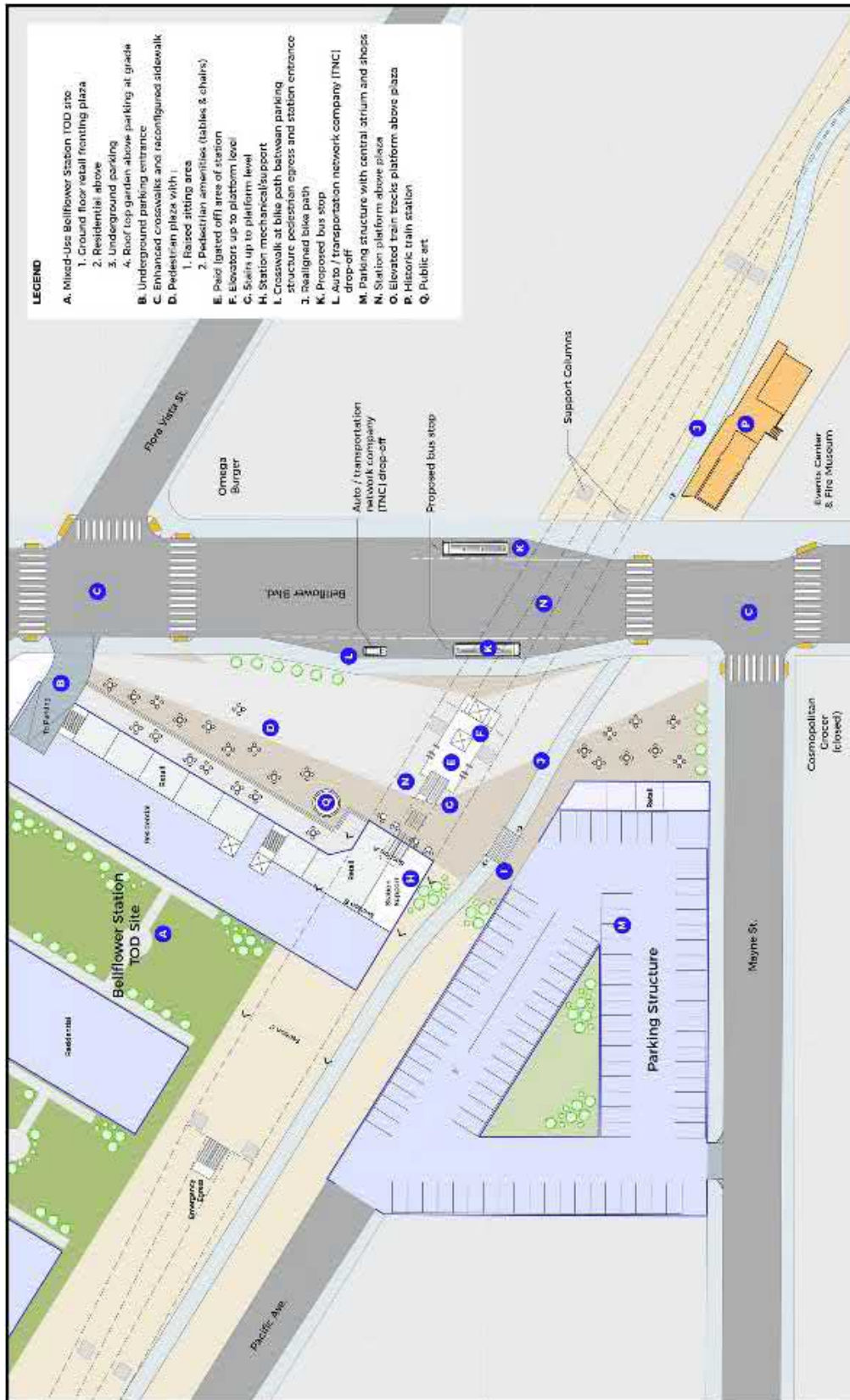
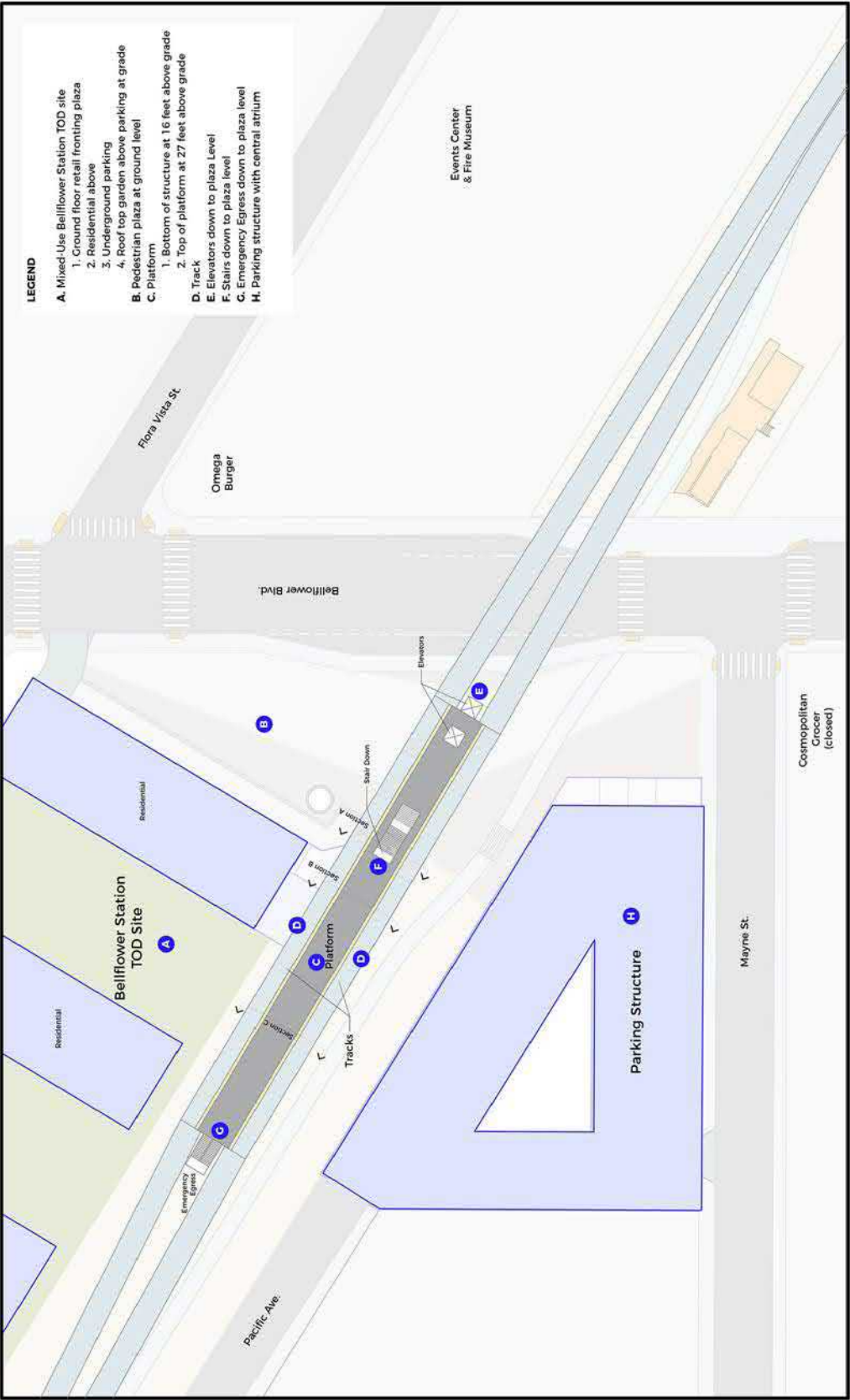
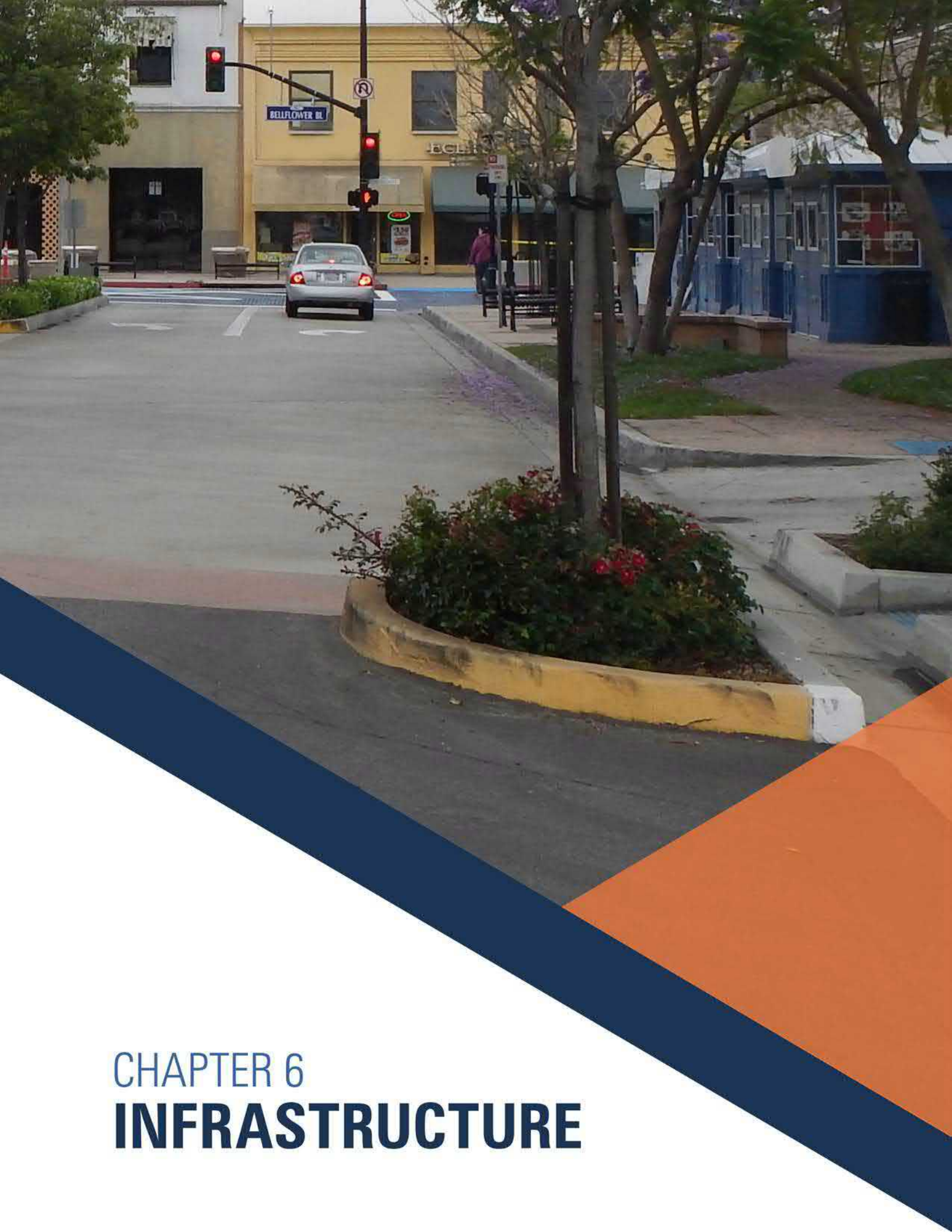


FIGURE 5.18 Bellflower Station Conceptual Design
Option 2 : Platform Level Plan





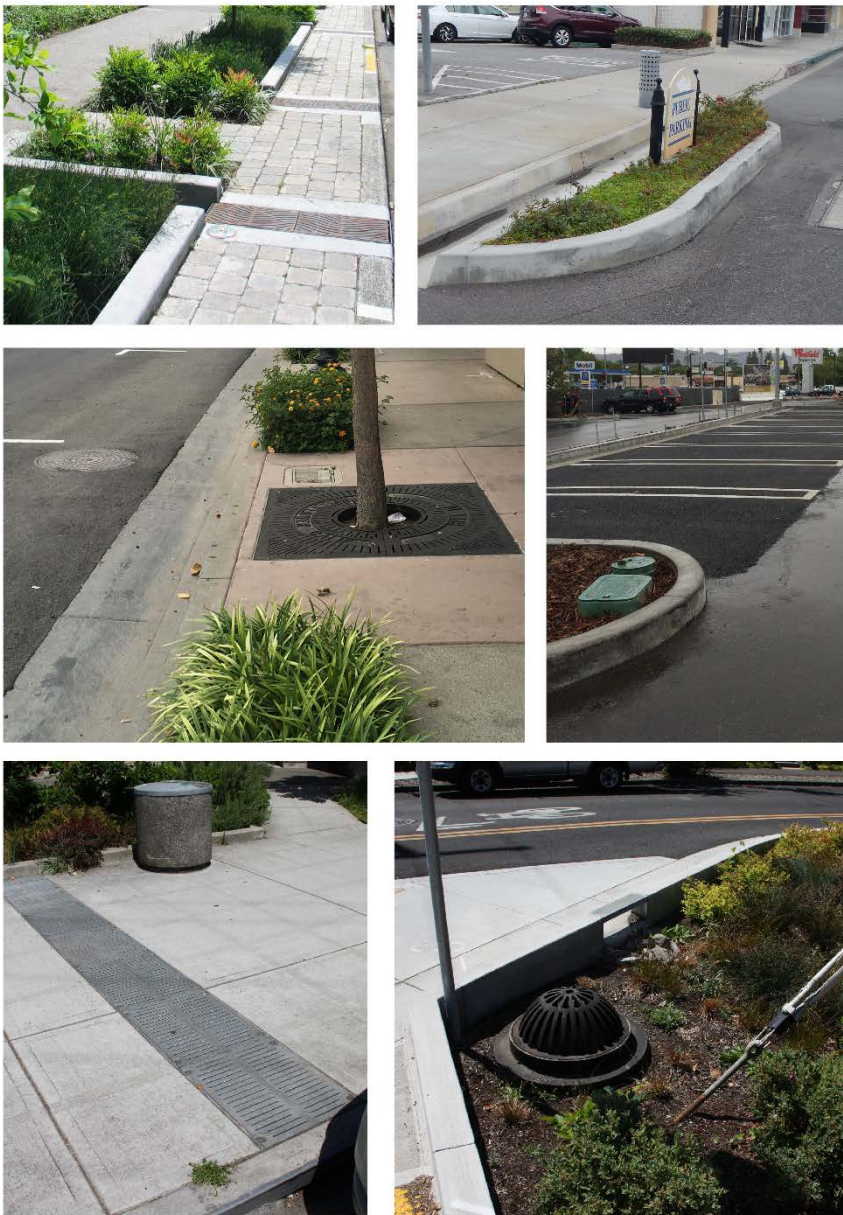
CHAPTER 6

INFRASTRUCTURE

6. INFRASTRUCTURE

Infrastructure and utility services such as water, wastewater, and stormwater drainage are critical elements that support the growth and change envisioned in the Downtown Bellflower Station Area Specific Plan. The infrastructure chapter provides an overview of the existing conditions of the water, wastewater, and storm drainage systems serving the Specific Plan area, along with any recommended upgrades to accommodate future development.

FIGURE 6.1 Examples of Infrastructure Elements



6.1 WATER

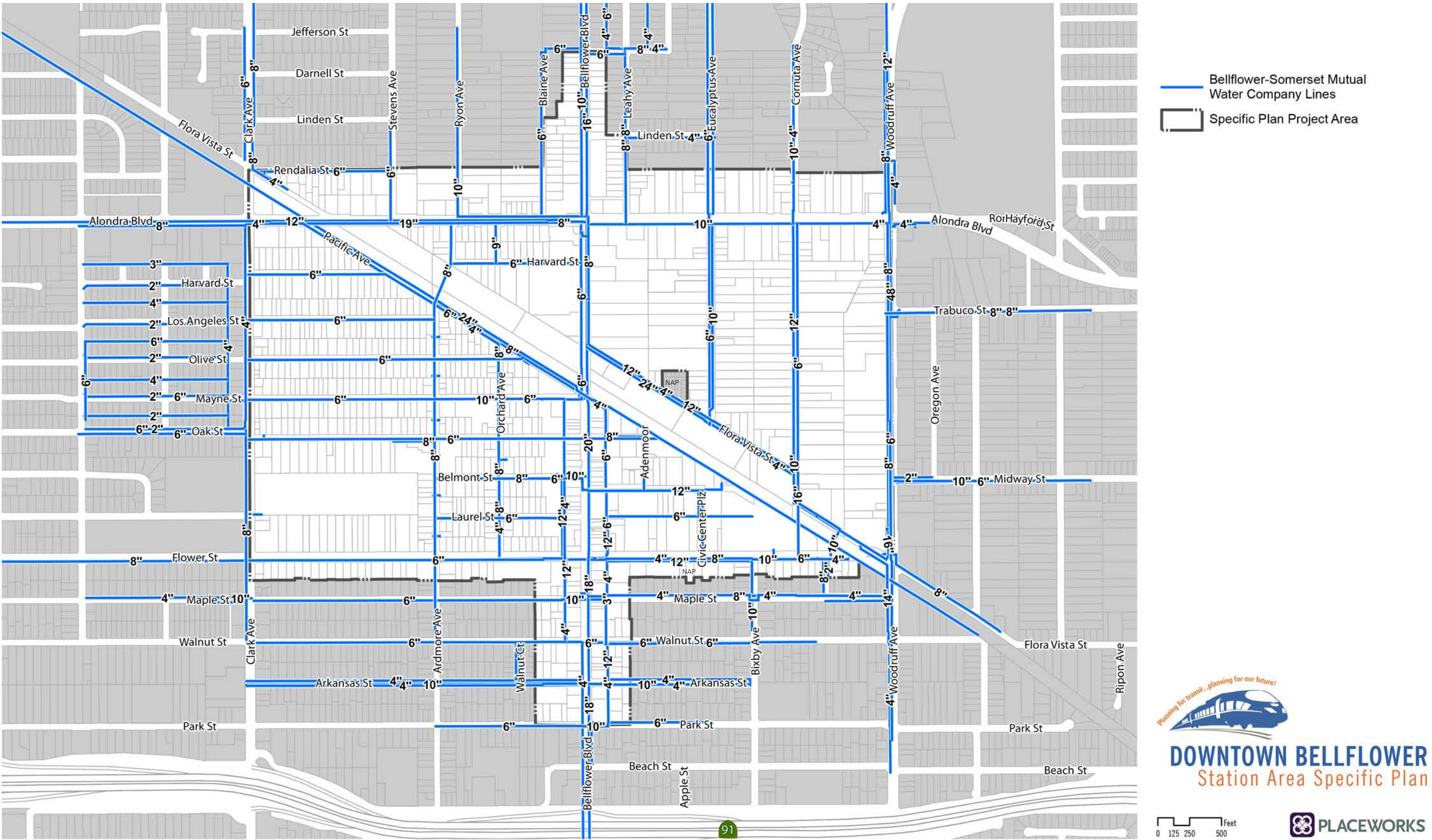
The Bellflower Somerset Mutual Water Company (BSM) provides potable water within the boundary of the Downtown Bellflower Station Area Specific Plan. Customer needs are served through local groundwater with supplemental quantities coming from recycled water. BSM owns roughly three quarters of the ground water supplies needed to serve its customers and leases the remainder from the City of Bellflower. Currently, most parcels within the Specific Plan area are not used as local groundwater recharge facilities. The primary groundwater production wells for the Specific Plan area are at the City Yard.

The Specific Plan area is serviced by pipes varying from four- to 48-inches. The largest pipe lines run along Woodruff Avenue, Pacific Avenue, Flora Vista Street, Bellflower Boulevard, and Alondra Boulevard. Most of the distribution pipes off the main lines are six- to ten-inches in size. Figure 6.2, Existing Water Lines, illustrates the water pipe line size and location for the Downtown Bellflower Station Area Specific Plan area. The pipe lines are composed of a variety of materials including cast iron, ductile iron, and asbestos cement.

The water lines are sufficient for the current land uses in the Specific Plan area. There have been no recent improvement projects and there are no proposed improvement projects for the Specific Plan area.

Given the increase in water demand from the proposed increase in land use intensity for the Mixed-Use Transit Overlay Zone (MUTOZ) area, it is likely that water mains would need to be upsized to provide water for both domestic use and fire protection. Proposed increases in water demands associated with the Specific Plan will need to be accounted for in future planning studies. The water provider, BSM, will need to evaluate and holistically analyze future development to decide whether water facilities require upgrades.

FIGURE 6.2 Existing Water Lines



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6.2 WASTEWATER SERVICES

Two sanitary wastewater systems exist in the Specific Plan area – local lines operated by the City of Bellflower and trunk lines operated by Los Angeles County Sanitation Districts (LACSD). The local lines are a series of primarily eight-inch gravity mains connecting to the trunk lines along Clark Avenue, Park Street, and Flora Vista Street. Figure 6.3, *Existing Wastewater System*, shows the location, width, and owner of the existing wastewater lines.

The City's wastewater collection system is maintained and managed by Los Angeles County Sanitation Districts (LACSD). The collection system consists of about 99 miles of gravity wastewater lines. Wastewater is collected by City lines and conveyed to trunk lines owned and maintained by Los Angeles County Sanitation District #2, which then treats the wastewater at a regional facility. Wastewater flows from the City ultimately flow to LACSD wastewater trunk lines and are treated by either the Los Coyotes Water Reclamation Plant (WRP), or the Joint Water Pollution Control Plant (JWPCP) in Carson. The Specific Plan area sewage flows only to the JWPCP.

The Specific Plan area was included in the City of Bellflower Wastewater Master Plan (SMP) drafted in March 2007. The purpose of the SMP was to evaluate the existing wastewater collection system in the City of Bellflower, which included modeling infrastructure to determine any segments in need of improvement and propose additional improvements for the future. The SMP notes three improvement tiers for wastewater lines across the City, based on depth of flow to diameter of pipe (d/D) ratio. These tiers correspond to priority for wastewater line improvement and correction measures, as shown in Figure 6.4.

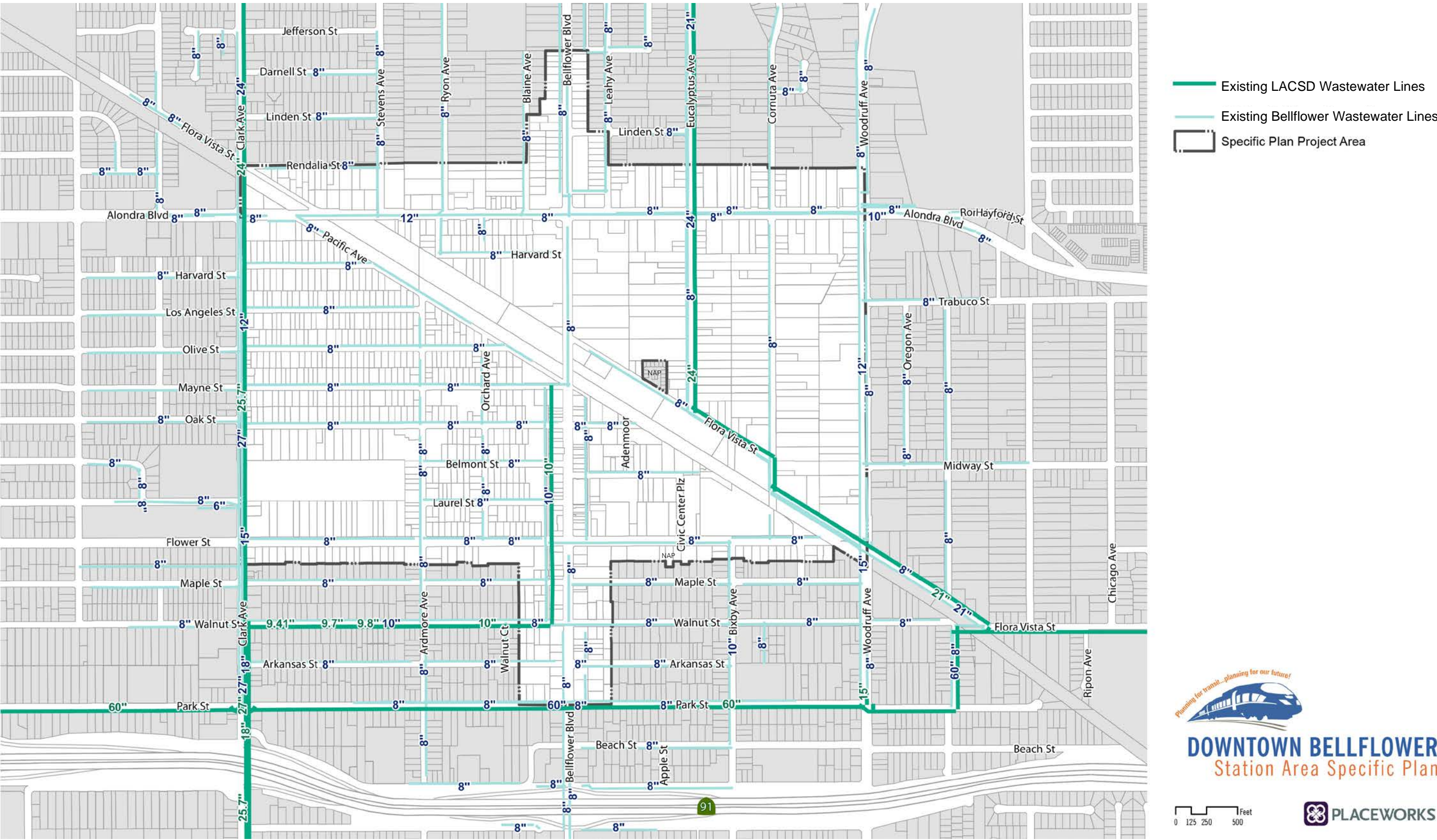
Priority 1 Improvement lines are located along Alondra Boulevard, Clark Avenue and Woodruff Avenue. Priority 2 Improvement lines are on Alondra Boulevard and Clark Avenue. The City currently has no wastewater improvement projects proposed for the Specific Plan area. In addition, there are no existing improvements required for the LACSD trunk lines or the JWPCP treatment facility.

Wastewater infrastructure in the Specific Plan area would likely need to be expanded to provide capacity for flows created by additional residential units and commercial space. There is adequate capacity in the LACSD trunk lines and the JWPCP treatment facility to allow for the proposed additional wastewater flows generated by the implementation of the Specific Plan, however the following City lines may require improvement to accommodate future buildout.

- » A portion of the MUTOZ area ultimately flows north to eight- and twelve-inch lines that are designated as Priority 1 and Priority 2 improvement areas. A portion of the MUTOZ area also flows south and west and ultimately ties into the twelve- and fifteen-inch lines along Clark Avenue which are also designated as Priority 1 and Priority 2 improvement lines.
- » Flows from the eastern MUCOZ area traveling to the west will pass through a Priority 3 improvement line. The western MUCOZ area ties into eight-inch lines that are adequate. However, these lines flow in a westerly direction and tie into a Priority 2 eighteen-inch line on Clark Avenue, and may pass through a Priority 3 improvement line on Ardmore Avenue.

Improvements to lines within the Specific Plan area will be addressed through development impact fees for all new construction. New developments will be required to pay fees to the City associated with the increase in wastewater flows and connection fees imposed by LACSD. Additionally, any new development that may impact existing LACSD facilities will be subject to review and requires LACSD approval before development can continue. Potential impacts to LACSD facilities and associated LACSD review requirements and connection fees must be analyzed on a project by project basis.

FIGURE 6.3 Existing Wastewater System



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6.3 FLOOD CONTROL AND STORM DRAINAGE

The Specific Plan area is mostly built out with a relatively high impervious condition. During storm events, most runoff does not have the opportunity to infiltrate and recharge groundwater except areas where there are existing pervious surfaces. This results in water flowing to stormwater drainage. Recent projects, such as The Mayne Events Center/Fire Museum, implement techniques that increase perviousness and provide opportunities for infiltration.

The City of Bellflower is served by two primary drainage systems operated and maintained by the City of Bellflower and the Los Angeles County Department of Public Works (LACDPW). Stormwater flows into City of Bellflower catch basins and storm drain pipes that range from eighteen-inches to 45-inches in diameter and are primarily reinforced concrete pipe (RCP). Stormwater then flows into LACDPW drainage culverts up to 90-inches wide. Stormwater flows discharge to the Los Cerritos Channel and ultimately into San Pedro Bay, Los Alamitos Bay, and the Los Cerritos Estuary. A portion of the Specific Plan area will be treated by the Caruthers Storm Capture facility when it is completed. The location and width of the storm drainage lines within the Specific Plan area are shown in Figure 6.5, *Existing Storm Drainage*.

The City's flood control is sufficient, however, LACDPW has noted that the County storm drain lines serving the Specific area have design flow capacities that are less than the 50-year frequency storm event. There are no planned improvement projects to the storm drainage system for the storm drain lines serving the Specific Plan area.

The implementation of the Specific Plan will not substantially alter the existing stormwater drainage pattern of the area and will use the existing drainage facilities within the public right of way. The City of Bellflower currently uses dry wells, tree wells, modular wetlands, bio-retention basins, and porous pavement in the downtown area. Additional methods to decrease stormwater runoff are included in the following section, *Sustainable Infrastructure Recommendations*.

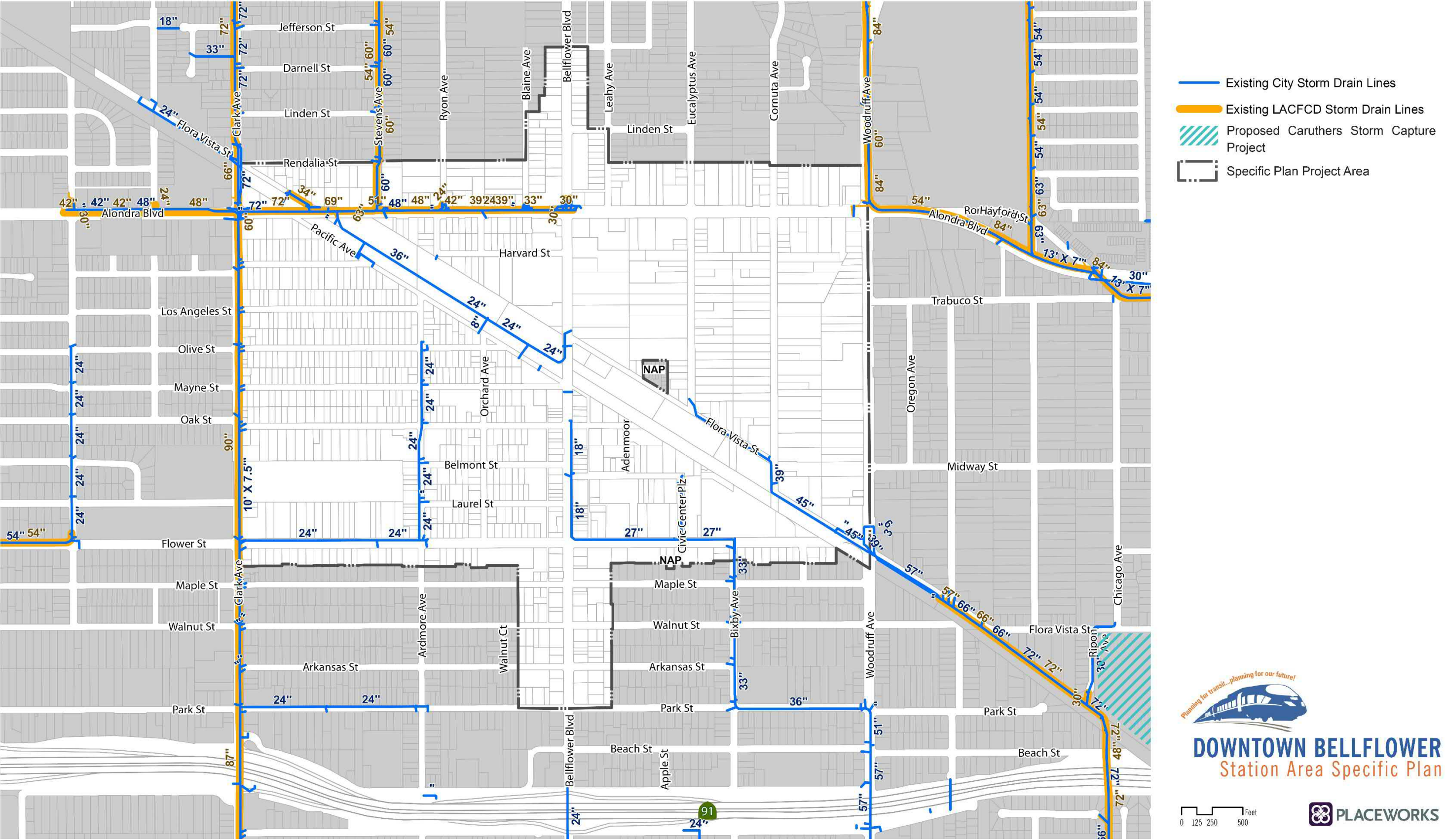
Additionally, the Los Angeles County Fourth-Term Municipal Separate Storm Wastewater Systems (MS4) permit requires that new development and significant redevelopment projects incorporate Low Impact Development Best Management Practices (LID BMPs), which will result in more runoff retained on-site, further reducing flows. LID BMPs address post-construction

storm water runoff management and preserve a site's predevelopment hydrology by minimizing the loss of natural hydrologic processes such as infiltration.

Infiltration is important to retain because when stormwater falls onto impervious surfaces, it cannot seep into the ground and therefore can cause flooding on roadways. Impervious roadways frequently wash stormwater runoff containing harmful pollutants into nearby water bodies, such as rivers and flood control channels, which lead directly to the ocean. These harmful pollutants include substances commonly present on roadway surfaces, such as dirt, oil, grease, toxic chemicals, and trash, and can harm wildlife in local water bodies as well as nearby plants and vegetation. Impervious roadways can also contribute to problems with stormwater quantity. Unlike flood control measures that are designed to handle peak storm flows, LID BMPs and treatment control BMPs are designed to retain, filter or treat more frequent, low-flow runoff or the "first-flush" runoff from storm events.

LID BMP options include first prioritizing full retention infiltration systems as they provide full retention of the water volume and can also contribute to groundwater recharge in some areas. The second priority is capturing and reusing ("harvest and reuse") stormwater on-site for either landscape irrigation or toilet flushing. Harvest and reuse BMPs are also full retention BMPs that provide additional water conservation benefits. Projects that have demonstrated they cannot infiltrate and/or harvest and reuse 100 percent of the water quality volume may manage the remaining volume through the use of biofiltration BMPs which treat stormwater and release it back into the storm drain system.

FIGURE 6.5 Existing Storm Drains



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6.4 SUSTAINABLE INFRASTRUCTURE RECOMMENDATIONS

The Specific Plan recommends adopting sustainable infrastructure into new development in an effort to improve water conservation, reduce potable water usage, support green infrastructure features within the public right-of-way and reduce the environmental footprint of the City. The following best practices and emerging trends and technologies that should be incorporated where feasible within the future redevelopment opportunities.

6.4.1 GREEN STREET FEATURES

Green streets are sustainable design features with many benefits. Green street design components include stormwater infiltration planters within parkways to treat lot runoff and roadway runoff; bulb out planters that provide traffic calming along with runoff treatment, tree boxes and light reflective paving surfaces which reduce heat island effects. Green street features should be coordinated with Public Works.

6.4.2 GRAY WATER SYSTEMS

Gray water systems collect and reuse the relatively clean waste water from showers, sinks, washing machines, and other kitchen appliances for toilet flushing or exterior landscaping, which can greatly reduce on-site potable water usage and wastewater generation fees. The process typically includes routing gray water, treating the water, and reusing the treated gray water within the building for toilet flushing or landscaping irrigation. Gray water systems are especially opportune and cost effective multifamily residential developments.



Bulb out planters provide a location to plant trees and other plants.



Stormwater infiltration planter between the street and sidewalk.



Bulb out planter with rainwater capture



Gray water and rainwater capture can be reused for landscape irrigation and toilet flushing.

6.4.3 CISTERN AND REUSE SYSTEMS

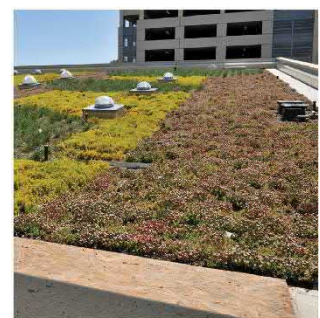
Cisterns are tanks that collect runoff water from impervious areas and store the water for toilet flushing, irrigation, or other uses. Both small- and large-scale harvest and reuse systems may be feasible based on certain project types. Depending on the type of potable demand for landscape irrigation and toilet flushing of a new development or redevelopment, the implementation of a harvested rainwater BMP would provide a multi-benefit solution that could satisfy both water quality regulations and provide for a sustainable water quantity solution that would offset potable water costs. The efficiency and cost-effectiveness for harvest and reuse systems increases when combined with on-site gray water recycling systems.

6.4.4 GREEN ROOFS AND GREEN WALLS

Green roofs and green walls offer some of the most advanced ways to reduce stormwater runoff volumes and common pollutants. As open space becomes more limited within high density areas, green roofs provide a solution with many additional benefits including stormwater treatment, internal and external cooling effects for the building and aesthetic benefits, all within a shared footprint. Green roofs are most feasible when there is a sturdy building structure included in a project. On the other hand, green walls require less structural stability and can be implemented on almost any vertical surface. Some opportunities include implementing green walls on the sides of large, above-ground parking structures. Green roof/wall design can be combined with harvest and reuse cisterns and gray water systems to provide a constant source of treated water for irrigation without increasing demands on local and regional potable water supplies.



Cisterns can be located above or below grade.



Examples of green roofs and walls.

6.5 BAMU OVERLAY ZONE

6.5.1 COMMUNITY DESIGN FRAMEWORK AND PUBLIC IMPROVEMENTS PLAN

The Community Design Framework and Public Improvements Plan establishes a vision for the Bellflower Alondra Mixed-Use (BAMU) Overlay Zone area. The Community Design Framework and Public Improvements Plan provides a conceptual understanding of the overall design intent and vision for the project area to provide guidance for future project applicants. The following elements comprise the Community Design Framework and Public Improvements Plan:

- A. **Functional Connectivity.** The BAMU Overlay Zone seeks to establish strong connections with the City's downtown area through the establishment of a vibrant pedestrian-oriented environment, connection to existing and future planned transit options and maximizing the potential of integrated mixed-use developments that contribute to a strong local economy.
- B. **Physical Connectivity.** The BAMU Overlay Zone promotes the establishment of on-site and off-site amenities to further enhance pedestrian scale and function. The Overlay Zone encourages the design of buildings and structures to enhance accessibility.
- C. **Framework Vision Plan.** In collaboration with the residents of Bellflower, a framework vision plan was established to guide the standards of provisions contained within the BAMU Overlay Zone. The framework vision plan describes the opportunities and constraints within the Overlay area and identifies the key features that influence the policies contained within the Overlay Zone. The Vision Plan is illustrative only and may require additional approvals for use of rights-of-way. The Framework Vision Plan is shown in Figure 6.6: Framework Vision Plan.
- D. **Economic Development.** The primary intent of the BAMU Overlay Zone is to establish a comprehensive economic development strategy that enhances opportunities for the private sector to invest in the Overlay area. Land use standards, regulations and permitted uses have been developed with the expressed purpose of encouraging investment in the Overlay area.

FIGURE 6.6 BAMU Framework Vision Plan



FIGURE 6.7 BAMU Conceptual Community Design Illustration



E. **Conceptual Community Design Illustrations.** In order to provide a visual depiction of the spirit and intent of the BAMU Overlay Zone, conceptual illustrations have been developed. These illustrations visually describe what may result from the realization of the standards and provisions contained in the BAMU Overlay Zone. These illustrations include:

1. Figure 6.7: Bellflower/Alondra Community Design Illustration provides a “bird’s eye” view of the Bellflower/Alondra intersection.
2. Figure 6.8: Conceptual Streetview Concept – Bellflower Boulevard provides a street level view of prototypical mixed-use development along Bellflower Boulevard.
3. Figure 6.9: Conceptual Streetview Concept – Eucalyptus Avenue provides street level view of prototypical residential development along Eucalyptus Avenue.

FIGURE 6.8 BAMU Conceptual Streetview Concept – Bellflower Boulevard

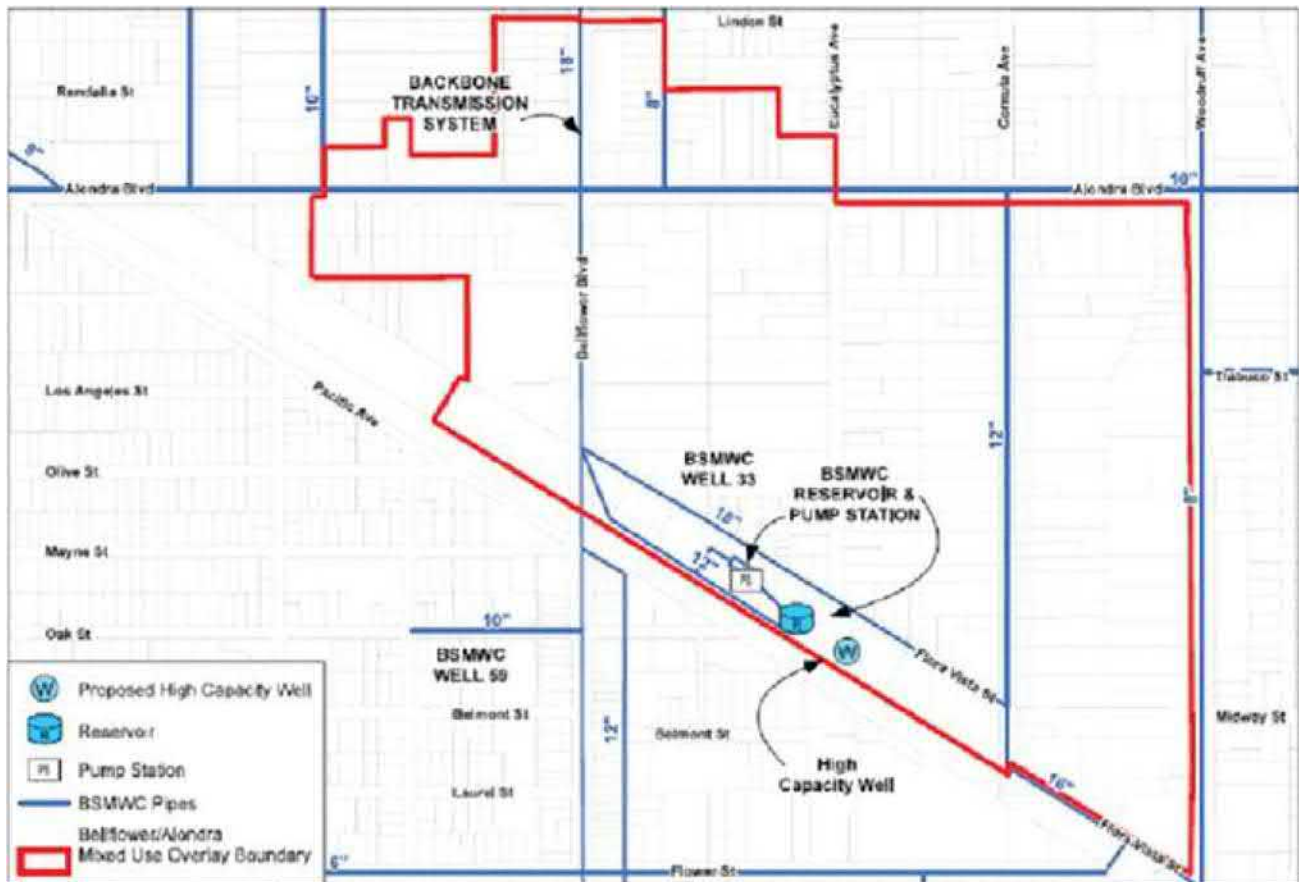


FIGURE 6.9 BAMU Conceptual Streetview Concept – Residential Sub Area

6.5.2 BAMU INFRASTRUCTURE

Water and wastewater infrastructure improvements within the BAMU Overlay Zone have been evaluated through the 2008 Bellflower Municipal Water System Water Master Plan and the 2007 City of Bellflower Wastewater Master Plan. As development projects are approved within the BAMU Overlay Zone, required infrastructure improvements must comply with these documents.

- A. **Water.** The BAMU Overlay Zone is currently served by an eighteen (18) inch backbone transmission main within Bellflower Boulevard, south of water Subsystem 2. The main extends along the Bellflower Bike Path and terminates at the City yard. Other water transmission pipes ranging in size from eight (8) inches to twelve (12) inches are contained within Alondra Boulevard, Leahy Avenue, and Flora Vista Street. Existing transmission pipes are also present in Cornuta Avenue to the east of the BAMU Overlay Zone, and just east of Bellflower Boulevard intersecting with Oak Street and Belmont Street. Refer to Figure 6.10: Existing Water System.
 - 1. Per the Bellflower Municipal Water System 2008 Water Master Plan, the primary improvement recommended within the BAMU Overlay Zone is a high capacity well at the City Yard. Other backbone and secondary transmission pipes within the BAMU Overlay Zone will remain in place and are not slated for upgrades. Pipes within Systems 2 and 3 are anticipated to be increased from their current three (3) inch or four (4) inch sizes to six (6) inch or eight (8) inch sizes.

FIGURE 6.10 BAMU Existing Water System

2. All future development projects within the BAMU Overlay Zone must comply with the 2008 Water Master Plan and its successor(s). Refer to 6.10: Existing Water System.

B. **Wastewater.** The BAMU Overlay Zone is currently served by the City's 95-mile gravity wastewater system. Tributary areas 43, 44, and 45 are located within the eastern portion of the overlay, with the intersection of Bellflower Boulevard and Alondra Boulevard served primarily by existing mains within those streets. Deficiencies are identified in the City's Wastewater Master Plan within Alondra Boulevard and Woodruff Avenue. Several lines adjacent to Alondra Boulevard and Flora Vista Street are also identified as deficient. The Master Plan confirms that the deficient reaches of wastewater mainlines are distributed throughout the City within Tributary Areas 1, 11, 28, 29, 30, 42, 44, 48, 49, 52, 59, 65, and 66. However, the most severe deficiencies occur in Tributary areas 48 and 42. Tributary area 48 is located directly east of Woodruff Avenue, to the east of the BAMU Overlay Zone. Refer to Figure 6.11, Existing Wastewater System. To alleviate capacity issues, two (2) alternatives were studied that involve diverting wastewater flows to County Sanitation

district trunk wastewater upstream of their current discharge points. Alternative 1 proposes four (4) connection points to eliminate deficiencies in McNabb Avenue south of Somerset Boulevard and Alondra Boulevard between Woodruff Avenue and McNabb Avenue. Deficiencies in Woodruff Avenue between Somerset Boulevard and Alondra Boulevard would be reduced significantly. The four (4) connection points are as follows:

- a. The west side wastewater in Woodruff Avenue at Somerset Boulevard;
- b. The east side wastewater in Woodruff Avenue at Somerset Boulevard;
- c. McNab Avenue at Somerset Boulevard;
- d. Carfax Avenue at Somerset Boulevard.

FIGURE 6.11 BAMU Existing Wastewater System



In addition to improvements listed above, a new eighteen (18) inch wastewater line would be built along Alondra Boulevard from Woodruff Avenue to Eucalyptus Avenue and connect to the CSD East Compton Boulevard relief trunk line. The

diversion would take the flow from the area north of Alondra Boulevard in Area 48 and divert it to the CSD trunk line. Refer to Figure 6.12: Future Wastewater System.

FIGURE 6.12 BAMU Future Wastewater System



- C. **Circulation Improvements.** Circulation improvements for the BAMU Overlay Zone seek to ensure that future development within the project area and the surrounding are consistent with the General Plan buildout assumptions. At the time the BAMU Overlay Zone was established in 2014, an evaluation of potential future traffic conditions and impacts, based upon the estimated buildout potential of the project area, was completed. Analysis utilized the traffic impact study prepared by Hartzog & Crabill, Inc. on May 24, 2011. The traffic impact study recommended mitigation measures for several roadway segments that were forecasted to operate at deficient levels of service with the proposed redevelopment project. The buildout assumptions for the BAMU Overlay Zone were evaluated and verified that the mitigations identified in the Hartzog and Crabill study provided an appropriate level of mitigation for the assumed buildout of the BAMU Overlay Zone.





CHAPTER 7

DESIGN GUIDELINES



7. URBAN DESIGN GUIDELINES

Design will play a crucial role in implementing the vision of creating of a transit supportive, high quality, pedestrian-friendly area. The layout of future development on infill sites, building placement and orientation, the design of public spaces, architectural character, and landscaping all contribute to the “sense of place” that is uniquely Bellflower. The design guidelines are intended to provide guidance to builders, engineers, designers, architects, city staff, and decision-makers from conceptual design to implementation. The following design principles have been established to reinforce the overall objectives of the Specific Plan:

ENCOURAGE TRANSPORTATION SUPPORTIVE DEVELOPMENT.

With the future development of the new Metro station, it is essential that all new investment within the Specific Plan is transit supportive. This includes ensuring adequate land use densities, creating an inviting pedestrian environment, and providing amenities for multi-modal transportation including bicyclists, pedestrians, and transit riders.

The Specific Plan area should be designed so that pedestrians, bicyclists, and vehicles can all safely coexist. Conflict zones should be reduced, and non-vehicular traffic and circulation should be encouraged. Convenient and comfortable pedestrian paseos connecting residential, commercial, and open space uses improves pedestrian, as well as vehicular circulation.



CREATE VIBRANT PUBLIC PLACES. There are many components that collectively help create a vibrant and unique downtown. The placement of buildings, street trees, attractive landscaping, decorative lighting, and pedestrian scaled amenities can all help to create a comfortable and memorable environment. Interesting building forms and facades can positively influence the pedestrian experience and help in generating pedestrian activity and increasing a sense of security.

While the Bellflower and Alondra corridors have historically been oriented to the automobile, it is the people, not the cars upon which downtown Bellflower depends. New development and redevelopment along the Bellflower and Alondra corridors should encourage people to stop and utilize the spaces as opposed to just pass through. Achievement of this principle can include human-scale details, signage, cohesive architectural imagery, pedestrian amenities and strong relationships between the buildings, sidewalks and other outdoor spaces.

PROMOTE HIGH QUALITY DESIGN. Building materials and landscaping should be chosen carefully for their ability to be maintained in a cost-effective manner at the same high quality as when they were originally installed.

Buildings should also incorporate sustainable design practices. The daylighting of buildings, the use of energy efficient appliances, and incorporating permeable surfaces are some of the ways to reduce energy demand and promote resource conservation.

7.1 ABOUT THE DESIGN GUIDELINES

The design guidelines should be used in conjunction with the development standards in Chapter 4 to evaluate proposed developments. While the design guidelines promote a quality design, they are not a set of rigid requirements. They are general and illustrative in nature and are intended to encourage creativity and variety on the part of licensed designers. In some instances, one guideline may be relaxed in order to accomplish another, more important, guideline. The overall objective is to ensure that the intent and spirit of the design guidelines are followed to attain the best possible design solutions.

Existing guidelines must be followed for underlying zoning found in Title 17 of the Bellflower Municipal Code ("BMC"). Design guidelines for the existing overlay zones and the proposed new overlays, Mixed-Use Transit Overlay Zone (MUTOZ) and Mixed-Use Corridor Overlay Zone (MUCOZ) can be found in this section.

7.2 SITE DESIGN (MUTOZ AND MUCOZ)

Site design is an important process critical to any development that may occur in Bellflower. A project's site plan will determine how buildings are placed on a site, where access will occur, and how structures and spaces are located in relation to each other and to adjacent off-site uses. The following guidelines must be integrated in the site design of all new projects. Alternatives will be permitted only if the intent of the design guideline is met as determined by the Director.

7.2.1 BUILDING PLACEMENT AND ORIENTATION

Building placement and orientation have a significant impact on the way people experience a site. Visually interesting buildings that are oriented to the street shape the area's character as well as the visitor's experience. Locating parking behind buildings, placing buildings closer to the public street edge, and placing a majority of active ground floor uses on the same frontage all contribute to making the public street frontage more inviting to pedestrians.

- » Buildings must have a strong presence and encourage activity along the street frontage.
- » Buildings must be oriented to the adjacent public street or internal street if the site contains multiple buildings.
- » When adjacent uses can mutually benefit from connection, appropriate linkages (e.g., common landscape areas, building orientation, pedestrian paseos and unfenced property lines) are recommended. Shared drives are highly encouraged.
- » Buildings may be set back from the street and alcoves provided to include small plazas, entry nooks, and outdoor café seating.
- » When commercial development is adjacent to residential uses, commercial access must not front onto residential uses.

7.2.2 OUTDOOR OPEN SPACE

Outdoor places, or people places, are spaces that extend the indoor environment to the outdoors. They are essential components of placemaking – contributing significantly to the character and activity levels of a place. These places are created through the clustering of buildings and landscapes to define outdoor space, plazas, or recreation areas, and can accommodate an array of functions including social gathering, working, dining, leisure, and recreation.



Buildings are oriented toward the street.



Open spaces provide the public with places for activity.



Pedestrian pathway.



Landscaping elements.

- » Common open spaces must be designed with consideration for solar and shade orientation, inclement weather, public access, safety and security, ease of maintenance, usability, and aesthetic quality to the extent possible.
- » Open space areas cannot have parking, driveway, or right-of-way encroachments.
- » Common and/or public open space must be accessible by pedestrian pathway from the interior of the building and along main public walkways.
- » Non-residential open space requirements may be satisfied by outdoor dining areas, plazas, or other usable outdoor use, as approved by the Director
- » For residential uses, private useable open space must be contiguous to the residential units served and screened for privacy.
- » Larger projects must contribute to and connect with a comprehensive network of integrated open spaces throughout the Specific Plan area.
- » Open spaces that incorporate amenities to facilitate outdoor activities, including seating, are encouraged.
- » Ground surfaces in open spaces must maximize permeable surfaces. The use of permeable paving is encouraged for the reduction of stormwater runoff and absorption of rainwater to the water table. Where it is not possible to provide significant permeable areas on site, collection, storage and re-use of stormwater is encouraged.

7.2.3 LANDSCAPING

Landscaping creates an environment that is comfortable, attractive, and complements and enhances the community as a whole while providing a unique sense of place at the Bellflower Station and along the Flower Street corridor. The following landscape design guidelines are provided to ensure quality and excellence that will add value to surrounding community and to create a sustainable environment through responsible design practices.

- » Landscaping must be an integral component of the overall project design. Areas not utilized by structures, storage, paved walks, plazas, driveways or parking must be landscaped.
- » Landscaping must be maintained in a weed free condition with a combination of trees, shrubs and ground cover.
- » Landscaping should enhance the quality of developments by framing and softening the appearance of buildings,

screening undesirable views, buffering incompatible uses and providing shade.

- » Landscape designs must emphasize water-efficient or drought tolerant plants.
- » Vines and climbing plants on buildings, trellises, perimeter walls, and fences are encouraged, both to create an attractive appearance and to minimize graffiti.
- » Landscaping along public streets must be consistent, formalized, and composed of signature plantings from the recommended street tree palette, Figure 5.13 (Chapter 5), to create an attractive and cohesive community identity.
- » Within 5 years of planting, trees along public streets must be taller than the first floor of adjacent buildings and achieve a minimum canopy of 15 feet.
- » Shrubs and understory plantings adjacent to the street or in medians must not exceed 18 inches at maturity.
- » Trees and shrub spacing must allow for maintenance access at maturity.
- » Landscaping elements, such as bioswales, that absorb water runoff and provide biotreatment are encouraged where appropriate.



Landscaping adjacent to the public way can provide shade and enhance the pedestrian experience.

7.2.4 SITE ACCESS

Site access configuration effects the safety and experience of pedestrians, bicyclists, and those in vehicles. Well-designed access enhances interest along the public street, improves circulation, and minimizes potential conflicts.

- » Pedestrian access and circulation from transit stops and public sidewalks must be prioritized in the site design.
- » Site access and internal circulation must be arranged in a logical manner that promote safety, efficiency, convenience and minimize conflict between vehicles and pedestrians. Access must be designed to promote safety by providing unobstructed sight lines into the accessway from surrounding public areas and be well-lit.
- » Define site access entries with special paving, lighting and/or landscape treatments.
- » Site plans must balance the provision of adequate vehicular access and the desire to eliminate unnecessary driveway entrances and provide shared access with other properties. The use of common or shared driveways between adjacent uses is strongly encouraged.
- » Driveways must be sited to avoid interfering with traffic flow along adjacent streets and located as far from intersections as possible.



Consolidated driveways and pedestrian pathways reduce conflicts.

- » Passenger pickup and drop-off areas should not interfere with on-site and off-site vehicular and pedestrian circulation.
- » Pedestrian access to commercial and office sites must be provided from adjacent residential neighborhoods when appropriate.



Photovoltaic cells provide shade and electricity.



Landscaping separates walkways and provides shade.

7.2.5 PARKING

Parking configuration, placement, and access is essential to the function and vitality of a neighborhood. Visibility of parking areas has a significant impact on the character and vitality of street life. Parking areas can also provide significant opportunities to reduce environmental impacts with proper placement and landscape treatment.

- » Large surface parking areas, where cars would dominate views, must be avoided. Parking areas in front of the building is discouraged. Parking must be located at the rear and/or side of the property when feasible. Large surface parking lots must be divided into a series of small parking areas with convenient access that relates to adjacent buildings.
- » Except for driveways and/or entrances required for access to parking, no parking or loading can be visible from a public street. Where visibility from a public street is unavoidable, landscaping techniques must be used to provide screening.
- » A safe pedestrian path must be provided from all parking areas to the main entrances of building(s). Unobstructed visibility and clear delineations between pedestrian paths and vehicular travel aisles must be provided. The use of landscaping, walkways, and decorative hardscape to delineate pedestrian circulation is encouraged.

- » For mixed-use projects, parking facilities for residential uses and commercial uses should be separated.
- » Developments are encouraged to provide bicycle storage facilities such as decorative racks or lockers. Bicycle storage such as racks or lockers should be provided and be easily accessible from the street and the pedestrian routes. Bicycle storage should be easily accessible from the street, transit stops and pedestrian routes.
- » The use of photovoltaic cells to cover parking areas is encouraged.

Parking Lot and Structure Landscaping

- » Landscaping must be used to separate parking from buildings and to reduce the visual impact of paved surfaces. Landscaping at the base of buildings is encouraged to soften the transition between building and parking lot.
- » Landscaping must be used within large paved areas to reduce heat island effect. Shade trees and shade structures should be provided in parking lots to reduce the amount of heat absorbed by paved parking surfaces. Trees should achieve a shade canopy within 10 years of planting.
- » The landscaping and design of parking lots and structures must incorporate sustainable design features where feasible. Such features include, but are not limited to, permeable paving, bioswales, and native landscaping to prevent water runoff, reduce solar heat gain, and minimize the need for extensive maintenance.

Parking Structures

Parking structure design must be compatible with the architectural design of adjacent buildings and consistent with the overall design of a project.

- » The integration of retail and other uses is encouraged along the street frontage of the parking structure.
- » Parking structures should be designed to provide natural surveillance into the structure, elevators, and stairs from adjacent activity, public areas, and from within the parking structure. The location of building elements and exterior treatments should be designed to ensure daytime and nighttime visibility.

7.2.6 SERVICE AND LOADING

- » Loading, unloading, and service areas must be located and screened to minimize direct exposure to public view and reduce visual impacts.
- » Delivery truck unloading zones must not obstruct vehicular circulation and parking areas. Loading and service areas



The parking structure facade is coordinated with adjacent buildings.



Retail space can be integrated along the street of a parking structure.

must be separate from the primary public access where feasible.

- » Loading area location and design should prevent truck back-up maneuvers from or onto public rights-of-way or parking aisles.
- » Commercial loading areas must be placed as far as possible from residential units and be completely screened from adjacent residential development.



Vine growth on utility screen.



Trash and utility enclosures.

7.2.7 SCREENING: FENCES, WALLS, AND GATES

- » Walls and fences must be architecturally designed and consistent with the overall character of the site.
- » Walls and fences must be kept as low as possible while performing their intended function. Walls must be kept to a minimum height to avoid a “fortress” appearance.
- » Fences and walls must have an articulated design. Articulation can be created by having regularly spaced posts, changes in height, and by using different building materials at the base, posts, or the cap of the fence/wall.
- » The incorporation of vines on walls and fences is encouraged to reduce the heat island effect and help reduce graffiti.

7.2.8 UTILITY, STORAGE, TRASH, AND RECYCLING FACILITIES

- » Service areas, trash enclosures, loading facilities, mechanical and other equipment must be located and screened so that they are not visible from a public street, open space, or from the upper stories of adjacent structures. Screening devices must be compatible with the architecture, materials, and color of adjacent buildings to the extent possible.
- » Outdoor trash and storage areas must be covered to reduce pollution introduction.
- » Refuse storage areas must be conveniently located for building users.
- » All rooftop mechanical systems must be enclosed to mitigate visual and noise pollutions. Rooftop equipment on flat roofs must be located and screened in such a way as to not be visible from ground level. Flat roofs must have a minimum 3-foot high decorative parapet to screen views of roof-mounted utilities.
- » Fire sprinkler standpipe assemblies must be located within the interior of the building.

7.3 BUILDING DESIGN (MUTOZ AND MUCOZ)

7.3.1 BUILDING FORM AND MASSING

Building massing is the overall shape and size of a building in terms of scale, bulkiness, and relationship to exterior spaces. Care must be taken to articulate the building to create a welcoming public realm and reduce the appearance of oppressive and monolithic building forms. The site and neighboring conditions are key to developing appropriate massing.

- » Massing design must include appropriate variation in the footprint, setbacks, wall plane projections and recesses, wall height, and roof form.
- » Building height must be compatible with and transition from the heights of adjacent developments.
- » Substantial projects must be designed to appear as several smaller buildings to provide an intimate scale. Variations in roof form, height, and profiles can be used to provide visual interest and reduce the overall mass of the building
- » Architectural elements such as recessed or projecting balconies, porches or other elements that add visual interest, human scale and character to the neighborhood are encouraged. Upper floors should have varied setbacks to accommodate balconies, seating and other architectural treatments.
- » Massing must consider passive solar and wind orientation to maximize natural daylighting and ventilation.












Changes in massing and form help reduce the visual impact of buildings.

7.3.2 FRONTAGES

Building frontages—the portion of the building facing the primary street have been categorized by type in this Specific Plan to address the design elements that are appropriate for a building’s ground floor front facade. The street frontage environment includes any yard or setback area that extends to the street. It is an important element of urban design because it informs how a building engages the public realm and largely determines the character of the area and the quality of the pedestrian environment. At the ground floor, the frontage is typically the primary access point into a building and consequently visually reinforces a building’s “human scale.” The frontage types described and depicted on the following pages are appropriate for the MUCOZ and MUTOZ Overlay Zones as shown in Figure 7.1.

FIGURE 7.1 Permitted Frontage by Overlay Zone

FRONTAGE TYPES	MUCOZ	MUTOZ
A. Shopfront		
B. Forecourt		
C. Arcade		
D. Terrace		
E. Stoop		
F. Porch		



Indicates permitted frontage type per Overlay Zone. This table is comprehensive of all permitted frontage types.

A. SHOPFRONT FRONTAGE

Description

A shopfront is a frontage where the building facade and entrance are at sidewalk grade and close to the pedestrian zone. Shopfronts include large areas of transparent openings and doors and are commonly equipped with cantilevered roofs or awnings. Shopfronts typically provide access directly from sidewalks and are oriented to display ground-level commercial uses.

This frontage type is typically used for commercial use. This frontage type can be used in conjunction with terrace, and or forecourt to create a more engaging street.



Illustrated Elevation



Examples of Shopfront Frontage (above) Sample illustration (below)

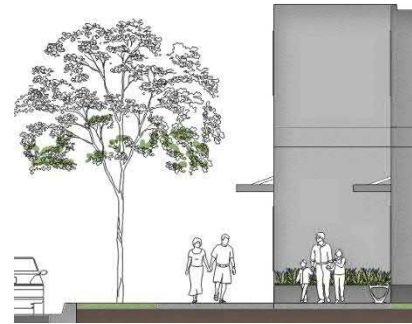


B. FORECOURT FRONTAGE

Description

A forecourt is a frontage where a portion of the building facade is recessed from the primary building facade. The forecourt may be used as an entry court and open space for residential uses, or as additional shopping or seating areas for commercial uses. Forecourts with large trees and lush landscaping offer visual and environmental variety to the urban streetscape.

This frontage type is appropriate for either residential and/or commercial uses. A combination of both uses can be achieved by using the forecourt as a residential entrance while commercial uses occupy street adjacent building space. This type can be used in conjunction with shopfronts and stoops as a transition into residential frontage.



Illustrated Elevation



Examples of Forecourt Frontage (above) Sample illustration (below)

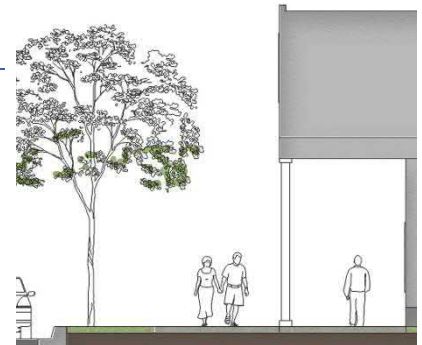


C. ARCADE FRONTAGE

Description

An arcade is a frontage where the building facade is aligned close to the pedestrian zone with a colonnade recessed into the building. The colonnade supports habitable commercial or residential space above. The colonnade covering the sidewalk provides pedestrian shelter and opportunities for covered outdoor dining.

This frontage type is appropriate for the Bellflower Station area.



Illustrated Elevation



Examples of Arcade Frontage (above) Sample illustration (below)



D. TERRACE FRONTAGE

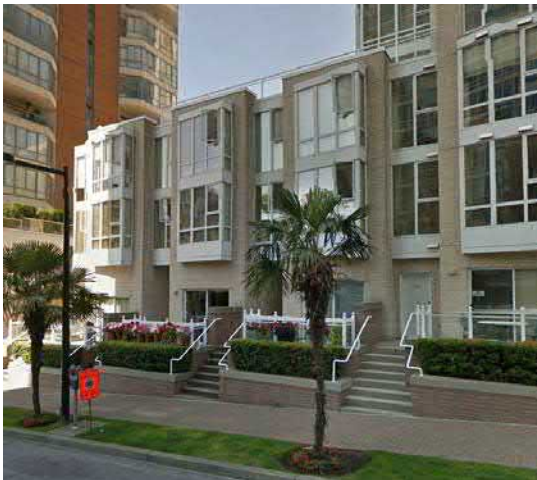
Description

A terrace is a frontage where the building facade is set back from the street, paseo, or open space, by an elevated open area that is paved or planted. This frontage type can effectively buffer building uses from the sidewalk.

This type is recommended for residential and commercial use as it allows for semi-private use of frontage areas.



Illustrated Elevation



Examples of Terrace Frontage (above) Sample illustration (below)

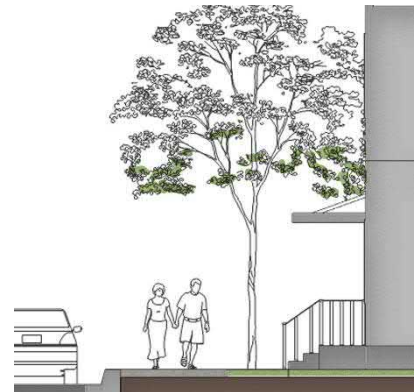


E. STOOP FRONTAGE

Description

A stoop is a frontage where the building facade is separated from the street, paseo, or open space, by an entrance to the elevated ground floor of the building. The entrance is usually an exterior stair and landing and may be covered.

This frontage type is recommended for ground-floor residential uses to facilitate a transition and to provide separation between an active public street and a private residence.



Illustrated Elevation



Examples of Stoop Frontage (above) Sample illustration (below)



F. PORCH FRONTAGE

Description

Porch frontage is common where the facade is set back from the right of way and a porch is appended to the facade, encroaching into the front yard. This frontage type is primarily associated with single family houses, but can be used with other building types depending on the context and design. Porches typically have a fence defining the area of the porch as well as an overhang or overhead coverage.

A fence or wall at the property line should not be used to define the private space when porches are provided.



Illustrated Elevation



Examples of Porch Frontage (above) Sample illustration (below)



7.3.3 FACADES AND ARTICULATION

Facades are the external building face(s) that are visible from a public street or open space; The design of facades often sets the tone for the rest of the building and the neighborhood. The design, rhythm, and composition of facades, achieved through the arrangement of surface articulation, architectural elements, and materials, provides visual cues that support navigability and mobility for pedestrians, thereby enhancing the safety, convenience, and comfort of the public realm.

- » Building facades must be well designed and detailed in a way that makes them appear human in scale and reduces visual mass. Design elements may include varying materials, voids of open space, changes in plane, and vertical elements, such as pilasters. Architectural elements and accents must be appropriate for the overall building design and form an integral part of the building.
- » Buildings must have a clear base, middle, and top by providing horizontal differentiating treatment. The base must visibly anchor the building to the ground and the eave or parapet must provide a visual termination of the building.
- » Design features must be consistent on all elevations of a structure. Buildings must incorporate 360-degree architecture, whereby similar and complementary massing, materials, and details are incorporated into side and rear facades.
- » The highest level of articulation must occur on the front facade and facades visible from publicly accessible areas. Blank walls must be avoided and cannot face onto pedestrian spaces and streets.
- » Facades must incorporate “human” or intimate scale architectural details and materials, such as arches, trellises, awnings, signage, recessed entries, or transparent storefronts on lower walls.



Well designed facade articulation in combination with the use of a variety of quality materials and colors creates interest and helps define a neighborhood.



Materials and colors emphasize entrances and openings, corners, and building design.

- » The use of awnings, canopies, and overhangs that fit entrance or window openings are encouraged to provide visual appeal and protection from the weather.
- » Surface detailing cannot serve as a substitute for distinctive massing.

7.3.4 COLORS AND MATERIALS

Appropriately applied color and material choices create interest and support a sense of place in a neighborhood when combined with facade modulation. Materials must comply with section 7.6.4.G2.

- » All facades and elements of the primary structure must be unified by a complimentary and consistent use of building materials, textures, and colors.
- » Facade treatment materials and architectural elements must be of a good quality, durable, and weather resistant to create buildings that are long lasting and can be adaptively reused over time.
- » Color and material applications on a facade must be varied to create visual articulation and accentuate architectural details compatible with the architectural character of the building.
- » Materials and colors must be used to imply form changes, particularly for entrance lobbies, massing changes, and different uses or tenants.
- » Construction details must be authentic and applied with consistency.
- » Recommended primary materials include glass, stucco, insulated concrete forms, stone, and brick masonry. The use of natural and local materials is encouraged. Metal awnings are not allowed.
- » Materials and technologies that minimize environmental impacts, reduce energy and resource are highly encouraged.
- » Gutters, scuppers and downspouts must be integrated into the exterior design and not "tacked on." These items must be finished to blend in with the background material or be integrated into the architectural design.

7.3.5 BUILDING ENTRANCES

Well positioned and identified entrances orient the building, create a welcoming public realm, and frame the character of the streetscape.

- » Primary building entrances must be located along major corridors (Bellflower Boulevard and Flower Street).
- » All building entrances must adhere to the requirements of the Americans with Disabilities Act (ADA) and have direct at-grade access at ground level.
- » Primary and secondary building entries, including courtyard doors or gates used as building entries, must be attractively designed as an important architectural feature and may include the use of architectural features, landscaping, and lighting.
- » Rear entrances must not be more prominent than the front, primary entrance. Public rear entrances must be visible and easily located.
- » For mixed-use buildings, separate entrances and pedestrian accessible pathways must be provided for residential and non-residential uses.



Separate entrances for retail, residential, and parking should be accessible from the sidewalk.



7.3.6 CORNER DESIGN

Intersections and the corners that frame them present the opportunity to create identity and positive visual impact for the neighborhood.

- » Development occurring at corner locations must anchor the corner. This can be accomplished using a reverse building placement where the main building is placed on the corner or by using a prominent landscaping feature.
- » Buildings at street corners must incorporate design features that highlight the intersection. These features may include architectural elements such as distinctive corner massing, vertical design elements or prominent pedestrian spaces such as plazas.
- » Development occurring near the Bellflower Civic Center Plaza should complement and enhance the Plaza.



Architectural design and massing emphasizes and engages the street corner.

7.3.7 WINDOWS

Windows are a key architectural element for providing rhythm and scale on a facade. Windows provide a building with natural light and fresh air and frame views.

- » The size of doors and windows must relate to the scale and proportions of the overall architectural style.
- » Windows must maximize desirable natural lighting and air circulation.
- » Windows must allow building users to have “eyes on the street” for natural surveillance.
- » For residential developments, windows and doors must be recessed to prevent wall surfaces from being monotonously flat.
- » For commercial uses, ground level facades must have a minimum 50 percent of commercial windows to provide transparency, visual interest, and a safe and active street scene. A section of facade must not exceed 20 linear feet of blank wall.



Commercial and residential windows are oriented toward the public street.

7.3.8 ROOFS

Roof form and design are important to the overall architectural character of the building.

- » Roofs and rooflines must be cohesive in design throughout a building or group of buildings, including accessory buildings. Overly complex and distracting roofs are not encouraged.

- » Roof massing, materials, and design must be consistent with the desired architectural style and complimentary to adjacent structures.
- » The use of light-colored roofing materials or green roofs to reflect heat and reduce cooling in buildings is encouraged.
- » The use of solar panels on roofs is encouraged.

7.3.9 ARCHITECTURAL LIGHTING

- » Lighting must be designed as part of the overall architectural style enhance the building's form, colors, and materials.
- » Lighting must minimize contribution to light pollution. The lighting of full facades or roofs, lights aimed directly at the sky, lights projecting off site, and exposed bulbs are discouraged.
- » The latest technical and operational energy conservation concepts must be considered in lighting designs.
- » Primary and secondary building entrances, vehicle entrances, driveways, parking and service areas, pedestrian entrances, walkways, and activity areas must be properly lighted to maintain a safe environment.

7.3.10 GREEN BUILDING AND SUSTAINABLE DESIGN

- » Building and site design must strive to integrate sustainable practices that conserve energy and water resources, reduce waste, and reduce the effects of urban heat gain.
- » Buildings must integrate passive design strategies including natural daylighting and ventilation.
- » Use of Leadership in Energy and Environmental Design (LEED) building components and achieving LEED certification is encouraged.



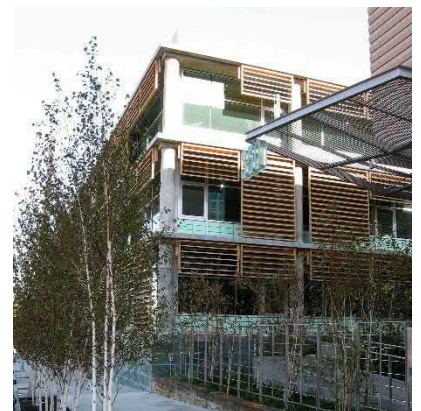
Human-scaled pedestrian lighting enhances public activity areas.



Lighting should complement a building's architectural design.



Green roofs help manage water run-off and reduce heat gain.



Architectural design that integrates passive design and sustainable materials is encouraged.



Well designed and selected streetscape elements contribute to the public realm.

7.4 PUBLIC REALM DESIGN (MUTOZ AND MUCOZ)

7.4.1 STREETSCAPE DESIGN

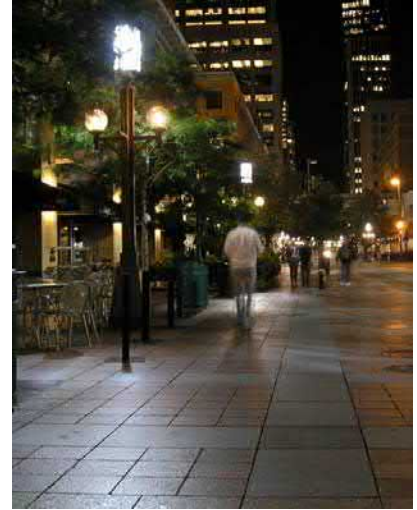
Along with landscaping, well designed and selected streetscape elements, such as furniture and public art, contribute to the public realm by delineating paths of travel, providing places to rest and shade, and defining the character of an area by providing visual and structural continuity.

- » Streetscape design elements, including street furniture and public art, must be selected to provide a cohesive environment and identifiable character.
- » Color and material selection that enhances a unified street furniture “look” is encouraged.
- » The selection of street furniture products, including seating, planters, bollards, trash receptacles and newspaper racks, must be durable, decorative and able to withstand inclement weather.
- » Street furnishings and amenities must not obstruct pedestrian or bicycle circulation. Street furnishings cannot impede the path of travel required to comply with the Americans with Disabilities Act (ADA). Street furniture cannot impede entry to the building or preclude applicable emergency exit requirements.
- » Permanently affixed streetscape amenities must minimize impacts on water runoff.
- » The street tree palette is in Section 5.6, *Street Trees*.
- » Entry and monument guidelines are in Section 5.7, *Entries and Monumentation*.
- » Tree grates must occur along street edges and plazas where a continuous walking surface is needed. Grate sizes must be a minimum of four feet in diameter. Knockouts must be provided to enlarge the inside diameter for supporting a larger tree trunk as the tree grows.
- » Tree guards must extend vertically from tree grates and serve to protect trees in highly active areas. Tree guards must be narrow and painted in a similar color and relate to other site furnishings.

7.4.2 LIGHTING

- » Exterior lighting must be designed with consideration of day and nighttime activities in the area. Lighting must create a nighttime ambiance that complements the adjacent buildings and public realm and promotes a sense of safety.

- » Pedestrian-scaled lighting for sidewalk and street illumination is encouraged. Lighting must enhance the pedestrian experience at night.
- » Lighting must be provided at regular intervals to prevent the creation of light and dark pockets.
- » Light fixtures and structural supports must be architecturally compatible with the adjacent building(s).
- » The latest technical and operational energy conservation concepts must be considered in lighting designs.
- » Lighting cannot be animated.
- » Overhead service wires or exposed conduit must not be used.



7.4.3 SIGNAGE

All development within the MUTOZ and MUCOZ must have a “sign program” as part of the project design. The sign program will identify the location for all signage that may be located on the building, the allowable sign materials, lighting methods and sign design. Temporary signs and banners will be included in the sign program.

- » At no time, can the sign program allow for signage that exceeds the standards within the Town Center District.
- » “Box” signs, “canister” or “can” signs are prohibited.
- » Pedestrian-oriented, non-illuminated hanging “blade” signs that either hang under a colonnade or canopy, or project from the facade of a building, will not be counted within the sign area formula that is used to calculate the maximum amount of sign area for each building.
- » Pedestrian-oriented, non-illuminated hanging or projecting blade signs exceeding two square feet maximum per sign face, or two square feet for a double-faced blade sign, are prohibited.
- » Franchise identifying features may only be located on the main structure.



Lighting fixtures should create visual interest and enhance the streetscape.



Hanging blade signs.



7.5 ADDITIONAL DESIGN GUIDELINES FOR MIXED-USE TRANSIT OVERLAY (MUTOZ)

The following Mixed-Use Transit Overlay Zone Design Guidelines provide specific guidance for the parcels within the MUTOZ in addition to design guidelines provided in sections 7.1 through 7.4 of this specific plan. This area requires greater attention to detail due to its prominent location and its function as a multi-modal hub for Bellflower. Special attention to the treatment of buildings and the public realm can enhance the character of the Bellflower Station area and establish a unique sense of identity.

- » Buildings must be oriented to emphasize the station area as a node of activity.
- » Special architectural elements must be incorporated on buildings, such as articulated entrances, a tall first story, or prominent roof forms.
- » Focal points should be developed to create a defined sense of identification. Plazas, fountains, artwork, site furniture and lighting, textured and decorative pavement, universally-accessible changes in levels and vertical building features may be combined to create focal points and identity.
- » Each project proposed within the MUTOZ must include a public open space amenity, or some form of physical interface for the pedestrian that integrates hard and soft surfaces. Such features may include, but not be limited to:

Formal Plazas. A formal plaza would be a publicly accessible open space which has a design that is influenced by classical urban planning design. A formal plaza would typically include landscaping combined with seating areas, art or water features, kiosks and other outdoor features.

Urban Gardens. An urban garden can be located on the ground level, or on upper levels of a structure. Urban gardens include ornamental landscaping arranged in raised or at-grade planters or planting areas, potted plants and trees. Many times, there are sculptures or other forms of public art that are included within the urban garden.

Covered Colonnades. Colonnades are linear in design and generous in depth. The intent is to provide a comfortably wide, covered pathway that is adjacent to the openings of a building. Sometimes the second floor of a building is utilized to create the “covered” element of the colonnade.

Sidewalk Dining. Sidewalk dining may occur wherever a sidewalk space is ample enough to accommodate dining furniture without impeding pedestrian access of the sidewalk. Sidewalk dining may be defined with a railing or planters or be open and accessible.

Pedestrian Alleys and Walkways. A pedestrian alley or walkway is typically a “lane” that does not follow the alignment of a vehicular street, but provides a pedestrian access to either a public space or some other feature within the interior of a development. Pedestrian alleys or walkways must be designed in such a manner so as to be inviting to pedestrians. Issues such as lighting, security, line of sight, cleanliness and visual appeal are important considerations to a well-designed pedestrian alley or walkway. Sometimes public art, street furniture and access to shops and public spaces are features of pedestrian alleys and walkways.

Direct Public Transit Connection. A direct connection to public transportation facilities, or amenities that service public transit facilities such as benches, informational kiosks, walkways, parking facilities or other similar amenities.



7.6 DESIGN GUIDELINES FOR THE BAMU

7.6.1 PURPOSE AND INTENT

The Bellflower/Alondra Mixed-Use (BAMU) Overlay Zone Design Guidelines provide guidance on a wide variety of design and aesthetic components that contribute to achieving the overall vision of the BAMU Overlay Zone. These guidelines provide direction for new development as well as redevelopment, remodeling and rehabilitation of existing buildings. The Design Guidelines encourage creativity and innovative design while working to ensure development with consistent and cohesive imagery that contributes to a strong sense of place.

7.6.2 USER'S GUIDE

The Design Guidelines supplement section 4.7, BAMU Overlay Zone. The Design Guidelines provide guidance on a variety of design elements and are intended to further define the desired character and imagery within the BAMU Overlay Zone.

To effectively utilize the Design Guidelines, and gain optimum understanding of the document and its purpose, the following basic process is recommended:

- A. Review the common design principles below for an understanding of the general design priorities for the area.
- B. Review the General Area-Wide Design Guidelines for guidance applicable to all new development, redevelopment and remodeling within the BAMU Overlay Zone.
- C. Review the design guidelines section(s) applicable to your specific land use or development type for guidance on a range of items that will help your project contribute to the overall design of the BAMU Overlay Zone.
- D. Schedule a meeting with the Director to review your ideas, identify processes and requirements, ask questions, and discuss potential issues, solutions, and approaches.
- E. If you have any additional questions, contact the Director.

7.6.3 COMMON DESIGN PRINCIPLES

This section provides the basic framework for project design and the goals that each project is expected to achieve. New construction projects typically have greater opportunities to address the design principles more completely, while projects that involve renovations or additions to existing buildings are typically more limited in their ability to address each specific principle. For renovations or additions to existing buildings, the Design Guidelines should be followed to the greatest extent feasible based on the area and magnitude of work being conducted. For example, a facade rehabilitation project should at minimum follow guidance on architectural imagery, detailing, colors and materials. For a facade rehabilitation project, the limited area of the project may not allow for provision of new gathering spaces or other site amenities. Conversely, a project consisting of a full demolition and reconfiguration of buildings on a site should incorporate amenities, landscaping, etc. as the scope of work allows the project to integrate such items into the design.

The following common design principles contribute to encouraging compatible relationships between existing uses, proposed new uses and future development, all in an effort to help ensure that projects in the BAMU area are well integrated and demonstrate sensitivity to the contextual influences of the area especially in terms of building scale and architectural design. It is expected that all projects will implement the principles outlined below to the greatest extent feasible. The information in this design guidelines manual will help in achieving this goal.

A. **Design Principle 1: Sustained High Quality Development.**

The BAMU area is anticipated to redevelop over an extended period of time. The development within the BAMU will also have a positive impact on the surrounding neighborhoods. Development and improvements are cost-intensive and should not be thought of as temporary. Building materials and landscaping should be chosen carefully for their ability to be maintained in a cost-effective manner at the same high quality as when they were originally installed. Planning and design of buildings should consider being able to adapt to future conditions and trends.



Awnings, pavement texture and storefront windows contribute human-scaled character.



Distinctive signage and public gathering spaces contribute to "sense of place"



Pedestrian paseos create rhythm along corridor and contribute to development character



Prominent corner feature anchors building

- B. **Design Principle 2: Human-Scaled Development.** The Bellflower and Alondra corridors have historically been oriented to the automobile. It is people, not cars, upon which the BAMU area depends for its success. Development along the corridors should be inviting and scaled to its users, while acknowledging the roles of access and mobility through public transit and automobile transportation. The extension of a human-scale character throughout the corridor is a basic design principle that should be addressed by all projects. Achievement of this principle can include human-scale details, pedestrian amenities and strong relationships between the buildings, sidewalks and other outdoor spaces. See photo.
- C. **Design Principle 3: Sense of Place.** New development and redevelopment along the Bellflower and Alondra corridors should encourage people to stop and utilize the spaces as opposed to just pass through. A large component of creating an inviting environment is promoting a sense of place by creating a defined "here." Public gathering spaces, cohesive architectural imagery, landscaping and signage can all contribute to defining a place. See photos.
- D. **Design Principle 4: Development Character.** Development character is achieved through the thoughtful provision and arrangement of front and side building setbacks, location of structures, plazas, courtyards, outdoor dining areas, pedestrian paseos and linkages, view corridors, landscaped areas and other details that when combined create a positive rhythm along the commercial corridors. The structures, spaces, and landscaping between them create a physical environment rhythm along Bellflower Boulevard and Alondra Boulevard. New development and redevelopment should promote a consistent and coherent rhythm of structures and open spaces along the street edge. See photo.

7.6.4 GENERAL AREA-WIDE DESIGN GUIDELINES

This section provides design guidelines applicable to all development, redevelopment and remodeling within the BAMU Overlay Zone, including commercial.

- A. **General Design.** The design of development in the BAMU Overlay Zone should follow the following general design guidelines:
1. Provide site access, parking and circulation that are arranged in a logical and safe manner.

2. Developments should integrate and encourage use of alternative modes of transportation, such as bicycles and buses, by providing safe, convenient and attractive facilities.
3. Buildings at street corners should incorporate design features which highlight the intersection. These features may include architectural elements such as distinctive corner massing and vertical design elements or prominent pedestrian spaces such as plazas. See photo.
4. Building location and massing should be consistent throughout the corridor to frame and define public space.
5. In larger shopping centers/commercial sites, focal points should be developed to create a defined sense of identification. Plazas, fountains, artwork, textured pavement, universally-accessible changes in pavement levels and vertical building features may be combined to create focal points and identity. See photo.
6. Site plans should balance provision of adequate vehicular access and desire to eliminate unnecessary driveway entrances and provide access points that are coordinated with other properties.
7. Buildings should be sited and designed so property boundaries are not emphasized. Care should be taken when addressing the interface between two adjacent properties.
8. Buildings should be oriented to create and define usable outdoor spaces.
9. Ports of buildings may be set back from the street and alcoves may be provided to include small plazas, entry nooks, and outdoor café seating.
10. Site furniture and light fixtures should reflect the same design concepts as the public right-of-way street furniture.



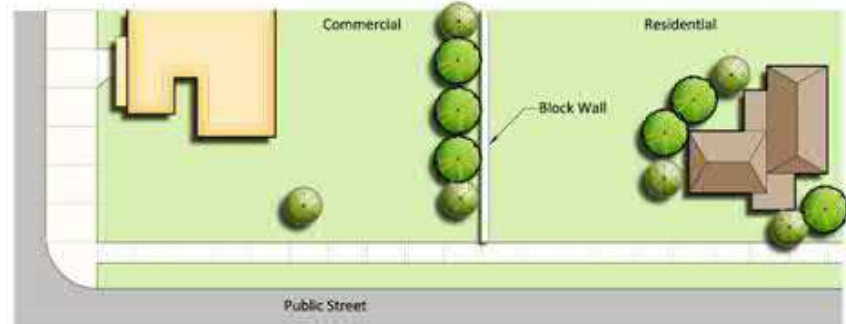
Fountain provides focal point

B. Compatibility.

1. Consider the area's scale and character and demonstrate sensitivity to the influences of the surrounding area, especially the existing residential neighborhoods.
2. The siting or placement of buildings should recognize the particular characteristics of the site and should relate to the surrounding built environment in pattern, function, scale and character.

- Commercial and residential uses should be buffered from each other as much as possible. Building orientation, landscaping and increased setbacks should be used to provide adequate separation between incompatible uses. See Figure 7.2.

FIGURE 7.2 Buffers



Block wall and landscaping provide buffer between commercial and residential uses

- When adjacent uses can mutually benefit from connection, appropriate linkages (e.g., common landscape areas, building orientation, pedestrian paseos and unfenced property lines) are recommended.



Changes in wall plane and roofs used to break up massing

C. Continuity.

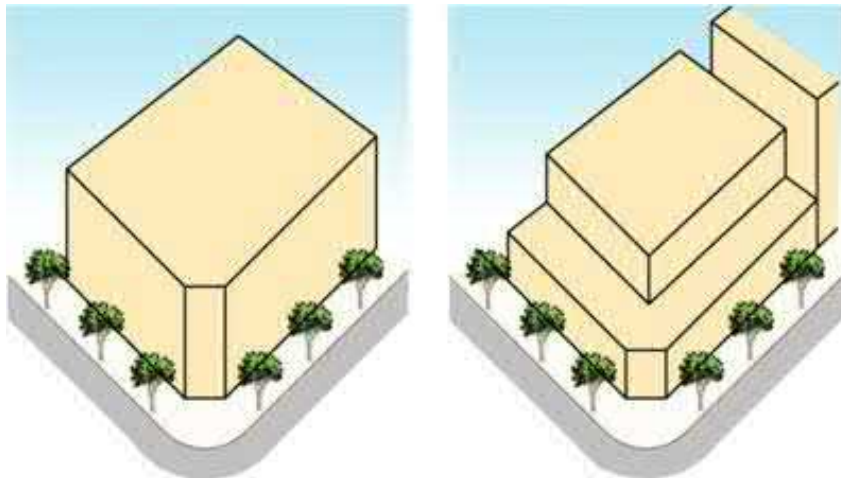
- Continuity among buildings contributes to community identity, levels of pedestrian activity and economic vitality.
- Infill buildings that are much wider than existing facades should be divided down into a series of appropriately proportioned structural bays or components.
- Key facade elements on existing buildings (i.e., wall planes and modulation, window and door arrangements, entries, transoms, awnings and cornice treatments) should be repeated in the design of a new neighboring building.

- Mass.** In this Specific Plan, mass is considered a three-dimensional form such as a cube, box, cylinder or pyramid. The way the forms are sized directly relates to the way building elements are emphasized or de-emphasized. Voids of open space in the forms can change their appearance and make the building more interesting and less imposing.

- Each building should have a recognizable base, body, roof lines and entry.

2. Several smaller buildings instead of one large building should be used to provide an intimate scale.
3. Varying materials between base and body of a building can break long wall planes.
4. Changes in vertical planes break up a boxlike appearance. Vertical elements such as pilasters help create bays to give the appearance of several smaller buildings.
5. Surface detailing should not serve as a substitute for distinctive massing.
6. Massing designs may include:
 - a. Variation in the wall plane (projections and recesses);
 - b. Variation in the wall height;
 - c. Roofs located at different levels. See photos.
7. Upper floors have varied setbacks to accommodate balconies, seating and other architectural treatments. See Figure 7.3.
8. Recessed entries and articulation in the storefront mass should be included.
9. Tall dominating structures should be broken up by creating horizontal emphasis through the use of trim, awnings, eaves or other ornamentation and by using a combination of complimentary colors.

FIGURE 7.3 **Step Back Upper Floors**



UNDESIRABLE BUILDING MASS



DESIRABLE BUILDING MASS



Storefronts with awnings face pedestrian area, contributing to human-scaled development

E. **Scale.** Scale is the proportion of one object to another. “Human” or intimate scale incorporates buildings and architectural details and materials on lower walls that relate to human scale such as arches, trellises or awnings should be utilized.

1. Articulated storefronts with carefully arranged doors, windows, arches, trellises or awnings, rather than blank walls, should face onto pedestrian spaces and streets. See photo.
2. Projections and recesses are encouraged to add texture and differentiate between buildings.

F. **Rhythm.** Rhythm describes the relationship of the building components as well as the relationship of individual buildings to one another.

1. Rhythm should be more complex than simply the repetition of architectural details.
2. Rhythm may be expressed by changing materials and colors and also expressed by varying elements.

G. **Architectural Design Elements.**

1. Architectural Imagery.
 - a. To reduce mass and bulk, facades should be “broken” by vertical and horizontal variations in wall and roof planes, building projections, projecting ribs, reveals, balconies, doors and window bays, and similar design elements. See photo.
 - b. Articulate building forms and elevations to create interesting roof lines, building shapes, and patterns of shade and shadow while maintaining compatibility with surrounding buildings.
 - c. Design features must be consistent on all elevations of a structure. Side and rear elevations visible from public considerations as the primary public elevation.
 - d. Roof materials and colors are important aspects of the overall building design. These elements should be consistent with the desired architectural style and complimentary to adjacent structures.
 - e. Parapets should have sufficient articulation of detail such as precast treatments, continuous banding or variety in pitch (sculpted). See photo.
 - f. Rooftop equipment on flat roofs should be screened and not visible from ground level. Buildings with flat



Balconies, material changes, and changes in wall planes along the facade help reduce building mass



Parapets with precast trim are continuous along all facades

or low-pitched roofs should incorporate parapets, pitched facades or architectural elements designed to screen roof-mounted mechanical equipment and be architecturally compatible with the design of the building facade.

2. Materials and Colors.

- a. Limit the number of building materials used on a facade. As a general rule, use no more than two different materials (excluding glass windows) on a single facade. See photo.
- b. Appropriate building materials and finishes include:
 - i. Stucco with a lightly textured finish such as smooth or sand;
 - ii. Stone veneer;
 - iii. Wood/wood composite siding.
- c. Appropriate accent or detail materials include:
 - i. Wrought iron;
 - ii. Steel;
 - iii. Tiles.
- d. Inappropriate building materials that should be discouraged on facades include:
 - i. Plywood;
 - ii. Hardboard;
 - iii. Unfinished lumber;
 - iv. Aluminum siding;
 - v. Corrugated sheet metal or tin siding;
 - vi. Textured T-111 siding.
- e. Materials and finishes should be durable and weather resistant.
- f. Material choice and location should minimize reflective surfaces or glare.
- g. Changes in material should generally occur when there is a change in the plane of the facade. If possible, the change in material should occur on inside corners of the building. If a change is proposed along the line of a single plane, a pronounced expansion joint or trim piece should be used to define a clear separation. See Figure 7.4.



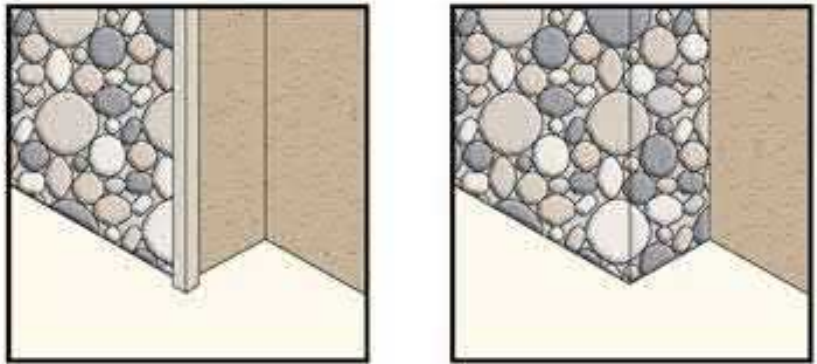
Building materials are limited to stucco and brick veneer



Scuppers are integrated into the design of the building

FIGURE 7.4 Material Treatment at Corners

Change in materials should occur at inside corners



INAPPROPRIATE



APPROPRIATE

- h. Gutters, scuppers and downspouts should be integrated into the exterior design and not “tacked on.” These items should be finished to blend in with the background material or be integrated into the architectural design. See photo.
 - i. Color applications on a facade should generally be limited to one (1) or two (2) main colors and one (1) to two (2) accent colors that complement the main color(s) of the structure.
 - j. Painted building surfaces should have a matte finish. Trim work may have a glossy finish.
3. Roofs.
- a. Roofs should be given design consideration and treatment consistent to that of the rest of the building exteriors.
 - b. Roofline elements including parapet walls should be developed along all elevations.
 - c. Roofs and rooflines should be continuous in design throughout a building or group of buildings.

H. Lighting.

- 1. Exterior lighting should be designed as part of the overall architectural style of the building(s) and should highlight interesting architectural features. The lighting of full facades or roofs is discouraged.
- 2. Accent lighting, when provided, should complement the building color and materials.

3. Light fixtures and structural supports should be architecturally compatible with the adjacent building(s).
4. Lighting fixtures with exposed bulbs should not be used.
5. The latest technical and operational energy conservation concepts should be considered in lighting designs.
6. Vehicle entrances, driveways, parking and service areas, pedestrian entrances, walkways, and activity areas should have a sufficient level of lighting to provide security and safety.
7. Lighting should be provided at regular intervals to prevent the creation of light and dark pockets.
8. Pedestrian-scaled lighting for sidewalk and street illumination is encouraged. See photos.
9. Lighting should not be animated.
10. Overhead service wires or exposed conduit should not be used.



Two types of pedestrian-scaled lighting

I. **Landscaping.**

1. Utilize landscaping as an integral component to overall project design.
2. Landscape designs emphasizing water-efficient or drought tolerant plants are encouraged.
3. Vines and climbing plants on buildings, trellises, perimeter walls, and fences are encouraged, both to provide attractive appearance and to minimize graffiti.
4. Trees and shrubs should be selected based on their mature size and root characteristics. Plants with root systems that uplift hardscape materials should be avoided.
5. Trees and shrubs should not be planted so close together that they create maintenance problems at maturity.

J. **Walls and Fences.**

1. Walls and fences should be kept as low as possible while performing their intended function. Walls should be kept to a minimum height to avoid a “fortress” appearance.
2. The design of fences and walls, as well as the materials used, should be consistent with the overall development’s design.
3. Fences and walls should have an articulated design. Articulation can be created by having regularly spaced



Wrought-iron details enhance fence



Use of photovoltaic cells on covered parking and awning

posts, changes in height and by using different building materials at the base, posts or the cap of the fence/wall. See photo.

4. Fences and walls should be painted to match or complement the color of the building. However, walls constructed with stone or brick may be unpainted in order to display the natural color of the materials.
5. Incorporation of vines on retaining and decorative walls to reduce the heat island effect is encouraged.

K. **Green Building and Sustainable Design.**

1. Materials and technologies that minimize environmental impacts, reduce energy and resource are highly encouraged.
2. Use of photovoltaic cells on roof tops, covered parking areas or integrated into building architecture is encouraged. See photos.
3. Use of Leadership in Energy and Environmental Design (LEED) building components or achieving LEED certification is encouraged.
4. The use of pervious surfaces and pervious paving are encouraged.
5. Outdoor trash and storage areas should be covered to reduce pollution introduction.
6. Buildings are encouraged to utilize natural lighting whenever feasible.
7. Sweeping, vacuuming, or high pressure, low volume devices should be used on outdoor areas instead of spraying or hosing with water.
8. Landscaping should be used within large paved areas to reduce heat island effect.

7.6.5 GENERAL COMMERCIAL DESIGN GUIDELINES

This section provides design guidelines and concepts that are applicable to all commercial projects within BAMU Overlay Zone including retail, service, and office uses. It should also be noted that the General Commercial guidelines found below will be utilized for Specific Commercial Use Guidelines subsection.

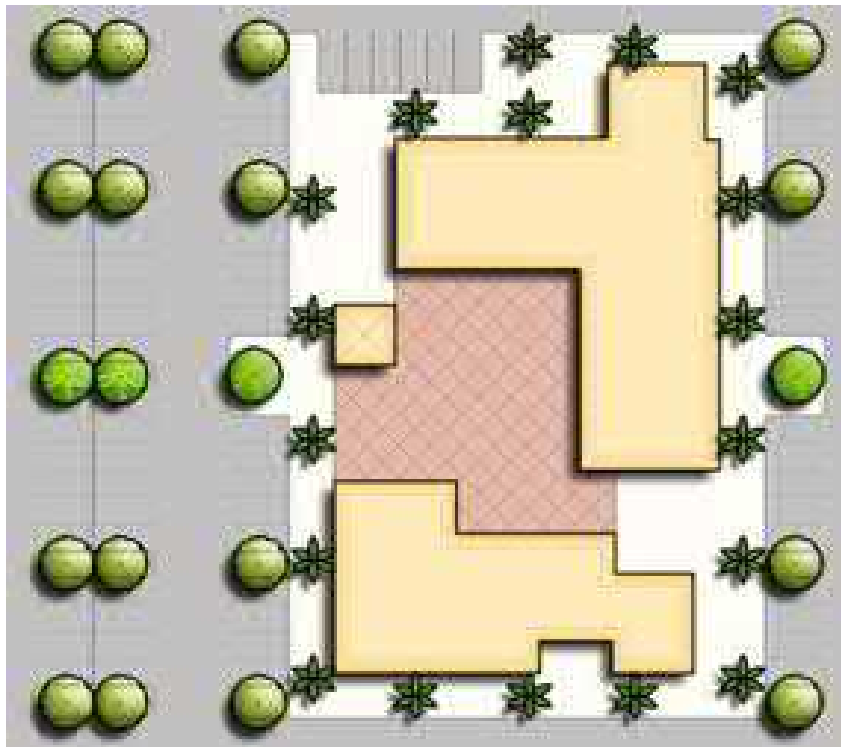
A. Site Planning and Design Details.

1. Building Siting.

- a. Buildings should be oriented to face either a public street, private street or pedestrian outdoor space and provide pedestrian access from the public sidewalk. See Figure 7.5.

FIGURE 7.5 Building Orientation

Buildings are oriented to face a pedestrian outdoor space



- b. Building siting and design should encourage pedestrian activity.
- c. When possible and appropriate, interior spaces are encouraged to extend into the outdoors, both physically and visually. This may be achieved through awnings and overhangs, outdoor dining spaces or other features.



Plaza is adjacent to and visible from the public street

- d. Service areas should not be oriented onto publicly accessible spaces.

2. Pedestrian Activity Areas.

- a. Pedestrian activity areas should be visible and accessible from the public street. See photo.
- b. Pedestrian activity areas should provide site amenities such as seating areas, public art, water features and other appropriate amenities that encourage pedestrian utilization.
- c. Pedestrian activity areas should provide a sufficient level of shade for users. Landscaping, canopies or other methods of providing shaded areas are strongly encouraged.

B. Parking and Circulation Guidelines.

1. Site Access.

- a. Site access and internal circulation should promote safety, efficiency, convenience and minimize conflict between vehicles and pedestrians.
- b. The number of site access points to a parking lot should be minimized and located as far as possible from intersections. Parking lot access points should not interfere with the function of adjacent roadways. See Figure 7.6.
- c. Use of common or shared driveways between adjacent uses is strongly encouraged.

FIGURE 7.6 Buildings with Shared Parking Areas and Driveways



- d. When commercial development is adjacent to residential uses, commercial access should not front onto residential uses.
- e. Loading and service areas should be separate from the primary public access.
- f. Use special paving, lighting and/or landscape treatment to define site entries.

2. Pedestrian Circulation.

- a. Pedestrian access from surrounding residential neighborhoods should be provided. This access should be designed to promote safety by providing unobstructed sight lines into the access way from surrounding public areas and be well-lit.
- b. Safe, convenient pedestrian links should be designed between parking areas and businesses.
- c. Unobstructed visibility and clear delineations between pedestrian paths and vehicular travel aisles should be provided. Use of landscaping, walkways, and decorative hardscape to delineate pedestrian circulation is encouraged.
- d. Access to transit stops should be integrated into the pedestrian circulation network.

C. Architectural Design Elements.

1. **Architectural Imagery.** The use of standardized “corporate” or “franchise” architectural styles is strongly discouraged. See photo.
2. **Building Façades.**
 - a. Primary building entries should provide a prominent sense of entry for easy identification. The use of architectural projections, columns, entry lobbies or other design elements are strongly encouraged.
 - b. The size of doors and windows should relate to the scale and proportions of the overall structure. Monumental entries that are not scaled to the pedestrian are discouraged.
 - c. The use of security grilles on windows is discouraged because they communicate a message of high crime and are difficult to integrate into the building design. If security grilles are necessary, they should be placed inside the building behind the window.



Building design matches adjacent development, not corporate style



Window-to-wall ratios on upper floors are lower than on ground floor

- d. Opening composition is required.
- e. Upper story window to wall ratios should be lower than the ground floor. See photo.
- f. Windows on the first floor should encompass:
 - i. Retail uses: a minimum of fifty percent (50%) and maximum of seventy percent (70%) of the building facade.
 - ii. Office uses: a minimum of forty percent (40%) and a maximum of fifty percent (50%) of the building facade.
- g. Shaped window frames and sills should be used and must be proportional to the window framed.
- h. Public rear entrances should be visible and easily located.
- i. Rear entrances should not be more prominent than the front, primary entrance.

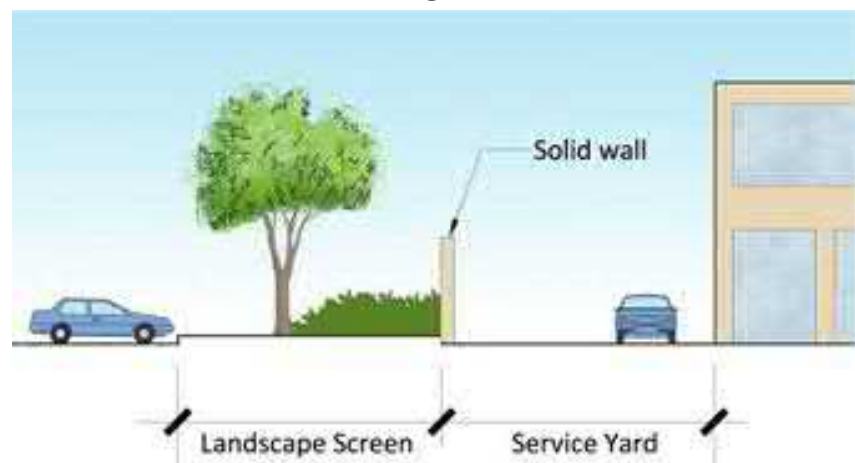


Trash enclosure with trellis cover

3. **Storage, Refuse and Equipment Screening.**

- a. Storage, refuse and equipment areas should be screened from publicly accessible spaces and/or neighboring residential uses. Landscaping and/or architectural enclosures can be used to screen these areas. See Figure 7.7.
- b. Screening should be compatible with the design of adjacent architecture. Enclosures should use forms, materials and color applications similar to the primary building(s).

FIGURE 7.7 Building Orientation



Wall and landscaping screen service area from public parking area

- c. Refuse storage areas that are visible from upper stories of adjacent structures should provide an opaque or semi-opaque horizontal cover or screen to reduce unsightly views. The horizontal screen/cover should be integrated into the enclosure design and compatible with adjacent development. See photo.
- d. Refuse storage enclosures should be located so that the doors do not interfere with landscaping and pedestrian and vehicle circulation.
- e. Dimensions of refuse enclosures should not exceed the reasonable space required for anticipated uses.
- f. Utility equipment should be located at the rear of building. When equipment is required to be located along street frontages, it should be screened from public view.
- g. Rooftop equipment should be fully screened from public view by parapet walls or roof elements.

D. Landscaping.

1. General.

- a. Landscaping should enhance the quality of commercial developments by framing and softening the appearance of buildings, screening undesirable views, buffering incompatible uses and providing shade.
- b. The use of drought tolerant plant materials is encouraged. See photo.
- c. Areas not utilized by structures, storage, paved walks, plazas, driveways or parking should be landscaped. See photo.



Drought tolerant landscaping provides a softened parking edge and walkways

2. Parking Lot Landscaping.

- a. Landscaping should be used to separate parking from buildings and to reduce the visual impact of paved surfaces.
- b. Parking lots should be separated from the street frontage by landscape buffers and berms to reduce visual impacts.
- c. Landscaping at the base of buildings is encouraged to soften the transition between building and parking lot.



Bicycle racks adjacent to pedestrian walkway



Well-defined primary building entry

3. Site Furniture.

- a. Site furnishings (i.e., benches, bicycle racks, bollards, trash receptacles and newspaper racks) should be compatible with the design of adjacent buildings.
- b. Site furnishings should not create pedestrian/vehicular conflicts. Developments are encouraged to provide bicycle storage facilities such as racks or lockers. See photo.
- c. Bicycle storage should be easily accessible from the street, transit stops and pedestrian routes.

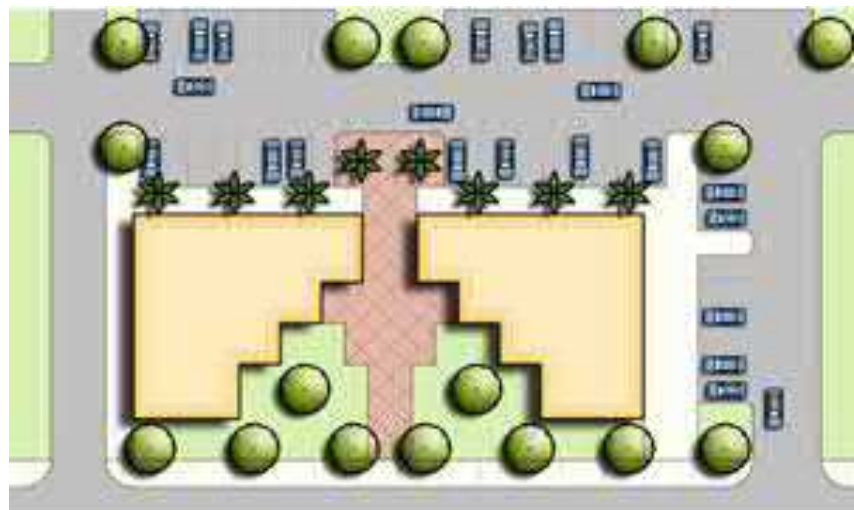
Special Use Commercial Design Guidelines.

The guidelines contained in this subsection provide supplementary design guidance addressing more challenging development types. Certain types of development present design challenges that require unique solutions. Development types in this section include:

A. **Office.** Office uses have physical and functional characteristics that are not typical of commercial development. Office uses are typically within multi-story buildings that are larger in scale. There are fewer public entries and higher utilization of on-site parking. Within the buildings, the types of tenants are usually consistent.

1. **Site Organization.** Parking should not be located between the front of the building and the street. Surface parking should be located at the rear of the site or the side of the building. See Figure 7.8.

FIGURE 7.8 Parking Location



Locate parking to the side and rear of the building

2. Building Design.

- a. Office development should incorporate variations in vertical and horizontal wall planes to reduce scale and massing.
- b. Primary building entries should be well defined and provide a “sense of entry” for the building. Use of architectural features such as columns and awnings is encouraged. See photo.
- c. Office buildings should have the primary entry visible from the public street and be accessible from pedestrian pathways or parking areas.
- d. Entry lobby interiors should be well-designed and of high quality when visible from the building’s exterior.
- e. Development occurring at corner locations should provide creative architectural and site design features. Art objects, plazas and other appropriate features are encouraged.

B. Drive-Through Businesses. Drive-through businesses include restaurants, banking institutions with drive-up teller/ATM access, pharmacies and other similar facilities. Drive-through businesses have additional design considerations related to on-site circulation, noise impacts and vehicular access.

1. Site Organization.

- a. The primary visual presence along the major street frontage should be the building, not a drive-through lane or parking.
- b. Menu board speaker placement should be located to reduce excessive noise and reduce impacts to surrounding areas and should be landscaped in a manner that screens or minimizes the impacts. Use of visual feedback systems that do not have a speaker are encouraged.
- c. Drive-through lanes should accommodate vehicle stacking at the menu board and pickup windows without impacting other on-site circulation.
- d. Drive-through aisles should be separated from adjacent streets and parking areas. Landscaping and/or low masonry walls should be utilized. See photo.



Landscaping separates drive-through from adjacent street

2. Building Design.

- a. Outdoor eating areas are encouraged. Outdoor eating areas should provide details such as low walls, fountains, trellis elements, etc.
- b. Franchise identifying features should only be located on the main structure.
- c. Exterior doors, equipment rooms, and service/employee entries should be designed with complementary architectural treatment.

C. **Large Anchor Retail.** Large anchor retail development is characterized by larger than average retail square footage under one roof. Large anchor retail development requires extensive parking to accommodate higher-than-average parking demand. Large anchor retail uses are often located on sites with multiple buildings of various sizes.

1. Site Organization.

- a. Parking lots for large anchor retail uses are discouraged from being located entirely in front of the building. Smaller lots to the sides and/or rear of the building are encouraged.
- b. The number of entrances and exits should be designed and located to avoid interfering with traffic flow along adjacent streets.



Colonnades and portico used to break up mass of building.

2. Building Design.

- a. Exterior walls should terminate with a building cap such as a roof or cornice.
- b. Distinct and interesting rooflines instead of flat roofed structures are encouraged.
- c. Exterior wall treatments such as arcades, porticos, offset planes and colonnades should be used to break up the mass and scale of the building. See photo.
- d. Design of outdoor storage or sales areas (i.e. garden centers) should be consistent with the architecture of the primary building, utilizing articulated walls or fencing to enclose the space.

3. Loading and Service Areas.

- a. Loading and service areas should be located and designed to minimize direct exposure to public view. Walls or landscape screening should be used to reduce visual impacts.

- b. Loading area location and design should prevent truck back-up maneuvers from or onto public rights-of-way or parking aisles.

D. **Public Assembly Facility.** Public assembly facilities include buildings where large groups of people gather and utilize large common spaces. These uses typically have significant on-site parking requirements and are characterized by higher profile architectural design.

1. **Site Organization.**

- a. The location and orientation of buildings, parking lots, driveways and entries should be designed to minimize noise and traffic impacts on adjacent properties.
- b. Avoid locating parking between the front of the building and the street. Parking should be located at the rear and/or side of the property.
- c. Exterior spaces, plazas and courtyards should be designed to minimize noise impacts on adjacent properties.
- d. Screening and buffering should be provided between places of assembly and adjacent residential uses.

2. **Circulation and Parking.**

- a. Parking should be located in close proximity to the primary entrance. A safe pedestrian path should be provided from all parking areas to the main entrance.
- b. Parking should be screened from public view by walls, landscaping or other appropriate methods.
- c. Passenger pickup and drop-off areas should not interfere with on-site and off-site vehicular and pedestrian circulation.

E. **Hotels and Motels.** Hotels and motels are a commercial use with some residential attributes. They are often highly visible from primary roadways. Therefore, their design has a high level of impact on the surrounding area.

1. **Site Organization.**

- a. The primary visual presence along the major street frontage should be the building and driveway approach not a parking lot.

- b. Delivery and service loading areas should be located toward the rear of the property and screened to minimize impacts.
- c. Recreational facilities such as swimming pools should be designed to offer privacy for users and to minimize noise impacts on adjacent uses.
- d. Common open space should be provided on-site.

2. **Building Design.**

- a. Upper floors should be set back to lessen the appearance of mass or bulk.
- b. For structures over two stories, access to guest rooms should be provided from interior hallways.
- c. Avoid room entrances directly adjacent to parking lots or exterior walkways.
- d. Walkway, stairway and balcony railings, and other similar architectural details should be consistent with the building design.
- e. Mechanical equipment including swimming pool equipment should be located to minimize impacts on adjacent uses and screened from public view using landscaping or architectural enclosures.

- 3. **Parking and Circulation.** A covered drop-off zone for vehicles and pedestrians independent of drive aisles should accommodate guest loading and unloading.

- 4. **Landscaping.** A five-foot minimum landscaped buffer should separate ground floor units from on-site walkways parking facilities and other on-site amenities.

- F. **Mixed-Use.** Mixed-use projects are defined as developments that combine commercial/office and residential uses or structures on a single lot, or as components of a single development. The uses may be combined either vertically within the same structure, or spread horizontally on the site in different areas and structures.

1. **Site Organization.**

- a. Parking facilities for residential uses and commercial uses should be separated.
- b. Commercial loading areas and refuse storage facilities should be placed as far as possible from residential units and be completely screened from adjacent residential development.



Residential units above retail space have consistent materials, but details differentiate uses

- c. Parking area lighting and security lighting for the commercial uses should be shielded so as not to spill over into residential areas. Residential units should also be shielded from illuminated commercial signs.
- d. Commercial spaces should have the primary entry visible from the public street and be accessible from pedestrian pathways or parking areas.

2. Building Design.

- a. Separate entrances should be provided when residential and commercial uses are provided in the same structure.
- b. The architectural imagery and use of materials should be consistent throughout the entire mixed-use project. However, differences in materials and/or architectural details should be used to differentiate the residential portion of the project from the commercial portion of the project. See photo.
- c. The design of storefronts should be consistent with the design guidelines for general commercial development. The residential portion of a mixed-use project should be consistent with the design guidelines for multi-family development.
- d. Transparency between the commercial spaces on the ground floor and the street is highly encouraged.
- e. Building design should incorporate measures to reduce the impacts of business activities, such as noise, on residential uses. See photo.



Balcony covers outdoor dining space, providing a noise buffer



Balcony covers outdoor dining space, providing a noise buffer

- G. **Parking Structures.** Parking structures provide an option to increase parking without utilizing additional land for surface parking. They can be integrated in a single building with other uses, or stand alone. Parking structure design must consider architectural compatibility, size, scale, bulk and function as it relates to adjacent buildings and overall design of a project.

- 1. **Site Organization.** Adjacent residential uses should be buffered from parking structures through building orientation, landscaping or walls.

2. Access and Circulation.

- a. Complete and uninterrupted vehicle circulation should occur on all levels of the parking structure.

- b. Entrances and exits should be located so that vehicles do not enter or exit onto residential streets or at busy intersections.
- c. Pedestrian circulation should be clearly delineated and separated from vehicular circulation. The use of landscaping, walkways and decorative paving is encouraged. See photo.



Glass-backed elevator shaft and cab



Retail store lines ground floor of parking structure

3. **Security and Lighting.**

- a. Visibility into the structure from adjacent activity and public areas should be maximized to provide natural surveillance.
- b. Locating elevators and stairs on the perimeter of the structure is highly encouraged to provide natural surveillance from exterior public areas.
- c. Elevators should be located where the door and open and elevator cab should have glass facing the public view. Any glass tinting should be minimal to ensure daytime and nighttime visibility. See photo.
- d. Interior walls should be painted a light color to improve illumination.

4. **Building Design.**

- a. Exterior elevations should incorporate design components and materials similar to or compatible with the primary building(s). This includes fenestration patterns, colors and materials.
- b. Retail or other uses are encouraged along the exterior at the ground level of the parking structure where appropriate. See photo.
- c. Landscaping along the setback area is encouraged.
- d. The exterior elevations of the parking structure should exhibit horizontal rather than sloping design elements.
- e. Exterior elevations should be designed to minimize untreated facades.
- f. All mechanical equipment and piping should be painted to match the interior of the structure.

- 5. **Screening.** All appurtenances (i.e., transformers, ventilation shafts, maintenance access, etc.) should be screened from public view.

- H. **Motor Vehicle Dealerships.** Features of vehicle dealerships typically include outdoor storage and display of vehicles vehicle display oriented toward the street and vehicle drop off and pick up areas.

1. **Site Organization.**

- a. Noise sources should be located and oriented away from residential properties.
- b. All storage areas should be screened from view from the public right-of-way and adjacent residential areas.

2. **Building Design.**

- a. Ancillary structures such as enclosed storage areas should be architecturally consistent with the primary building on the site.
- b. Service uses should be contained entirely within a building of solid construction.
- c. The access points to the service bays should not face the public right-of-way.
- d. Vehicle washing areas should be designed and located so they are not visible or audible from the public right-of-way or residential areas.

3. **Walls and Fences.** Security fencing and perimeter walls should be decorative and consistent with the architecture of the primary buildings.

4. **Other.**

- a. Public address systems should not be used in outdoor areas. Any public address system should confine noise within an enclosed building.
- b. Compressors and other pneumatic equipment should be located on the interior of a site to minimize noise impacts on adjacent properties.
- c. Storage facilities should be provided on-site for vehicles parts and potentially hazardous materials.
- d. Storage facilities should be enclosed entirely within permanent buildings.

- I. **Service Stations and Car Washes.** Service stations and car washes are intensive uses that are characterized by large volumes of vehicular traffic and expansive areas of paving that generally allow vehicles to maneuver freely. Aesthetics, traffic, use of equipment and machinery and storage are common concerns associated with these uses.



Corner design element at service station



Sloped roof on pump island canopies

1. Site Organization.

- a. Projects located at street corners should provide a strong design element to anchor the corner. This can be accomplished using a reverse building placement where the main building is placed on the corner or by using a prominent landscaping feature. See photo.
- b. Building elevations containing car wash openings should not face towards the public right-of-way or toward residential uses.
- c. Rather than adopting a standard design, elevations that are unique to the community and are not a corporate or franchise design are strongly encouraged.

2. Building Design.

- a. High quality durable building materials must be used.
- b. Service stations and car washes should incorporate facade materials that create a textured design.
- c. Reflective glossing and fluorescent surfaces are prohibited.
- d. Car wash equipment must be enclosed within a permanent structure.
- e. Car wash buildings must incorporate noise control measures that minimize noise generated by machinery blowers or other mechanical equipment.
- f. Pump island canopies should use a hip gable or other roof system to match the architecture of the primary buildings on the site. See photo.
- g. Recessed lighting or internally illuminated canopies must be utilized to reduce glare.

3. Circulation and Parking.

- a. Driveway cuts should be limited to two per site.
- b. Where commercial development abuts the service station two-way vehicular access integrated with the adjacent commercial development should be provided.
- c. The location of filling pumps, car washing facilities or other on-site facilities must be designed to avoid vehicle stacking or overflow onto adjacent streets.

- d. Self-service facilities such as water and air or vacuuming must be located so that they do not obstruct onsite circulation.
- e. Separate parking areas must be provided for on-site retail.
- f. A fuel delivery truck unloading zone should be provided and should not obstruct vehicular circulation and parking areas.

4. **Landscaping Walls and Fences.**

- a. Landscaping along the perimeter of the site to reduce visual impacts is encouraged.
- b. Landscaped berms or hedges should be located along public streets.

- J. **Telecommunication Facilities.** Telecommunications facilities include poles, towers, antenna support facilities and components. These facilities have special design and location considerations in order to minimize adverse visual impacts on the surrounding area.

1. **Location.**

- a. Telecommunications facilities should be located to minimize their visibility. The use of landscaping is encouraged to screen the facility.
- b. Co-location of facilities is encouraged.

2. **Facilities Design.**

- a. Telecommunications facilities integrated into the design of existing buildings is encouraged.
- b. Efforts should be made to conceal the facility within or behind existing architectural features.
- c. Roof mounted facilities should be located and screened appropriately to limit the visual impact on the building silhouette.
- d. Facilities should be screened on all sides.
- e. All telecommunications facilities should use materials colors and textures that blend with the natural setting and built environment. Reflective surfaces should not be used.
- f. Telecommunications facilities should be as small as possible and the minimum height necessary.

- g. Applicants should consult the General Commercial Guidelines in addition to the guidelines contained within this subsection.

7.6.6 RESIDENTIAL DESIGN GUIDELINES

This section provides design guidelines and concepts applicable to residential projects within the BAMU Overlay Zone.

A. Site Planning.

1. Developments should relate to the adjacent street and neighborhood context. Developments that ignore the street and create an isolated enclave are highly discouraged.
2. Developments should generally be oriented parallel to public or internal streets. See Figure 7.9.
3. Doors should be visible from the street or other public areas and windows should allow residents to have “eyes on the street” for natural surveillance.
4. Clustering of multi-family units should be a consistent site planning element. Whenever possible, buildings should be configured around courtyards, gathering areas and open spaces. See photo.
5. Pedestrian circulation should be considered when designing a multi-family development, not only within the site, but to and from the site as well.
6. Windows and entries should be placed to maximize natural surveillance of the site. Corridors should be visible from as many units as possible.



Buildings face a pedestrian activity space



Building mass is broken up by balconies, changes in wall plane and detailing

FIGURE 7.9 Residential Building Orientation



Buildings are orientated parallel to street

B. Building Scale and Massing.

1. Architectural elements such as recessed or projecting balconies, porches or other elements that add visual interest, human scale and character to the neighborhood are encouraged. See photo.
2. Varied building heights are encouraged to provide visual interest and give the appearance of a collection of smaller structures. The development's building height should create a transition from the heights of adjacent development, rather than abrupt height changes. For example, a three-story building facade should not be located adjacent to a single-story, single-family home.



Detached garages and trash enclosures utilize the same materials and colors as the primary buildings

C. Building Entries.

1. Courtyard doors or gates used as building entries should be attractively designed as an important architectural feature of the building or development.
2. Individual unit entries from the exterior of the building should have a strong relationship with a fronting street, internal walkway or courtyard as appropriate to the overall siting concept. A transition area from the public space or walkway to the private dwelling unit entry such as a porch, steps or landscaped walkway, should be provided.

D. Stairs.

1. Building facades that enclose stairways should include residential-type windows to reduce the visual bulk of the stairwell and enhance safety. Building facades enclosing elevator shafts should use architectural treatments to reduce visual mass.
2. Where prefabricated stairs are used, additional design features such as screen walls, enhanced railings or accent colors should be used to enhance appearance. The additional design features should be consistent with the overall building design.

E. Building Materials and Color.

1. The development's dwelling units, community facilities and parking structures should be unified by a consistent use of building materials, textures and colors.
2. Garages should incorporate architectural detailing, such as patterned garage doors or painted trim.
3. Carports, detached garages and accessory structures should be designed as an integral part of the

development's architecture. They should be similar in material, color, and detail to the primary building(s) of the development. See photo.

F. Roofs.

1. Roof pitches and materials should appear residential in character and should consider the prevailing roof types in the neighborhood.
2. Carport roofs should be compatible with the roof pitch and materials of the primary building(s). Avoid flat, unarticulated roofs.
3. Roofs should allow for solar panels.

G. Mechanical Equipment and Vents.

1. The following must be screened from views from publicly accessible spaces:
 - a. Electric and water utility meters;
 - b. Heating/ventilation/cooling equipment;
 - c. Irrigation and pool pumps;
 - d. Rooftop mechanical equipment;
 - e. Other mechanical equipment.
2. Appropriate methods of screening include fencing, landscaping, roof parapets and architectural enclosures. The design of screening devices must be compatible with the primary building(s).
3. Mechanical equipment must be located as to minimize noise impacts on residential units.

H. Outdoor Recreation and Open Space.

1. Residents should have access to usable open space for recreation and social activities. Open spaces should be conveniently located for the majority of the units.
2. Open spaces and courtyards should be designed to be as visible from as many dwelling units as possible or along main walkways.
3. Open space areas should be sheltered from the noise and traffic of adjacent streets or other incompatible uses.
4. A series of connected open space areas of varying shape, appearance and usage are encouraged.
5. Open space should be configured and sized to be usable areas and not merely leftover areas.

6. Open space intended for private use should be provided adjacent to the units it serves.
7. Boundaries between private and common open spaces should be clearly defined by low walls or plant materials. See photo.
8. Outdoor recreation and open space areas should be visible from as many units as possible and from private open space areas. Direct convenient access from ground level, private open space to the communal play area is encouraged.
9. Recreation areas should be located away from public streets, parking or entry areas unless physically separated by walls, fencing or dense landscaping.
10. Hard surface areas for outdoor activities should be provided. These active recreation areas should be separated from vehicular use areas.
11. Seating areas should be provided adjacent to outdoor recreation areas. Seating location should consider comfort factors including sun orientation, shade and wind. See photo.



Low wall/fencing separates private open space from common area

I. Miscellaneous Site Elements.

1. Site Furniture.
 - a. Site furnishings (e.g., benches, bicycle racks, bollards, trash receptacles and newspaper racks) should be compatible with the design of adjacent buildings.
 - b. Site furnishings should not create pedestrian/vehicular conflicts.
2. Refuse Storage Areas.
 - a. Refuse storage areas should be located within parking garages or to the rear of buildings and screened from publicly accessible areas.
 - b. Refuse storage areas should be located conveniently close to the residential units.
 - c. Refuse containers should be covered.
 - d. Architectural enclosures should be used to enclose outdoor refuse containers.
 - e. The design of enclosures should be architecturally compatible with other buildings on the site, and their design should use similar forms, materials and colors.



Seating along common open space is shaded by trellis

- f. Refuse storage should provide an opaque or semi-opaque horizontal cover or screen to reduce unsightly views. The horizontal screen/cover should be integrated into the enclosure design and should be compatible with the adjacent development.
- g. The refuse storage areas should be located so that the doors do not interfere with landscaping and pedestrian and vehicle circulation.
- h. Dimensions of refuse enclosures should not exceed the reasonable space required for anticipated use.
- i. Mailboxes should be located in highly visible, conveniently accessible areas, per USPS requirements.
- j. Incorporation of design features, such as a built frame consistent with the primary building's architectural design, is encouraged.

J. Landscaping.

- 1. Use of landscaping is encouraged to define and accent specific areas such as building and parking lot entrances and the main walkways to community facilities.
- 2. Plant materials should be used to define the edge between public and private space, buffer adjacent uses when appropriate, and screen service areas.

K. Parking and Circulation.

- 1. Large surface parking areas, where cars would dominate views and increase perceived density, should be avoided. Surface parking should be divided into a series of small parking areas with convenient access that relates to adjacent dwelling units.
- 2. Parking areas should be located in the development's interior and not along street frontages. Screening measures, such as landscaping and walls, should be used to minimize visual parking impacts.
- 3. Cross circulation between vehicles and pedestrians should be minimized. A clearly marked walkway should be provided from the parking areas to the main entrances of the buildings.
- 4. Access to transit stops should be integrated into the pedestrian circulation network.
- 5. Bicycle storage such as racks or lockers should be provided and be easily accessible from the street and the pedestrian routes.

6. Sight lines from dwelling units to the parking area should be provided.

L. **Lighting.**

1. Light standards should be residential/pedestrian in scale and be spaced appropriately for the fixture, type of illumination and pole height. See photo.
2. Lighting in parking areas should be arranged to prevent direct glare into adjacent dwelling units.
3. When appropriate, wall-mounted lighting may be incorporated. Wall-mounted lights should be architecturally compatible and pedestrian scaled.



Lighting should be pedestrian-scaled

7.6.7 PUBLIC SPACE AND AMENITIES DESIGN GUIDELINES

This section is intended to provide design guidance on publicly accessible open spaces including walkways, plazas and courtyards, as well as site amenities or features.

A. **Plazas and Courtyards.**

1. A plaza or courtyard should have an articulated edge (buildings, benches, landscaping, etc.) to provide a sense of arrival.
2. Site amenities such as seating, lighting, planters, drinking fountains, distinctive paving, art work, and bicycle racks should be provided. See photo.
3. Plazas should incorporate focal points such as sculptures or water features where space permits. See photo.
4. Furniture and fixtures used in the plaza areas should complement the building architecture.
5. Soft and hard-surfaced areas should be incorporated into the overall plaza design.
6. Decorative paving used in the plaza areas should complement the paving pattern and colors used elsewhere on-site.



Fountain provides focal point, art work and seating

B. **Sidewalk Dining.**

1. Street furniture should be compatible with the adjacent public improvements, private structures, and building materials. The design materials and colors used for chairs, tables, lighting, and other fixtures including umbrellas and awnings should be generally consistent with both the architectural style and colors used on the building facade.



Removable umbrellas are used to shade tables, but do not interfere with existing trees

2. The street furniture cannot restrict motor vehicle sight lines.
3. The street furniture cannot impede entry to the building and must not preclude applicable emergency exit requirements.
4. All barriers and/or surrounding outdoor dining areas should withstand inclement outdoor weather and one hundred (100) pounds of horizontal force at the top of the barricade when in their fixed positions.
5. Outdoor dining facilities should be for sit-down food and beverage service only; no stand-up service.
6. Seating (tables and chairs) should not be removed from outdoor dining areas during business hours.
7. To identify the most appropriate location(s) adjacent to the restaurant to locate outdoor dining, consider the relationship between the characteristic of the spaces and activities surrounding possible outdoor dining locations. Outdoor dining facilities must comply with Chapter 4 of this Specific Plan.
8. The use of retractable awnings or removable umbrellas is generally encouraged. See photo.
9. Awnings and umbrellas should not cover more than fifty percent (50%) of the outdoor dining area, providing patrons with a choice between sun and shade.
10. Awnings should maintain at least eight (8) feet of vertical clearance above the sidewalk level and should not exceed fifteen (15) feet in height.
11. Umbrellas should be mounted in a stable base to minimize accidental or windblown tipping over.
12. Awnings and umbrellas should be located to not interfere with street trees. See photo.
13. Umbrellas and awnings should be cleaned at least once per month.
14. Barriers surrounding outdoor dining areas should complement the facade of the building and the public realm, as well as be highly transparent (e.g., wrought iron fencing).
15. Within the outdoor dining area, especially near entrances, provide trash receptacles for use by patrons and restaurant employees to minimize litter.

16. When designing an outdoor dining area, provide appropriate lighting to create a nighttime ambience that complements the adjacent buildings and public realm.
17. Lighting should be incorporated into the facade of the building, and should complement the style of building. Lights on buildings should not be glaring to pedestrians, and should illuminate only the outdoor eating area.
18. Plants, trees, and flowers, when used within the outdoor dining area, should be in portable containers to promote the most flexible use of the space.
19. For potted plants, include a saucer or other suitable system to retain seepage.

C. Walls and Fences.

1. Walls and fences are generally used for security purposes, to define ownership, to mitigate nuisances such as noise, and to screen areas from public view. Walls and fences should be kept as low as possible while performing their functional purpose.
2. Landscaping should be used in combination with walls to soften otherwise blank surfaces. Vines planted on walls are strongly encouraged to hide flat wall surfaces to help reduce graffiti.

D. Lighting.

1. Pedestrian scale lighting is strongly encouraged. The style and color of lighting should be consistent with the character of a public space.
2. Bollards are intended to separate pedestrians from vehicular traffic areas and to light sidewalk surfaces. Bollard design should coordinate with other streetscape furnishings. In locations, where emergency access may be necessary, removable bollards are encouraged.

E. Site Amenities.

1. Site amenities form elements of commonality, which help to establish the identity of an area and provide comfort and interest to its users. Individual site amenities within an area should have common features, such as color, material, and design to provide a cohesive environment and more identifiable character.
2. Refuse receptacle design should coordinate with other streetscape furnishings.



Pots located out of pedestrian circulation area



Consolidated newspaper rack

3. Pots and planters should be located where pedestrian flow will not be obstructed. See photo.
4. Pots and planters should be durable and have natural color tones that complement the adjacent structures.
5. Bicycle racks should be selected that are durable. Based on their performance, “loop racks” and “ribbon bars” are encouraged, and should be sized according to parking requirements. Bicycle racks that are sculptural or also serve as public art may be used.
6. The design of newspaper boxes should be consolidated into one rack. The rack should be attractive on all sides and properly anchored. See photo.
7. Visual features, such as fountains, should be incorporated into public spaces to attract pedestrians.

F. Street Furniture.

1. Seating is an important amenity that should be provided throughout commercial areas.
2. The design and selection of street furniture should include considerations for the security, safety, comfort and convenience of the user.
3. Unified street furniture “look” is encouraged. The color and appearance of street furniture products should be selected to complement other design elements.
4. Where possible, furnishings should be grouped together.
5. A greater number and type of furnishings should be provided in higher-use pedestrian traffic areas. Furnishings should not obstruct pedestrian circulation.



Tree grate

G. Landscaping.

1. Tree grates should occur along street edges and plazas where a continuous walking surface is needed. Grate sizes should be a minimum of four feet in diameter. Knockouts should be provided to enlarge the inside diameter for supporting a larger tree trunk as the tree grows. See photo.
2. Tree guards should extend vertically from tree grates, and serve to protect trees in highly active areas. Tree guards should be narrow and painted in a similar color and relate to other site furnishings.

7.7 DESIGN GUIDELINES FOR BVOZ

7.7.1 DESIGN GUIDELINES

The design guidelines and criteria for the Bellflower Village Overlay Zone are intended to provide flexibility in site design and architectural design to allow for the integration of mixed-uses, commercial and residential uses within the boundaries of the Bellflower Village Overlay Zone.

- A. **Architectural Design/Style.** Bellflower Village does not require a particular architectural style or design. The development within the boundaries of the overlay zone must be required to be architecturally consistent in style and theme, and that all structures have “four-sided” architecture treatment. The design methods must eliminate the appearance of bulk, by creating varied elevation planes, broken and varied rooflines, the use of balconies, patios, lanais, patio covers, chimneys and bay windows and similar architectural treatments.

Furthermore, multiple-storied buildings must be designed in such a manner as to reduce or eliminate the “canyon feel” that would otherwise occur when the building planes between two structures are flat and without variation.

- B. **Construction Materials and Use of Colors.** Development within the Bellflower Overlay Zone must utilize quality construction materials throughout the development project, including interior fixtures. In addition, multiple construction materials must be utilized on the building exteriors to provide varying and interesting textures, shading patterns, varying building relief and interesting architectural accent treatments. The use of building material varieties must be done in such a manner that is compatible with the chosen architectural theme for the project. Where feasible, the use of overhead trellises, awnings and canopies must be incorporated into the project. Insofar as possible and feasible, “green” building design and construction must be utilized.
- C. The following materials are prohibited as exterior building treatment and fencing:
1. Plain concrete block;
 2. Plain concrete (unless utilized in an overall architectural design theme);
 3. Chain link;
 4. Corrugated metal;

5. T-111 plywood; and
 6. Vinyl siding.
- D. **Landscaping and Hardscape.** A master landscape and hardscape plan must be approved pursuant to the provisions of section 4.8.6.

7.8 DESIGN GUIDELINES FOR BVOZ-N

7.8.1 DESIGN GUIDELINES

The design guidelines and criteria for the Bellflower Village Overlay Zone – North is intended to provide flexibility in site design and architectural design to allow for the integration of mixed-uses, commercial and residential uses within the boundaries of the Bellflower Village Overlay Zone – North Area.

- A. **Architectural Design/Style.** The Bellflower Village Overlay Zone – North does not require a particular architectural style or design. The development within the boundaries of this overlay zone is required to be architecturally consistent in style and theme, and that all structures have “four-sided” architecture treatment. The design methods must eliminate the appearance of bulk, by creating varied elevation planes, broken and varied rooflines, the use of balconies, patios, lanais, patio covers, chimneys and bay windows and similar architectural treatments. Furthermore, multiple storied buildings must be designed in such a manner as to reduce or eliminate the “canyon feel” that would otherwise occur when the building planes between two structures are flat and without variation.
- B. **Construction Materials and Use of Colors.** Development within the Bellflower Village Overlay Zone – North must utilize quality construction materials throughout the development project, including interior fixtures. In addition, multiple construction materials must be utilized on the building exteriors to provide varying and interesting textures, shading patterns, varying building relief and interesting architectural accent treatments. The use of building material varieties must be done in such a manner that is compatible with the chosen architectural theme for the project. Where feasible, the use of overhead trellises, awnings and canopies must be incorporated into the project. Insofar as possible and feasible, “green” building design and construction must be utilized. The following materials are prohibited as exterior building treatment and fencing:
1. Plain concrete block;

2. Plain concrete (unless utilized in an overall architectural design theme);
 3. Chain link;
 4. Corrugated metal;
 5. T-111 plywood; and
 6. Vinyl siding.
- C. **Landscaping and Hardscape.** A master landscape and hardscape plan must be approved pursuant to the provisions of section 4.9.5.
- D. **Enhanced Architectural Design.** Multi-family residential developments that incorporate enhanced or desired architectural design elements are eligible to participate in the Enhanced Architectural Design point system. A point value is assigned to each of the desirable architectural design elements identified in the tables below. If at least 60 points are awarded by the Director, through incorporation of the various desirable architectural elements, then the applicant may request an Administrative Conditional Use Permit (ACUP) for the proposed multi-family residential development. See Table 7.1. Maximum points will be awarded when the Director has determined that proposed elements have been applied comprehensively to the entire project, including, without limitation, all structures, buildings, and open spaces. Elements that are determined to not meet the maximum number of points will be awarded 0 to 5 points when 5 points are the maximum or 0 to 10 points when 10 points are the maximum.

TABLE 7.1 BAMU Desirable Elements
(For Multi-Family Attached or Detached Residential)

DESIRABLE ELEMENTS		DEFINITION OF ELEMENT	POINTS (Maximum points allowed unless otherwise specified)
1	Eliminate large blank exterior walls	Provide varied architectural treatments and articulation. Incorporate features such as varied building elevation plane depth, bay windows, corbels, cornice treatments and other similar architectural details. The intent is to provide visually-interesting design that has compatible architectural treatments.	5
2	Front entry	Projects may incorporate the following entry design elements (a maximum of 5 points will be awarded if more than 1 element is incorporated): a. Front Porch. Provide a minimum 5-foot wide porch that spans the length of the building width; or b. Appearance of Front Porch or Entryway. Architectural articulation on the front of the residential structure which resembles a porch; or c. Strong Entry. Any architectural articulation on the front of the unit which provides a "sense of entry."	5 2 5
3	Provide double front door or single door with side-lights	Use of double front doors or a single front door with decorative side-lights.	5
4	Provide additional building material (other than stucco)	Provide at least 1 additional exterior wall material other than (or in conjunction with) stucco, including stone and/or brick veneer; wood siding; etc. The additional material(s) must be consistently applied and must work harmoniously with connecting materials. Piecemeal embellishment does not qualify. A minimum of 1/3rd of the primary façade and 1/4th of the remaining wall surfaces.	10
5	Provide large trees	1 specimen tree, a 72-inch box or larger. (A maximum of 10 points may be obtained with specimen trees.) 1 48-inch box tree for every 2 units. (A maximum of 10 points may be obtained with specimen trees.)	5 2
6	Additional landscaping	Provide shrubs and vegetation along front elevation to soften views of the front elevation of each unit.	5
7	Building height	Points will be given if the residence is 3 stories or more.	5

TABLE 7.1 BAMU Desirable Elements
(For Multi-Family Attached or Detached Residential)

DESIRABLE ELEMENTS	DEFINITION OF ELEMENT	POINTS (Maximum points allowed unless otherwise specified)
8 Provide open space	Provide at least 10 to 14% percent of total project area as open space. No more than 1/2 of open space may be provided as hardscape.	2
	a. 15% - 19%	5
	b. 20% - 24%	8
	c. 25%+	12
9 Provide varying roof planes and ridgelines	Provide varying roof planes and ridgelines to eliminate identical roof ridgelines.	5
10 Provide varying roof massing	Design must minimize large expanses of the roof to ensure the roof does not dominate the overall view of the building elevation. The use of dormers, additional gables, and similar design features can be considered.	5
11 Provide upgraded roof materials	Use roof materials other than asphalt architecturally compatible shingles. The roof material must be rated for a minimum of:	
	a. 30 years	2
	b. 40 years	5
	c. 50 years	8
12 Decorative hardscape for driveways	Provide private paving material other than plain asphalt or concrete. Interlocking unit pavers are preferred, but stamped, colored concrete is acceptable. A minimum of at least 30 percent of the total surface area of the driveway must be comprised of decorative hardscape material to be eligible for this category.	5
13 Connection to public spaces	Provide connection to Greenway Trail, plazas and other similar spaces.	2
	Provide open connection publicly accessible.	5
14 Distinctive architectural style	Provide architectural theme that carries design/elements on all 4 sides of building.	10

TABLE 7.1 BAMU Desirable Elements
(For Multi-Family Attached or Detached Residential)

DESIRABLE ELEMENTS		DEFINITION OF ELEMENT	POINTS (Maximum points allowed unless otherwise specified)
15	Buffering	Trash area, storage area, transformers, mechanical equipment, etc.:	
		1 item	2
		2 or more	5
16	Exterior lighting	Design to be unobtrusive, integrated with project design; low profile.	2



CHAPTER 8

ADMINISTRATION & IMPLEMENTATION

8. ADMINISTRATION AND IMPLEMENTATION

8.1 SPECIFIC PLAN ADOPTION

The Bellflower Transit Oriented Development Specific Plan (the “Specific Plan”) was prepared in conformity with Government Code §§ 65451, et seq. Together with the Bellflower Municipal Code (“BMC”) and other applicable law, this Specific Plan sets forth the development and zoning regulations for the Specific Plan area.

8.2 MUNICIPAL CODE REFERENCE

This Specific Plan takes precedence over the BMC in the event of any conflict. This Specific Plan provides the zoning regulations and procedures for the Specific Plan Area. Any standards or regulations that are silent in this Specific Plan will revert to the BMC.

This Specific Plan provides the entire zoning for this Specific Plan area. The entire property is zoned “TOD SP” and the applicable zoning regulations for this Specific Plan area are those set forth in this Specific Plan.

8.3 DEFINITIONS

In addition to the defined terms set forth in this Specific Plan, unless the contrary is stated or clearly appears from the context, the following definitions govern the construction of the words and phrases used in this Specific Plan. Words and phrases not defined by this chapter have the meaning set forth elsewhere in the BMC or applicable law.

The following provide definitions of terms used in this Specific Plan:

“Applicable Law”: All Federal, State and City laws including the Bellflower Municipal Code (“BMC”).

“Director”: Director of Planning and Building Services, or designee.

“Façade”: The vertical surface of a building that is set parallel to a Frontage Line and facing a street.

“Frontage Line”: The property lines of a lot fronting a street or other public way, or a park, green, or paseo.

“Frontage Type”: These are prototypical arrangements of architectural and site design elements that illustrate ways in which the intended streetscape character can be achieved through design. Appropriate degrees of privacy for adjoining buildings must be considered.

“Live-Work Unit”: A dwelling unit that incorporates non-residential uses. Permitted non-residential live/work uses are restricted to permitted uses within the zone in which it is located. Live/Work uses must comply with all of the following:

- A.** Live/Work units must be internally accessible between the residential area and the non-residential area. The non-residential area must be directly accessible to a non-resident from the ground level via an entry/exit separated from a residential entry/exit;
- B.** An employee of the non-residential use must reside in the dwelling unit. However, the non-residential use may have employees that do not reside within the dwelling unit;
- C.** Live/Work uses must observe similar operational and delivery hours and walk-in/client visits as other permitted uses within the zone in which it is located.
- D.** Outdoor storage is not permitted;
- E.** Live/Work uses cannot store or generate hazardous materials or employ hazardous processes;
- F.** Noise, vibration, dust, odors, fumes, smoke, heat, electrical interference or other similar nuisances from Live/Work activities cannot be perceived beyond the individual unit; and
- G.** Live/Work activities cannot increase pedestrian or vehicle traffic beyond that ordinarily associated with the zone in which it is located nor can it reduce the number of required off-street parking spaces available for use.

“Mixed-Use”: Multiple land uses and activities within the same building or the same general area.

“Planning Commission”: The Planning Commission established by BMC Chapter 2.32. The Director may, however, recommend that the City Council act as the planning agency for a project by resolution as authorized by BMC § 2.32.050. Upon adoption of such a resolution, the City Council will act as the Planning Commission for purposes of this Specific Plan.

8.4 SPECIFIC PLAN AMENDMENTS

The Specific Plan provisions are intended to be flexible and responsive to the needs of the City and future commercial development within the Specific Plan area. Furthermore, the Project Review (PR) Process is intended to consolidate typical City review processes and provide the City with the opportunity to ensure future commercial development proposals comply with those development and design standards contained in this Specific Plan and other relevant requirements of the City. Therefore, separate design-related approvals are not generally required. The provisions of this Specific Plan also offer interpretive flexibility to the Director that should accommodate most future development.

It should be noted that this Specific Plan does provide relief for those future developments that deviate from those standards and provisions contained in this Specific Plan, including the following:

- A. Amendment to any portion of the Specific Plan as initiated by the City.
- B. Amendment to the text or maps.
- C. Amendment in any internal Specific Plan Area boundary.
- D. Amendment in standards or regulations, including landscaping and design standards.
- E. Amendment that would not have a substantial adverse impact on surrounding properties.
- F. Amendment that would not alter any findings contained in the environmental document prepared for the approval.
- G. Amendment that would not affect the ability to meet infrastructure and service provisions contained in the Specific Plan.
- H. Amendment to design criteria such as paving treatments, architectural details and related criteria.
- I. Amendment to landscape treatments, fencing, lighting, trails, and entry treatments, provided the modifications are in substantial conformance with the purpose and intent of the specified design criteria.
- J. Amendment to the Specific Plan that would not significantly affect the overall design and visual quality of the area including, without limitation, the following:

- » Building setback deviation – up to ten percent (10%);
- » Height of fence or wall deviation – up to ten percent (10%). In commercial zones, a modification of twenty percent (20%) may be allowed;
- » Projections into required setbacks – up to ten percent (10%);
- » Building height increase – up to ten percent (10%);
- » Reduction in off street parking requirement – up to ten percent (10%);
- » All other dimensional or percentage limitations or requirements – up to ten percent (10%).
- » Any other amendment deemed appropriate by the Director.

The Director may request additional information from the applicant in order to make the necessary findings to approve the amendment. In addition, the applicant would need to provide additional architectural enhancements or other design benefits which would provide the Director with incentive to allow the relaxation of any development standard. The Director must make the following findings (as applicable) to approve the amendment to this Specific Plan:

- A.** The adjustment(s) does not change the vision or policy intent of the Specific Plan (the Vision is located in Chapter 2 and policies are included in Chapters 3 and 5 of the Specific Plan);
- B.** The adjustment will not produce a result that is detrimental to the neighborhood.

The decision of the Director becomes final unless appealed pursuant to the provisions of BMC Chapter 17.112 (Appeals).

8.5 REVIEW AND APPROVAL AUTHORITY OF THE DIRECTOR

The Director has the authority to do the following:

- A.** Implement the Specific Plan in accordance with Applicable Law.
- B.** Interpret the Specific Plan. In the event that a specific use or type of use is not listed as a permitted use, the Director has authority to determine whether the proposed use is similar to a permitted use and whether the use is permitted, permitted subject to conditions, permitted as a temporary

use, or prohibited. In determining “similarity,” the Director must make all of the following findings:

- » The proposed use meets the intent of, and is consistent with, the goals, objectives and policies of the General Plan and the Specific Plan;
 - » The proposed use does not adversely impact the public health, safety and general welfare of the City’s residents; and
 - » The proposed use shares characteristics common with and is not of greater intensity or density or does not generate more environmental impacts than those uses listed in the permitted uses section.
- C.** Any applicant, interested person, or public official may appeal decisions of the Director to the Planning Commission in accordance with the Specific Plan or BMC, as applicable.
- D.** Make technical corrections to maps, diagrams, tables, and other similar documents that may be required to reconcile the changes made by the Specific Plan with any applicable law.
- E.** Make the following technical amendments:
- » Amendment to design criteria such as paving treatments, architectural details and related criteria.
 - » Amendment to landscape treatments, fencing, lighting, trails, and entry treatments, provided the modifications are in substantial conformance with the purpose and intent of the specified design criteria.
 - » Amendment to design criteria such as paving treatments, architectural details and related criteria.
 - » Amendment to landscape treatments, fencing, lighting, trails, and entry treatments, provided the modifications are in substantial conformance with the purpose and intent of the specified design criteria.
- F.** Administer, interpret, and enforce all requirements of this Specific Plan, including the acceptance and processing of all land use permit applications.
- G.** The Director can make a Parking Determination to identify the number and location of required parking spaces in compliance with the standards and guidelines contained in this Specific Plan.

- H.** Approve project applications. The project review process and procedures are described in the next section.
- I.** Approve proposed additions and enlargements to existing improvements. The approval process and procedures are described in the following section.

8.6 PROJECT REVIEW PROCESS AND PROCEDURES

All new development projects within Planning Areas 1-5, as shown in Figure 3.3, and being designed according to the overlay standards are required to undergo a project review process and require review by the Director. All projects designed according to the underlying zoning standards must comply with BMC Title 17. The project review process provides the City with the opportunity to ensure future commercial and residential mixed-use development proposals comply with those development and other applicable law. The following describes the PR process.

8.6.1 PROJECT REVIEW PROCEDURES AND SUBMITTAL REQUIREMENTS

The Director will review and confirm compliance of projects with this Specific Plan as follows:

- A.** Allowable uses and activities;
- B.** Site plans;
- C.** Building elevations/Architecture;
- D.** Landscaping plans;
- E.** Parking plans;
- F.** Floor plans;
- G.** Permanent and temporary uses and activities;
- H.** Hours and method of operations;
- I.** Business operation plan/description;
- J.** Photometrics Plan;
- K.** Master Sign Program;
- L.** Technical studies (i.e. parking, noise, etc.);
- M.** M. The Director can establish conditions of approval (as appropriate) with its approval.

To initiate the PR process, all project applicants must submit the following plans and materials to the Director. Plans and studies must be stamped and signed by a licensed professional. The Director must determine its completeness within 30 days. If complete, the Director will review and approve or deny the application. The Director's determination may be appealed to the Planning Commission.

- A.** Project Application
- B.** Scaled and dimensioned site plan;
- C.** Scaled and dimensioned elevations showing all building exteriors on all buildings;
- D.** Scaled and dimensioned floor plans and building cross sections (if determined necessary by the Director);
- E.** Grading plan (including drainage scheme), prepared by a licensed civil engineer;
- F.** Complete inventory of building materials and color palette;
- G.** Scaled and dimensioned landscape plan (including decorative hardscape);
- H.** Tentative Maps (if determined necessary by the Director);
- I.** Preliminary Title Reports (if determined necessary by the Director);
- J.** Any detail drawings and exhibits as necessary to provide information on various project features that require specific and detailed review. Such detail drawings may include the location and design menu of signage, the project entrance and the community recreation area;
- K.** Photometrics Plan (if determined necessary by the Director);
- L.** Description of operations (if determined necessary by the Director);
- M.** Studies (e.g. Parking) (if determined necessary by the Director).

If deemed appropriate, the Director has the ability to refer approval authority, on a case-by-case basis, to the Planning Commission and/or City Council, without the necessity of any public hearing.

8.6.2 REQUIRED FINDINGS

The Director will approve the Project Review application, plans, building materials, architectural design and landscaping/hardscape design exhibits, provided the following findings are made:

- A.** The project is consistent with the provisions of this Specific Plan.
- B.** The project's design and building architecture is complimentary and compatible with other projects within the Specific Plan area.
- C.** The overall project design and architecture are of high quality and innovative design, use quality construction materials, and creatively use landscape and hardscape materials in order to create a "signature" development for the City of Bellflower.
- D.** The project is consistent with the Bellflower General Plan.

8.7 PROVISIONS FOR EXISTING IMPROVEMENTS AND ADDITIONS

Existing permitted improvements built in conformance to all building and zoning codes in effect at the time of construction may be maintained and repaired as currently existing and approved administratively by the Director, pursuant to the following provisions:

- A.** Such existing improvements and site conditions may be maintained, repainted, repaired, and landscaping upgraded without having to conform to the provisions of this Specific Plan.
- B.** Review and approval of such repairs and similar actions is required pursuant to the provisions of this Specific Plan.
- C.** All proposed additions, enlargements and improvements must conform to those development standards and design guidelines described in the Specific Plan; except additions proposed under the underlying zones must comply with the BMC.

8.8 ENVIRONMENTAL REVIEW

At the time of the writing of this Specific Plan, there is no actual, physical development being proposed, nor are any new construction, grading, or other physical alterations to the environment being considered. The Specific Plan is a zoning-level Specific Plan document to guide and regulate future development around a future transit station and is viewed as a regulatory document to ensure practical and higher quality future development in the Specific Plan area. The Director will review all future development proposals on a case-by-case basis and will make applicable findings to ensure that all future development proposals are consistent with those standards and requirements contained in this Specific Plan document. As part of City's review, additional CEQA documentation may be required for each future development proposal, if deemed necessary and appropriate.

8.9 IMPLEMENTATION ACTION PLAN

The following Implementation Action Plan lists the specific actions or strategies that should be taken by the City of Bellflower, in coordination with local businesses, future developers, and other agencies where appropriate. Programs and policies for some of these items are already in place and are recommended to be continued within this Study Area. The Implementation Action Plan is organized by the following topic areas:

- A.** Economic Development Strategies and Initiatives
- B.** Mobility and Streetscape Actions
- C.** Parking Actions
- D.** Housing Initiatives

For each action there is a recommended timeframe for completion, the responsible party or entity, and potential funding source(s). The timeframes are identified as follows:

- » Short (1-5 years)
- » Medium (5-10 years)
- » Long (10 years or more)

Ongoing actions have also been identified. Actual implementation will be dependent on development activity, funding availability, and the Director resources.

TABLE 8.1 Implementation Action Plan

SPECIFIC ACTIONS	TIMEFRAME	PRIMARY RESPONSIBILITY	POTENTIAL FUNDING SOURCES
A. Commercial Uses			
A.1 Complete construction of the new parking garage on Bellflower Boulevard.	Ongoing, expected completion mid-2020	Public Works Department	Funding secured
A.2 Promote redevelopment of key parcels within the Study Area among developers, commercial brokers, and the business community to spur additional investment in older commercial properties.	Ongoing	Economic Development Department	City
A.3 Encourage and facilitate new dining and retail options by aiding in site location, tenant improvements, and permit processing.	Ongoing	Economic Development Department	City
A.4 Work with landlords of vacant properties to make necessary upgrades through the Business Assistance Program.	Ongoing	Economic Development Department	City
A.5 Coordinate with Metro on the final location, design, and construction of the transit station to achieve the urban design policies under Chapter 5, Objective 5.	Ongoing	Planning Department, Economic Development Department, and Public Works Department	City
A.6 Establish a Business Improvement District for the downtown and broader area to provide a Clean & Safe Program, Events, and Downtown Promotion. The BID requires approval by the affected property owners of the downtown businesses.	Ongoing, with goal of establishing it by FY2019-2020	Economic Development Department	City
A.7 Work with the Southeast Los Angeles County Workforce Investment Board to aid with job placement, job training, and other services to better align the local workforce with employer needs in Bellflower.	Ongoing	Economic Development Department	City
B. Mobility and Streetscape Actions			
B.1 Extend Bellflower Boulevard streetscape improvements from Mayne Street north to Alondra Boulevard.	Medium	Public Works Department	City, Private Development, Metro

TABLE 8.1 Implementation Action Plan

SPECIFIC ACTIONS	TIMEFRAME	PRIMARY RESPONSIBILITY	POTENTIAL FUNDING SOURCES
B.2 Extend Bellflower Boulevard streetscape improvements from Flower Street south to Park Street.	Medium	Public Works Department	City, Private Development, Metro
B.3 Encourage and facilitate the installation of bicycle facilities.	Medium	Public Works Department	City, Caltrans, Metro
B.4 Enhance crosswalks to improve pedestrian access within the Specific Plan area.	Medium	Public Works Department	City, Caltrans, Metro
B.5 Design and construct gateway signage at Bellflower Boulevard and Alondra Boulevard to direct people to the transit station and downtown.	Medium	Public Works Department	City, Private Development, Metro
B.6 Prepare a streetscape improvement plan along Alondra Boulevard to enhance the identify of Bellflower and call attention to the downtown.	Medium	Public Works Department	City, Private Development, Metro
C. Parking Actions			
C.1 Manage curbside parking along Bellflower Boulevard by establishing time limits to increase turnover and convenience to users. Enforce parking limits.	Short	Public Works Department, Planning Department, and Public Safety	City
C.2 Establish passenger loading areas along Bellflower Boulevard or within the vicinity to accommodate increased demand for ride-hailing services.	Medium	Public Works Department	City, Private Development
C.3 Adopt a parking occupancy target for public parking facilities to ensure adequate parking availability for visitors, residents and employees.	Short	City Council	
C.4 Establish a system to monitor parking occupancy regularly and recommend changes (if necessary) to managing parking supply	Medium	Public Works Department and Public Safety	City
C.5 Consider expanding the existing parking district as the opportunity arises through new development, in order to continue to promote shared parking within the area.	Long	Public Works Department	City
C.6 Coordinate with Metro to balance the parking demands of the transit users with residents and visitors in the Specific Plan area. Look for opportunities to share parking at the proposed transit station.	Short	Public Works Department and Planning Department	City

TABLE 8.1 Implementation Action Plan

SPECIFIC ACTIONS	TIMEFRAME	PRIMARY RESPONSIBILITY	POTENTIAL FUNDING SOURCES
C.7 Improve signage for off-street public parking locations and the parking structure at the Civic Center.	Long	Public Works Department	City, Metro
C.8 Continue the valet program for key locations in the downtown to address parking constraints at peak hours.	Ongoing	Economic Development and Public Works Department	City
D. Housing Initiatives			
D.1 Study inclusionary zoning best practices to determine affordable housing policies for multifamily housing projects.	Short	Planning Department and Economic Development Department	City, AB32, SB2
D.2 Study and adopt strategies to reduce and prevent homelessness in the Specific Plan area.	Ongoing	Economic Development Department, Public Safety Department, Planning Department, and Public Works Department	City, AB32, SB2
D.3 Study existing density bonus regulations to determine effectiveness and recommend updates.	Short	Planning Department and Economic Development Department	City, AB32, SB2
D.4 Identify grant application priorities to fund and support the development of affordable housing.	Ongoing	Economic Development Department	City, AB32, SB2
D.5 Identify and coordinate with affordable housing developers.	Ongoing	Economic Development Department	City, AB32, SB2

8.10 POTENTIAL FUNDING MECHANISMS

There are a number of grants, loans, and value capture funding mechanisms that could finance the infrastructure and community benefits identified in this Specific Plan. These resources are described below.

8.10.1 LOCAL TAX INCREMENT AND ASSESSMENT DISTRICTS

Enhanced Infrastructure Financing District

The Enhanced Infrastructure Financing District (EIFD) is a funding mechanism whose primary purpose is to finance a wide array of infrastructure projects with “communitywide significance,” including transit improvements and affordable housing. EIFDs are a California state legislation signed into law in September 2014 and further refined as a tool in 2015.

An EIFD can be established without voter approval by a city, county, or joint powers authority to fund specific infrastructure and economic development projects as outlined in the financing plan. EIFDs can leverage multiple funding streams to achieve these goals—including tax increment financing, if approved by voters, assessment revenues, fees, and other sources such as state and federal grants. EIFDs may not issue debt without a 55 percent vote of the district’s registered voters, nor can revenues be used to fund ongoing maintenance and operations. An EIFD can be established without geographic limitations on where it can be used, therefore it could be employed on a single corridor, such as Flower Street, around the new Bellflower Station, or throughout the whole Specific Plan area.

Revenues could be applied to a number of improvements including transit-oriented development, development that carries out sustainable communities’ strategies, affordable housing, and civic projects such as parks and libraries. EIFDs can furthermore finance infrastructure work including pedestrian connectivity, streetscape improvements, water and sewer infrastructure, and parking facilities.

Community Revitalization Investment Authorities

Community Revitalization Investment Authorities (CRIAs)—another funding mechanism recently passed in California to help carry out revitalization activities—share a number of similarities with EIFDs. CRIAs use tax increment to implement an economic revitalization financing plan within a defined area to construct, improve, or rehabilitate specified projects with community-wide benefits.

CRIAs may be established without voter approval after three public hearings with public support. It is governed by a board of appointed public and government representatives who can issue bonds without voter approval. A CRIA must operate within an investment area that meets the state's criteria of a disadvantaged community based on socioeconomic, public health, and/or environmental factors. The 2017 SCAG EIFD/CRIA Technical Assistance Application indicates that all or most of the Specific Plan area is eligible.

Financing can be used to fund public or private revitalization projects including infrastructure construction and repair, owner or tenant improvement loans, and affordable housing. CRIAs have an affordable housing set-aside: 25% of tax increment revenues must be used for very low, low, and moderate-income housing.

Special Assessment Districts

Special Assessment Districts can be used to fund any improvement that provides a "direct and special" benefit to the assessed property. Each property owner who receives a benefit pays a share of the costs of the improvements over a period of years. Improvements like the recommended sidewalks enhancements, street trees, lighting, art, and benches that improve connectivity, as well as safety improvements like private security, can be funded via Special Assessment, while "general" benefits like schools may not.

There are two primary challenges in establishing Special Assessment Districts, particularly for those in already developed areas. The first is that total property taxes can only increase a certain amount before new development is disadvantaged relative to properties not subject to an assessment. The second challenge is that assessment districts require a majority vote of property owners weighted by property value to pass. All the affected properties must stand to benefit from that particular improvement, and no assessment can exceed the "reasonable cost" of its special benefit to that parcel.

Business Improvement District

A Business Improvement District (BID) is a common type of Special Assessment District that assesses business and/or property owners to fund maintenance, marketing, and other activities, including additional public services or improvements. A BID is a powerful tool for raising funds to provide enhancements to the area, but cannot be used to issue bonds.

Under the California Parking and Business Improvement Area Law of 1989 and Property and Business Improvement District Law of 1994, a district can be established via a County resolution of intent to form a BID. If a majority of property owners do not protest the resolution during a subsequent public hearing, an advisory board would be appointed. Once formed, a special assessment can be charged to commercial property or business owners for an amount proportional to the benefits they will receive.

The City and Downtown Bellflower business and property owners are in the process of forming two BIDs in the Specific Plan area along Bellflower Boulevard: one located north of Flower Street to Alondra Boulevard; and the other located south of Flower Street to Rose Street. Funding raised could be applied toward enhanced sanitation and cleaning as well as other streetscape and pedestrian improvements.

Landscape and Lighting Assessment District

Like a BID, a Landscape and Lighting Assessment District (LLAD) is another type of Special Assessment District that could be applied to fund new street lights and pedestrian lights, landscaping, parkways, medians, and other amenities. An LLAD requires that benefits accrue proportionately to the assessed properties. LLADs are more flexible than BIDs in that they can issue bonds and require a simple majority of property owners for formation. Commercial corridors within the Specific Plan area may consider LLADs to fund area-wide lighting and streetscape improvements.

8.10.2 REGIONAL AND STATE SOURCES OF FUNDS

Affordable Housing and Sustainable Communities

The 2006 Global Warming Solutions Acts (AB 32) established a cap and trade system in California. The system establishes quarterly auctions of carbon allowances, whose proceeds are deposited into a Greenhouse Gas Reduction Fund. Using revenue from this fund, the Strategic Growth Council administers the Affordable Housing and Sustainable Communities (AHSC) program, which funds land-use, housing, transportation, and land preservation projects to support infill and compact development that reduces greenhouse gas emissions.

Approximately \$395 million in AHSC funding was announced in FY 2017–18; recipients will be announced in Summer 2019. Potential projects could include installation of new or improved pedestrian crossings, walkways, bikeways, or bus lanes, streetscape improvements, including, but not limited to the installation of lighting, signage, urban greening, street furniture, or other related

amenities for pedestrians, cyclists and transit riders, traffic calming projects. Funding can also be used for the acquisition and rehabilitation of affordable housing, or the conversion of nonresidential structures to residential dwelling units.

Projects that can show the Strategic Growth Council that they reduce vehicle miles traveled by locating near transit are most competitive for funds. However, the market for carbon emissions has shown itself to be relatively unstable. Rather than trading emission allowances, companies are reducing emissions. While this is certainly beneficial to the environment, it means that the future of this funding source is uncertain.

Infrastructure State Revolving Loan Fund

The California Infrastructure and Economic Development Bank (I-Bank) loans money at below-market interest rates for infrastructure projects around the state. The I-Bank is the state's general-purpose financing authority that finances public infrastructure and private development projects that promote economic development and revitalize communities.

Eligible project categories in Bellflower include the rehabilitation of streets, water supply and flood control, expanded public transit, and power and communications facilities. Categories also include new educational, cultural, or social facilities, as well as parks and recreational facilities.

Recent loan recipients in Southern California have included the City of San Gabriel, which borrowed \$3.8 million at 3.5 percent interest to upgrade, reconstruct, and rehabilitate its public streets.

SB2 Grants

The 2017 Building Homes and Jobs Act (SB2), managed by the California Department of Housing and Community Development (HCD), provides funding and technical assistance to all local governments in California to help cities and counties prepare, adopt, and implement plans and process improvements that streamline housing approvals and accelerate housing production. Funding will help cities accelerate housing production, streamline the approval of housing development affordable to owner and renter households at all income levels, facilitate housing affordability, particularly for lower- and moderate-income households, promote development consistent with the State Planning Priorities (Government Code Section 65041.1). Additionally, a Technical Assistance team is available to help jurisdictions identify activities and provide best-practice tools to help reach the goal of accelerating housing production. For

assistance, jurisdictions can reach out to Housing and Community Development (HCD), Office of Planning and Research (OPR), or the regional liaison for the Los Angeles County Region.

The program provides grants through a noncompetitive, over-the-counter process to eligible local governments who have an HCD-compliant housing element and submitted a recent Annual Progress report. Applications must demonstrate a nexus to accelerating housing production and demonstrate that it is consistent with State Planning Priorities or other planning priorities. Eligible project categories include local process improvements that expedite local planning and permitting, environmental analyses that eliminate the need for project-specific review, housing related infrastructure financing and fee reduction strategies, such as infrastructure financing districts, and approaches that encourage accessory dwelling units (ADUs) such as outreach, fee waivers, pre-approved plans, website zoning clearance assistance, and other homeowner tools or finance tools

8.10.3 FEDERAL SOURCES

Economic Adjustment/Revolving Loan Fund

The Economic Development Administration, a bureau in the U.S. Department of Commerce, administers the Economic Adjustment/Revolving Loan Fund (RLF), which assists state and local entities in creating and implementing strategies to improve local economic conditions in areas that have experienced structural change in their economic bases.

The RLF provides capital to help small businesses and entrepreneurs expand production capabilities with gap financing. Businesses in Bellflower looking for assistance would contact the local EDA Regional Office (Pacific Coast Regional) with information on interest rates. Maximum loans are \$650,000 per borrower with terms of seven years for working capital, 15 years for fixed assets, and 20 years for real estate. Start-up firms, as well as those looking to expand, could benefit from use of this loan program.



