

**Board of
Commissioners Meeting
November 19, 2016**

PRELIMINARY AGENDA

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FLORIDA INLAND NAVIGATION DISTRICT Board of Commissioners Meeting

9:00 a.m., Saturday, November 19, 2016

**Hutchinson Island Marriott Beach Resort & Marina
555 NE Ocean Boulevard
Stuart (Martin County), Florida 34996-1620**

Item 1. Call to Order.

Chair Cuozzo will call the meeting to order.

Item 2. Pledge of Allegiance.

Chair Cuozzo will lead the Pledge of Allegiance to the United States of America.

Item 3. Roll Call.

Secretary Donaldson will call the roll.

Item 4. Consent Agenda.

The consent agenda items are presented for approval. Commissioners may remove any items from this agenda that they have questions on or would like the Board to discuss in depth. Any items removed would then be included in the regular agenda in an order assigned by the Chair.

(Please see back up pages following the **COLOR** page)

- a) Volusia County Small-Scale Derelict Vessel Removal Program Application, Volusia County, FL.

RECOMMEND: Approval of the Consent Agenda.

Item 5. Additions or Deletions.

Any additions or deletions to the meeting agenda will be announced.

RECOMMEND: Approval of a Final Agenda.

Item 6. Public Comments.

The public is invited to provide comments on issues that are NOT on today's agenda. All comments regarding a specific agenda item will be considered following Board discussion of that agenda item.

Please note: Individuals who have comments concerning a specific agenda item should fill out a speaker card and communicate with staff prior to that agenda item.

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Item 7. Board Meeting Minutes.

The minutes of the following meetings are presented for approval.

- October 14, 2016 – Finance & Budget Committee Mtg. (Pls see back up pages 6-10)
- October 14, 2016 – Board Meeting (Please see back up pages 11-38)

RECOMMEND: Approval of the minutes as presented.

Item 8. Staff Report on Martin County Area Projects.

Staff will present a report on the District's Martin County area projects. (Please see back up pages 39-65)

Item 9. Comments from the U.S. Army Corps of Engineers.

U.S. Army Corps of Engineers (USACE) Intracoastal Waterway Project Manager, Ms. Shelley Trulock, is scheduled to present an update on projects and activities.

(Please see back up pages 66-72)

Item 10. Presentation and Update on the Okeechobee Waterway and Crossroads Channel Widener and Realignment Effort.

Dr. Mike Kabiling with Taylor Engineering is scheduled to provide an update on the Okeechobee Waterway (OWW) channel widener (advanced maintenance basin) in the Crossroads area. Where the Intracoastal Waterway (IWW), the St. Lucie Inlet, and the Okeechobee Waterway (OWW) converge in Martin County. This effort also looked at realignment of the existing OWW channel to naturally occurring deeper water. This area is one of the District's most frequently dredged areas. The establishment of the widener will reduce the District's dredging frequency from its current three (3) year dredging cycle and allow for potential costs savings.

RECOMMEND: (This item is presented for Board review and discussion only.)

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Item 11. Scope of Services and Fee Proposal for Final Design and Bid Administration for Crossroads Maintenance Dredging, Martin County, FL.

Design and permitting of the Okeechobee Waterway (OWW) Channel Widener is completing, and the Crossroads area (confluence of the OWW, the Intracoastal Waterway (IWW) and the St. Lucie Inlet) is scheduled to be dredged. Taylor Engineering has submitted a scope and fee quote in the amount of \$39,357.60 to finalize the design of this project, including the dredging of the advance maintenance area (new channel widener) and bid administration assistance.

Capacity is sufficient for this dredging event to utilize Dredged Material Management Area (DMMA) M-5. The proposal has been reviewed and is consistent with the previous work efforts and is necessary to continue this project.

(Please see back up page 73-78)

RECOMMEND: Approval of a proposal and fee quote from Taylor Engineering in the amount of \$39,357.60 for final design and bid administration services, Crossroads Maintenance Dredging, Martin County, FL.

Item 12. Presentation – Intracoastal Waterway Setback Policy Update.

Ms. Tori White, Deputy Chief, Regulatory, USACE Jacksonville District, has been invited to provide an update to the Board on issues and potential modifications to the Intracoastal Waterway Setback Policy.

(Please see back up pages 79-91)

RECOMMEND: (This item presented for informational purposes and Board discussion only.)

Item 13. Scope of Services and Fee Proposal for Engineering Services for Maintenance Dredging of the Intracoastal Waterway in the Vicinity of Jupiter Inlet, Palm Beach County, FL.

At the October 2016 Board meeting, the Navigation District (District) approved an Interlocal Agreement with the Jupiter Inlet District (JID) to share costs associated with maintenance dredging near the Jupiter Inlet.

The JID dredges the Jupiter Inlet basin approximately each year. The District maintenance dredges the Intracoastal Waterway (IWW) in this vicinity approximately every three years. The approved agreement affords both agencies costs savings and will allow more material to be placed on the nearby beach.

The District's proportional cost-share for this effort is \$51,500.00. This includes construction plans, specifications, inter-agency coordination, bid assistance and construction phase services. Staff has reviewed the submitted information and found it to be consistent and reasonable for this work.

(Please see back up pages 92-102)

RECOMMEND: Approval of scope and fee quote from Taylor Engineering in the amount of \$51,500.00 to for engineering services associated with the maintenance dredging of the Intracoastal Waterway in the vicinity of the Jupiter, Inlet, Palm Beach County, FL.

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Item 14. **Interlocal Agreement with the St. Augustine Port, Waterway and Beach District for Material Removal at Dredged Material Management Area SJ-1, St. Johns County, FL.**

Following the recent impacts of Hurricane Matthew, the St. Augustine Port, Waterway and Beach District (SAPWBD) has request an Interlocal Agreement with the Navigation District to remove material from Dredged Material Management Area (DMMA) SJ-1 near Matanzas Inlet. The SAPWBD has estimated the removal of up to 50,000 cubic yards of suitable material to be utilized for beach erosion mitigation, at no cost to the District. The District has also requested some minor repairs to the site upon project completion.

(Please see back up pages 103-110)

RECOMMEND: Approval of an Interlocal Agreement with the St. Augustine Port, Waterway and Beach District for material removal at DMMA SJ-1, St. Johns County, FL.

Item 15. **Review of the Navigation District's Proposed 5-Year Dredging and Dredged Material Management Area (DMMA) Plan.**

Staff has updated the District's 5-year plan for dredging and DMMA construction. This document is intended to be a "living" planning document that will be utilized to schedule future District projects. As situations change and opportunities are repositioned, the scheduling of these projects may shift significantly. However, this information provides a basis to plan and finance future waterway improvements.

(Please see back up pages 111-113)

RECOMMEND: *(This item is presented for Board review and discussion only.)*

Item 16. **Finance and Budget Committee Report.**

The District's Finance and Budget Committee met prior to the Board meeting and will provide their recommendations concerning items on the Committee's agenda.

(Please refer to the Finance and Budget Committee Agenda Package)

RECOMMEND: Approval of the recommendations of the District's Finance and Budget Committee.

Item 17. **Personnel Committee Report.**

The District's Personnel Committee met prior to the Board meeting and will provide their recommendations concerning items on the Committee's agenda.

(Please refer to the Finance and Budget Committee Agenda Package)

RECOMMEND: Approval of the recommendations of the District's Personnel Committee.

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Item 18. **Washington D.C. Report.**

The District's federal governmental relations firm has submitted a status report concerning activity on the District's federal issues.

(Please see back up pages 114-116)

RECOMMEND: *(This item is presented for Board review and discussion only.)*

Item 19. **Additional Staff Comments and Additional Agenda Items.**

- a) Re-scheduled inspection tour of the Okeechobee Waterway.
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Item 20. **Additional Commissioners Comments.**

Item 21. **Adjournment.**

If a person decides to appeal any decision made by the board, agency, or commission with respect to any matter considered at such meeting or hearing, he or she will need a record of the proceedings, and that, for such purpose, he or she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

**MINUTES OF THE
FLORIDA INLAND NAVIGATION DISTRICT**

Finance and Budget Committee Meeting

8:45 a.m., Friday, October 14, 2016

Marriott Hotel Pompano Beach

1200 North Ocean Boulevard

Pompano Beach, Broward County, Florida 33062-4061

ITEM 1. Call to Order.

Acting-Committee Chair Sansom called the meeting to order at 8:39 a.m.

ITEM 2. Roll Call.

Assistance Executive Director Janet Zimmerman called the roll and Acting-Committee Chair Sansom, Secretary Donaldson and Commissioner Blow were present. Committee Chair McCabe was absent. Ms. Zimmerman stated that a quorum was present.

ITEM 3. Additions or Deletions.

Acting-Committee Chair Sansom asked if there were any additions or deletions to the meeting agenda. Mr. Crosley stated that there were no additions or deletions to the agenda.

Commissioner Blow made a motion to approve the agenda as presented. The motion was seconded by Secretary Donaldson. Acting-Committee Chair Sansom asked for any further discussion, hearing none, a vote was taken and the motion passed unanimously.

ITEM 4. Public Comments.

Acting-Committee Chair Sansom asked if there were any public comments on issues that are not on today's agenda. There were none.

ITEM 5. Financial Statements for August of 2016.

Mr. Crosley presented the District's financial statements for August of 2016.

Mr. Crosley noted that the Fidelity Bank CD that matured in September was transferred into the District's Seacoast Checking account. This funding was partially used to pay end-of-year grant disbursements.

Mr. Crosley stated that past due Tax Certificates are being sold. As funds are collected by the Tax Collectors, they will be disbursed to the agencies. Miscellaneous revenue collected in the amount of \$81,589.87 reflects \$30,000.00 of project funding refunded by the U. S. Army Corps of Engineers (USACE), payments received for offloading DMMA SJ-14, and revenue collected from miscellaneous leases.

Mr. Crosley stated that the District's current expenses as of August 31, 2016 are \$30.4 million. That amount is more than usual and will continue to increase. The District has spent \$7.7 million more this Fiscal Year than revenue collected, that was expected and planned and includes the Broward deepening project. An additional \$2.2 million payment has recently been approved for this project and will be reflected in the October 2016 Balance Sheet. The dredging of the Ponce Inlet project will begin soon and may cost more than \$14 million. He asked for questions. There were none.

Commissioner Blow made a motion to approve a recommendation to the full Board of the financial statements for August of 2016. The motion was seconded by Commissioner Donaldson. Acting-Committee Chair Sansom asked for any additional discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 6. August of 2016 Budget Summary and Project Status Expenditure Reports.

Mr. Crosley presented the Budget Summary and Project Status Expenditure Reports for August of 2016.

Mr. Crosley stated that the remaining expenditures are within the District's budget. He asked for questions.

Acting Committee Chair Sansom noted that the District has \$1million budgeted in the Disaster Relief Account.

Mr. Crosley stated that \$1 million in Disaster Relief funding may not be enough to assist District counties affected by Hurricane Matthew. Staff has received numerous inquiries requesting funding help for cleanup.

Mr. Crosley stated that if necessary, staff could delay the construction of DMMA BV-4B until October of 2017.

Mr. Crosley stated that there will be an agenda Item added to today's Board Meeting to discuss storm impact.

ITEM 7. Auditor's Engagement Letter for the FY 2015-2016 Audit.

Mr. Crosley stated that the District's current audit firm, Berger, Toombs, Elam, Gaines & Frank, has submitted an engagement letter for the FY 2015-2016 financial audit, including a proposed cost of \$27,000.00, which is the same as last year's proposal. If the engagement letter is approved, this will be the eighth year that Berger, et al will have performed the District's annual audit. Last year, the Principal/Share Holder was rotated.

Mr. Crosley noted, that for publicly traded firms, the Sarbanes-Oxley Act requires that the audit shareholder/partner be rotated every 5 years. It does not require that the firm

be rotated. Currently, the Governmental Standards Accounting Board has no such requirement.

Mr. Crosley stated that if the Board does not approve the engagement letter, the Auditor Selection Committee will need to advertise a Request for Qualifications for a new audit firm. By state law, audit firms are selected by their qualifications and their fees are then negotiated.

Commissioner Blow made a motion to approve a recommendation to the full Board to accept the Engagement Letter from Berger, Toombs, Elam, Gaines & Frank for the FY 2015-2016 audit, with rotation of the audit shareholder/partner. The motion was seconded by Secretary Donaldson. Acting Committee Chair Sansom asked for any additional discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 8. Delegation of Authority Report.

Mr. Crosley presented the Executive Director's Delegation of Authority Report and stated that six (6) actions were taken from August 30, 2016 to October 4, 2016.

Mr. Crosley stated that staff has approved an agreement with Revize Web Design to update and re-organize the District's Web Site. He asked for questions.

Commissioner Donaldson asked when the District's Web Site project will be completed. Mr. Crosley stated in six (6) months or less.

ITEM 9. Additional Agenda Items or Staff Comments.

Acting-Committee Chair Sansom asked if there were any additional agenda items or staff comments. There were none.

ITEM 10. Additional Commissioners Comments.

Acting-Committee Chair Sansom asked if there were any additional Commissioner comments. There were none.

ITEM 11. Adjournment.

Acting-Committee Chair Sansom stated that hearing no further business the meeting was adjourned at 8:58 a.m.

**MINUTES OF THE
FLORIDA INLAND NAVIGATION DISTRICT**

Board of Commissioners Meeting

9:00 a.m., Friday, October 14, 2016

Marriott Hotel Pompano Beach

1200 North Ocean Boulevard

Pompano Beach, Broward County, Florida 33062-4061

ITEM 1. Call to Order.

Chair Cuozzo called the meeting to order at 9:00 a.m.

ITEM 2. Pledge of Allegiance.

Commissioner Chappell led the Pledge of Allegiance to the Flag of the United States of America.

ITEM 3. Roll Call.

Secretary Donaldson called the roll and Chair Cuozzo, and Commissioners Blow, Chappell, Crowley, Sansom, and Williams were present. Vice-Chair Netts and Commissioner O'Steen attended by telephone. Treasurer McCabe, Commissioner Dritenbas and Commissioner Isiminger were absent. Secretary Donaldson stated that a quorum was present.

ITEM 4. Consent Agenda.

Chair Cuozzo asked if there were any comments or questions regarding the Consent Agenda.

Mr. Crosley stated that the recommendation for Item B. should read; Approval of the “Palm Beach County” Small Scale Derelict Vessel Removal Application in the amount of \$30,000.00.

Commissioner Blow made a motion to approve the Consent Agenda as amended. The motion was seconded by Commissioner Sansom. Chair Cuzzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 5. Additions or Deletions.

Chair Cuzzo asked if there were any additions or deletions to the meeting agenda.

Mr. Crosley stated that he would like to add to the agenda: Item 16A, Storm Damage Discussion; and Item 16B, Request to Use District Site DMMA BV-11 for a Storm Staging Area, Brevard County.

Commissioner Sansom made a motion to approve the final agenda as amended. The motion was seconded by Secretary Donaldson. Chair Cuzzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 6. Public Comments.

Chair Cuzzo asked if there were any public comments on issues that are not on today’s agenda. There were none.

ITEM 7. Board Meeting Minutes.

Chair Cuzzo asked if there were any comments or questions regarding the September 8, 2016 First Budget and Tax Hearing Meeting Minutes, September 9, 2016 Finance and Budget Committee Minutes and the Board Meeting Minutes and the September 28, 2016 Final Budget and Tax Hearing Meeting Minutes. There were none.

Mr. Crosley stated that Commissioner O'Steen filled out and signed Form 8B, Memorandum of Voting Conflict for County, Municipal, and Other Local Public Officers, for: Item 16, Engineering Services Construction Observation for Pipeline Easement Modifications at Dredged Material Management Area SJ-14, St. Johns County, Florida; and, Item 20A, License Agreement to Provide Temporary Truck Access Through Dredged Material Management Area SJ-14, St. Johns County, Florida. The completed Form 8B will be attached to and become part of the official September 9, 2016 Board Meeting Minutes.

Secretary Donaldson made a motion to approve the September 8, September 9, and September 28, 2016 Meeting Minutes, as amended to include attaching Form 8B Memorandum of Voting Conflict for County, Municipal, and Other Local Public Officers to the Minutes. The motion was seconded by Commissioner Blow. Chair Cuozzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 8. Staff Report on Broward County Area Projects.

Mr. Crosley stated that Phase I of the Dredged Material Management Plan (DMMP) for the Intracoastal Waterway in Broward County was completed in 2003. Phase II of the DMMP was completed in 2004.

Mr. Crosley stated that the 50-year dredging projection for the 25 miles of channel in Broward County is 33,644 cubic yards and the storage projection is 72,334 cubic yards. This is the lowest dredging projection of any of the District's twelve (12) counties. The majority of this dredging (81%) is associated with the Hillsboro Inlet area.

Mr. Crosley stated that three upland Dredged Material Management Areas (DMMA) and one beach placement area will manage dredged materials from the waterway.

A western long-term storage site was suggested in the original DMMP, but this has not been realized because of costs and logistics. Land acquisition possibilities, especially for waterway access, continue to be a challenge in this county. However, the District remains open to future acquisition opportunities through exchange or at a reasonable cost.

Mr. Crosley stated that the existing District-owned DMMA's are currently leased to the City of Pompano Beach, MSA 726 (aka: Exchange Club Park); and MSA 727 (aka: Alsdorf Park 14th Street Boat Ramp); and Broward County, MSA 783 for parks, a boat ramp and port facilities, respectively. Through a series of public meetings, the District notified and coordinated with the citizens and officials of the City of Pompano Beach and City of Lighthouse Point regarding the clearing of invasive plants from MSA 726, the planting of a native plant buffer along the site perimeter, and the preparation of the site to support the forthcoming dredging of Broward Reach I and future dredging or access needs.

Mr. Crosley stated that the District is currently deepening the Intracoastal Waterway (IWW) from the 17th Street Causeway north past the Las Olas Bridge. The project is projected to be completed by December 2016 (more than six (6) months early). Funding for this \$19 million project was accumulated in the District's budget over three (3) fiscal years and distributed over two (2) fiscal years. This project will create navigation and docking opportunities for deep draft and larger vessels, and result in an increase in marine-related business. This economic benefit has been successfully demonstrated when the District completed the deepening of the Dania Cut-Off Canal between the Port and US Route 1. The completion of that project, with a project depth of -15' MLW, lead to the stimulus of additional marine-related business on the waterway. The Broward County IWW Deepening project is expected to result is similar marine-related benefits. Both

projects are cooperative efforts with the Marine Industries Association of South Florida, Broward County (including Port Everglades), and the cities of Fort Lauderdale and Dania Beach.

Mr. Crosley stated that additionally, maintenance dredging of Broward Reach I has currently been scheduled for 2017 with beach-quality dredged material being placed on the beach south of the Hillsboro Inlet. Any non-compatible material would be temporarily placed on MSA 726 for removal. This project may be completed by a USACE Hopper Dredge.

Mr. Crosley stated that the Broward County Waterways Economic Study was completed in early 2008 and updated in 2011. The study documented 1,767 marine-related businesses in the county employing approximately 21,455 people, with salaries of approximately \$1 billion and a total economic impact of \$4.5 billion. Property values were determined to be increased by \$6-7.2 billion by the presence of the IWW channel.

Mr. Crosley stated since 1986, the District has provided \$26.1 million in Waterways Assistance Program funding to 121 projects in the county having a total constructed value of \$62.8 million. The county, ten cities, and the Hillsboro Inlet District have participated in the program.

Mr. Crosley stated that notable projects funded include: The Fort Lauderdale Riverwalk, construction or rehabilitation of most of the area's saltwater boat ramps, maintenance dredging of the Dania Cut-Off Canal and the New River System, construction and improvements to the Dania Beach, Hollywood, Birch/Las Olas, Hillsboro Inlet and Cooley's Landing marinas, West Lake Park projects, and improvements to the Hillsboro Inlet channel.

Mr. Crosley stated that the District's Cooperative Assistance Program has provided funding assistance for the following projects with elements in Broward County: Florida Marine Patrol Officer Funding; Clean Marina Program, Clean Vessel Act, Manatee Acoustic Warning System, Hillsboro Canal Bank Stabilization, and Phase I of the Hugh Taylor Birch State Park Boat Dock and Education Center project. The District's funding assistance for the Broward County portion of these 19 individual projects was approximately \$2.9 million.

Mr. Crosley stated that the District's Interlocal Agreement Program has developed the following projects with elements in Broward County: Dania Canal Deepening, New River Deepening, Clean Marina Program; Clean Vessel Act, the installation and maintenance of Broward County Boating Safety Signage, the Demonstration of a Manatee Acoustic Warning System, and Phase I of basin dredging for the Bahia Mar and Las Olas Marinas.

Mr. Crosley stated that the District has partnered with the Marine Industries Association of South Florida for over 23 years on their waterway cleanup. The District provides up to \$10,000 per year for this program.

Mr. Crosley stated that the District has funded six (6) Small-Scale Derelict Vessel removal projects with the City of Fort Lauderdale through this program. He asked for questions, there were none.

Mr. Crosley introduced Ms. Lori Brownell, Broward Deepening Project Manager, with Taylor Engineering to provide a Broward Deepening project update. The primary project components include dredging, development of the DMMA access road, permit modifications for utility crossings, and the project schedule.

Ms. Brownell stated that the project contractor is Cashman Dredging and Marine Contracting. This project is utilizing the Port Everglades Dredged Material Management (DMMA) site. The 720-day Notice to Proceed (NTP) was authorized on January 21, 2016, immediately following the pre-construction meeting. The NTP project completion is January 21, 2018. Cashman has done a great job dredging and handling the dredged material. The project is ahead of schedule and dredging should be completed by the end of November. Per the agreement with Port Everglades, the DMMA is to be offloaded within 90-days after dredging has been completed.

Ms. Brownell stated that the project contract price is \$16.9 million with an alternate bid of \$200,000.00 to dredge the area at Las Olas Boulevard. There have been five (5) project change orders increasing the contractor cost to \$17 million. This project is to dredge the -10-foot+2 project channel depth to a -15-foot +2 project channel depth. Approximately 183,000 cubic yards of material will be removed. The project area is from the 17th Street Bridge to Las Olas Boulevard. The contractor is currently dredging just south of where the Fort Lauderdale Boat Show will take place. The Marine Industries Association of South Florida (MIASF) has been very helpful in explaining and educating the boating public about the project and now, larger boats know they can come by water to the Fort Lauderdale Boat Show.

Ms. Brownell stated that every morning Cashman Dredging, Inc. provides a marine traffic “Daily Position Report”. Anyone can join and receive this “Daily Position Report” by providing your e-mail to Cashman Dredging, Inc.

Ms. Brownell stated that, to date, there have been no complaints from Port Everglades about the District's use of the DMMA. Once the project is completed and the DMMA has been off-loaded, the District will receive a return of the DMMA use deposit.

Ms. Brownell stated that the project access road is owned by Port Everglades, Broward County and Florida Power and Light Company. It is limited to one-way traffic and no hauling from 6 to 8 a.m. and from 4 to 6 p.m. on N.E. 7th Avenue. A minor issue with the use of N.E. 7th Avenue arose and the MIAF was been helpful in resolving the minor problem.

Ms. Brownell stated that Florida Power and Light (FP&L) was concerned about project dust causing a problem with the electric lines, but to date that has not been an issue. four (4) utility crossing were identified and each crossing contractor conducted their own utility crossing survey. The utility crossing at the Swimming Hall of Fame area is currently being relocated. There are three (3) utility crossings at the Las Olas Boulevard area, with one (1) of the crossings in the process of being relocated. The City of Fort Lauderdale water line and the AT&T crossing have become a problem. AT&T is working to obtain the permits to relocate that line. It is hopeful that AT&T will have those permits by the end of October of 2016. Once the permits are obtained it will take six (6) to eight (8) weeks to relocate the line. The City of Fort Lauderdale water line will be completed by mid-December and then tested for water quality before approved for use. If those utility lines are not relocated and completed by the time the dredging contractor is working in that section of the waterway, that section will not be dredged. There would be 3,100 cubic yards of material left to be dredged from that area and that would be done later, probably by a USACE hopper dredge.

Commissioner Sansom noted that if that section of the waterway is not dredged it may not have a huge impact on the finished product.

Ms. Brownell stated that the District has received all required permits, change orders permit modifications, and Department of Transportation (DOT) general use permits. It is expected that this project will be completed by the end of December of 2016.

Mr. Crosley thanked Cashman Dredging, Inc, Taylor Engineering, Inc., Port Everglades, the Marine Industries Association of South Florida, the Cities of Fort Lauderdale and Dania Beach, and Broward County for their help with this project.

Mr. Steven Tobin with Cashman Dredging, Inc stated that the District and Taylor Engineering, Inc. have been fabulous clients to work with.

ITEM 9. Comments from the U.S. Army Corps of Engineers.

Ms. Shelley Trulock, the Intracoastal Waterway (IWW) Project Manager with the U.S. Army Corps of Engineers (USACE), will not be making a presentation today.

ITEM 10. Presentation – Professional Federal Legislative Services.

Mr. Jim Davenport, with Alcalde & Fay, stated that the Federal FY 2016 budget provided \$3.3 million for FIND IWW projects. He stated that is the most funding the District has received since the earmark framework was eliminated in 2011. In addition, the President's FY 2017 Budget Work Plan has included \$850,000.00 for the Intracoastal Waterway (IWW).

Mr. Davenport stated that he is working to obtain waterway funding in the Federal 2017 Work Plan, which is currently being formulated and will be released in early February 2017. Alcalde & Fay, the Jacksonville USACE, and FIND are working to obtain funding,

approximately \$8 million, to dredge the Broward Reach I, Sawpit, and Crossroads projects. For the past decade, the Federal Work Plan amount has either been \$0 or \$250,000.00.

Mr. Davenport stated that this is the first year, that he can remember, that the House of Representatives has failed to pass an Energy & Water appropriations bill. The bill was not passed because it included several controversial provisions. This bill will be included into an Omnibus spending bill in the future. This spending bill will include a line item for funding the IWW and the waterways, \$25 million for navigation and \$45 million for Inland Waterways. Congress returns November 14, 2016.

Mr. Davenport stated that the “no mitigation for maintenance dredging” wording was presented in the Magnuson Stevens bill but was not addressed in this Congress and the bill at present is dead. The House Water Resources Minority Staff would not allow the wording to be included, suggesting it was controversial. Representative Frankel decided not to go forward with the amendment with the assumption that the Committee Chairman or Ranking Member of the Subcommittee would object to its consideration and likely it’s approval.

Mr. Davenport stated that at the beginning of this calendar year, Alcalde & Fay will try to put language in the Water Resources Development (WRDA). A new “no mitigation for maintenance dredging” language draft has been developed and he has discussed the new language with the USACE. He also met with the Minority Staff Chairman Shuster and discussed the issue, FIND and the new language. Chairman Shuster now understands the issue. Also, the National Wildlife Federation has objected to the language. He noted that the National Wildlife Federation does not support dredging projects.

Mr. Davenport stated that the USACE has sent the “no mitigation for maintenance dredging” language draft back to him with some changes referencing the Magnuson Stevens Act. The USACE also sent the revised “no mitigation for maintenance dredging” language draft to the Minority Staff. The Minority Staff recessed and when they came back from recess, there was an urgency to get WRDA done. The Minority Staff contacted Representative Frankel’s office and said they wanted to keep the WRDA bill limited and the “no mitigation for maintenance dredging” language could be controversial. Mr. Davenport stated that this is aggravating because he felt that going to the USACE for a language review would be helpful. He contacted Representative Frankel’s office and was advised that she will offer this language as an amendment with a Republican co-sponsor. There was not enough time to obtain Republican co-sponsor. Efforts will continue at the next legislative session.

Commissioner Blow suggested contacting the various Marine Industries Associations and Boat US to obtain voices in favor of the “no mitigation for maintenance dredging” language to counteract the National Wildlife Federation’s opinion. Mr. Davenport stated that he agrees and will coordinate the proper time to contact other industries to weigh in on this topic.

Commissioner Sansom suggested letting the parties involved in this discussion know that the District is not trying to change their ideas about dredging. That FIND only wants them to know that this is not new dredging, it is maintenance dredging of areas that were previously dredged, often many years ago, to allow commerce operate on the IWW. Mr. Crosley agreed that is a good approach.

Commissioner Crowley questioned if it would be helpful to send a letter listing the work the District completes, including the various types of grant projects that the District has been involved in that have produced significant environmental benefits such as living shorelines.

Commissioner Sansom stated that the information should be kept focused, limited, and specific to only what the District is asking for help with, maintenance dredging of the IWW for marine commerce and the limited amount of dredging it takes to maintain the IWW for that commerce. The District should not take on a dredging of the waterways battle.

Mr. Davenport stated that there is an item of interest in the Senate WRDA bill called the “Non-Federal Interest Dredging Authority”. He stated that the way he reads the bill is that groups like FIND, that partner with the USACE and use their own money for dredging projects, could complete the dredging project and then ask the Federal government for reimbursement. This should be looked at once the bill is completed and passed.

Commissioner Blow stated that he appreciates Mr. Davenport’s work on behalf of the District.

Commissioner Crowley stated that Mr. Davenport’s strategies have greatly assisted the District in securing additional Federal funding.

Mr. Davenport stated that he looks forward to the District’s Washington visit in early March of 2017.

ITEM 11. Fiscal Year 2017 Work Plan and Fiscal Year 2018 Federal Funding Request.

Mr. Crosley stated that staff has discussed the FY 2017 Work Plan with the District’s federal legislative representative and the U.S. Army Corps of Engineers

(USACE). Attached is the proposed FY 2018 Federal funding request package that will be distributed during the District's Washington D.C. visit. Mr. Davenport noted that the Work Plan projects can change as long as the request stays in line with the USACE's requests.

Mr. Crosley noted that the most significant project is the \$14 million maintenance dredging in the vicinity of the Ponce de Leon Inlet which includes offloading Rattlesnake Island. The next project is the Brevard County Reach II maintenance dredging project and then the Broward County Reach I maintenance dredging project. It is essential that federal funding assistance is received for these projects.

Commissioner Crowley asked about the Baker's Haulover dredging. Mr. Crosley stated that project is a Regional Sediment Management Project with the USACE and it has been listed on the USACE Status Report. This is a Shore Protection funded project. Sand from this project will be placed on the beach, as needed. Because the Regional Sediment Management Project could be a long process, FIND should be prepared to dredge the area if necessary.

Commissioner Crowley stated that the area is close to half-way blocked. He stated that understanding the amount of time it takes to permit a project like this makes him nervous that FIND is not moving forward at this time. Mr. Crosley stated that staff will talk to Ms. Trulock about this project in November.

Commissioner Sansom made a motion to approve the District's 2018 Federal funding requests. The motion was seconded by Commissioner Blow. Chair Cuozzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 12. Presentations from the Selection Committee's Top-Ranked Firms for Completion of the Update of the Economic Analysis of the District's Waterways, Initiating with Palm Beach County, Florida.

Mr. Crosley stated that at the previous regular meeting on September 9, 2016, the Board reviewed and discussed the Selection Committee rankings for the referenced project. The Board then elected to hear presentations from the top three ranked firms.

Mr. Crosley stated that each firm will be given twenty (20) minutes for their presentation, including their questions and answers. Presentations will be held in random order. All other presenters will be requested to wait outside of the meeting room once the presentations have begun.

Mr. Shelton Perry and Mr. Danny Mayer, with GEC, Inc., presented the proposal to complete the Economic Analysis update of the District's waterways for 2017. Mr. Perry stated that GEC is a multi-disciplined planning and engineering firm specializing in water resources, transportation structure, planning, and design. GEC has multiple offices, with an office located in Vero Beach, Florida. GEC has conducted hundreds of feasibility studies and navigation economic studies throughout the United States.

Mr. Perry stated that GEC has abundant knowledge of marine feasibility, dredged material management plans, and economic impacts in Florida. GEC developed the scope of work to conduct multiple economic analyses for FIND from 2001 through 2008 with an updated analysis in 2011. GEC also worked with the USACE on feasibility and impact studies of the Jacksonville Harbor, Palm Beach, Tampa, and Panama City projects. GEC has complete analysis for the Palm Beach County Deepening Mega-Yacht Study, surveyed boating activity along the IWW in Broward County, and performed an Economic Analysis of the St. Lucie, Lake Worth and the Hillsboro Inlets. GEC has extensive experience

beyond Florida including analysis of the McClellan-Kerr Arkansas River Navigation System, Red River Waterway, Gulf Intracoastal Waterway and the Memphis Tennessee Riverfront Development project.

Mr. Perry stated that GEC has studied every county in Florida and is knowledgeable about Florida's waterways. In addition, GEC has knowledge of Florida's marinas, inlets, recreational and commercial boaters, what the IWW means to the quality of life in Florida, and property values.

Mr. Mayer stated that GEC's experienced technical approach will draw from, identify, and quantify the economic impact of the waterways on business sales, personal income, employment, and tax revenue. The influence of the IWW on property values will be estimated. GEC will complete marine business, employment, boat owner, and property value surveys. Boat owners will be surveyed and questioned about their boating, gas and food purchases.

Mr. Mayer stated that GEC questioned if FIND would like inlet economic impacts included in the approach. Inlets are critical features for boating on Florida's East Coast.

Mr. Mayer stated that the study would include the effects of restricted draft waters in certain counties, the impact of different waterway maintenance scenarios and property value impacts. He asked for questions.

Chair Cuozzo asked about the marine-related business survey. Mr. Mayer stated that GEC will search key words in the yellow page ads for marinas, biz code, and talk to boat owners. Boaters would be surveyed about their marine related food, gas, and clothing purchases.

Chair Cuzzo asked if GEC will talk to the District Commissioners for suggestions. Mr. Mayer stated that GEC will talk to the local Marine Industries Associations and Chamber of Commerce.

Commissioner Chappell asked about the percentage of the effort/cost that will go toward data collections. Mr. Mayer answered 80%. Commissioner Chappell asked if any of the data from previous FIND economic research could be used to save the District money. Mr. Mayer there could be cost savings by use of e-mail versus USPS.

Commissioner Blow asked the source used to determine the IWW economic impact on real estate. Mr. Mayer stated that they would use the property appraiser's records and recent real estate sales. Adjustments would be based on those ratios. Commissioner Blow stated that the property appraiser's records do not always reflect the true value of a property. He suggested Zillow and Realtor.com.

Mr. Dan Levy, with AECOM, presented the proposal to complete the Economic Analysis update of the District's waterways for 2017. He stated that AECOM employs 85,000 people in 150 countries. In Florida, AECOM has 12 offices, 1,206 employees and 4 local accessible subcontractors. AECOM has local knowledge of, and interaction with, the local government. He introduced Mr. Michael Giovanniozzi, PE, Project Manager for this project. Mr. Giovanniozzi lives in Palm Beach County.

Mr. Giovanniozzi introduced Mr. Joseph Berlin, Technical Lead for this study. Mr. Berlin has extensive experience performing economic studies. Mr. Berlin was employed by GEC in 2007 and worked on the FIND Economic Analysis at that time.

Mr. Berlin stated that surveys would be sent and followed-up to over 800 marine-related businesses in Palm Beach County. The general boating public would be surveyed

to obtain their recreational boater spending. Property appraiser and property evaluation information would be collected. A data base would be developed to assimilate this data. Business data would be collected under a non-disclosure agreement. Starmark would be involved in public outreach and discussion.

Mr. Giovanniozzi stated that as the project manager, he would be the primary contact for this project. Several management staff have experience in navigation, dredging, and waterway planning. Two technical advisors that will be working on this project are Mr. Joseph Wagner and Ms. Nancy Leikauf.

Mr. Giovanniozzi discussed similar projects that AECOM has performed and provided a project cost estimate, additional services, and project talking points. This project is about gathering solid information in a timely fashion. He asked for questions.

Chair Cuzzo asked about the approach to contacting Palm Beach County businesses. Mr. Levy stated that the Florida Department of Revenue has a sales-tax base that can be used to identify the marine-related businesses with contact information. The Marine Industries Association would be contacted for information. Marine business revenue is confidential and there would be a non-disclosure statement to keep that information confidential. The District's web page with a hot-link could be used to collect data as well.

Commissioner Blow asked about using Facebook and Twitter. Mr. Levy stated that proper use of social media will provide valuable information.

Secretary Donaldson asked about coordinating the various people and sub-contractors for this project. Mr. Levy stated that all staff selected to work on this project

are here today, are local to South Florida, are experienced and have worked together on previous projects. All data would be collected and assimilated in a timely manner.

Ms. Valerie Seidel, with the Balmoral Group, presented the proposal to complete the Economic Analysis update of the District's waterways for 2017. She introduced Ms. Courtney Cortez, Research Analyst and Mr. David Rivenbark, Ph.D., Consulting Economist who will be working on this project.

Ms. Seidel stated that the FIND Request for Qualifications (RFQ) requested several required elements be included in the Economic Update, including: reports in plain English, literature review, interviews and surveys, input-output model, hedonic property model and alternative scenarios.

Ms. Seidel stated that the literature review will include data from prior reports, and relevant recent studies by FIND and other agencies. Property evaluations will include the 2014 economic valuation of the St. Johns River. Primary economic impact comes from recreation boaters, boating commerce, public fishing expenditures, residential property adjacent to the IWW and Lake Okeechobee, and the increase of property values, including FIND's procurement of land and land management.

Ms. Seidel stated that The Balmoral Group will identify businesses along the waterway from several sources, the Arc GIS, property appraiser's offices, water-dependent business employment via Nielsen site reports, and the Department of Revenue. Also, business located on the waterway and businesses will be included that are related to boating but not directly located on the waterway that provide services.

Ms. Seidel stated that interviews will be conducted using best management practices, meaning that the questions will be asked with sensitivity. The questions will be

structured to identify links between business revenues and the waterway and asking business owners about their proprietary information in a non-threatening manner is essential.

Ms. Seidel stated that a list of licensed boaters will be created from boating license data, which will then be buffered to delineate various boat sized groups. A postcard will be mailed and the survey can be completed on line. The boater spending detail will include, but not be limited to, repairs, fuel, food and beverage, rental fees, guide map/books, and lodging.

Ms. Seidel stated that the alternative scenarios will include the economic impact to the IWW with and without proposed dredging and channel maintenance. The scenario would show the economic impact to marine business, property owners, service providers, employment, restaurants, gasoline, etc.

Ms. Seidel stated that there is a property valuation premium attached to waterfront property. That premium extends about one-mile from the waterfront and homes with proximity value left-out of the survey would leave out about one-half of the property impact value.

Ms. Seidel stated that the Balmoral Group was founded in 2004 as a Florida professional engineering firm focusing primarily on water resources. The Balmoral Group has worked for 67 counties within the State of Florida. Projects include: Tampa Bay Estuary, 2011; St. Johns River, 2014; Indian River Lagoon, 2013; Martin County Coastal Evaluation, 2012; and EASS water demand, 2015. She asked for questions.

Chair Cuozzo asked how the Balmoral Group would physically contact the business to survey them. Ms. Seidel stated that several methods will be used. Businesses can be

contacted by: telephone; physical surveying within the community; electronic surveys; and boater survey information distributed at boat ramps.

Commissioner Williams noted that Florida has a high degree of transient boaters and he asked how those boaters would be surveyed. Ms. Seidel stated that the most popular and frequently used areas for transient boaters would be identified and those areas would physically be surveyed. Also, the Lightspeed GMI on-line panel would establish transient boater activity. She stated that the Balmoral Group is currently completing that survey for the Florida Department of Environmental Protection.

Chair Cuzzo stated that because the FIND staff must work with the company selected to complete this project, he asked for staff input.

Mr. Crosley noted that the selection committee ranked GEC just slightly above the Balmoral Group. At the last District Board meeting, staff heard considerable positive feedback about the Balmoral Group. From the presentations, graphics, and information provided today, he feels that the Balmoral Group would be a good fit for the District's economic impact study.

Ms. Zimmerman stated that any one of the three companies presenting today could perform this study. She was most impressed with the Balmoral Group's economic experience, their potential cost savings, and the use of Lightspeed GMI to survey transient boaters.

Commissioner Crowley asked if staff has checked company references. Mr. Crosley stated, not at this point. Today the companies will be ranked one, two, and three. Then references are checked. Negotiations would then be completed with the top-ranked firm and if unsuccessful, negotiations would then begin with the second ranked firm.

Commissioners commented on the three (3) firm's presentations and qualifications.

Secretary Donaldson stated that he is ready to rank the applicants.

Commissioner Williams stated that one presentation was outstanding. He stated that he is ready to rank the applicants.

Ms. Zimmerman asked for commissioners on the telephone to rank the firms and send them to her in an e-mail.

Commissioner Williams made a motion to approve the final ranking of the firms in descending order as; the Balmoral Group, AECOM, and GEC. The motion was seconded by Secretary Donaldson. Chair Cuozzo asked for discussion. Hearing none, a vote was taken and the motion passed. Commissioner Crowley voted against the motion.

Commissioner Crowley stated that he would rank GEC ahead of AECOM. He asked that the vote be re-considered and that all the ranking sheets be turned in and tabulated before voting.

Secretary Donaldson made a motion to approve to re-consider the motion ranking the firms. The motion was seconded by Commissioner Crowley. Chair Cuozzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

Ms. Zimmerman stated that the ranking forms have been tabulated and the results are; the Balmoral Group first with a tie between GEC, and AECOM for second.

ITEM 13. Board Ranking and Selection of the Top-Ranked Firm for a Continuing Services Contract for the Update of the Economic Analysis of the District's Waterways, Initiating with Palm Beach County, Florida.

Secretary Donaldson made a motion to approve the Board's recommended final ranking of the firms in descending order; the Balmoral Group, with a tie between GEC, and AECOM for a continuing services contract for Economic Analysis Services to update

the analysis of District waterways, initiating with Palm Beach County, Florida. The motion was seconded by Commissioner Crowley. Chair Cuzzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 14. Review of the Navigation District's Proposed 5-Year Dredging and Dredged Material Management Area (DMMA) Plan.

Mr. Crosley stated that because of time constraints, he suggested moving this item to the District's November 2016 agenda.

ITEM 15. Interlocal Agreement with the Jupiter Inlet District for Intracoastal Waterway Maintenance Dredging near Jupiter Inlet, Palm Beach County, Florida.

Mr. Crosley stated that the Navigation District (District) and the Jupiter Inlet District (JID) have developed an Interlocal Agreement to share costs associated with maintenance dredging in the vicinity of the Jupiter Inlet.

Commissioner Sansom made a motion to approve the Interlocal Agreement with the Jupiter Inlet Dredging to proportionally share costs associated with dredging of the Intracoastal Waterway and the Jupiter Inlet, Palm Beach County, Florida. The motion was seconded by Commissioner Blow. Chair Cuzzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 16. Finance and Budget Committee Report.

Acting Committee Chair Sansom stated that the District's Finance and Budget Committee met before today's Board meeting and the committee reviewed and recommends approval of the August 2016 financial statements, the delegation of authority and the expenditure and project status report. He asked for questions. There were none.

Acting Committee Chair Sansom made a motion to approve the August 2016 financial statements, the delegation of authority, and the expenditure and project status

report as recommended by the District's Finance and Budget Committee. The motion was seconded by Commissioner Blow. Chair Cuzzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

Mr. Crosley stated that the District's Finance and Budget Committee also discussed and recommends approval of the Engagement Letter from Berger, Toombs, Elam, Gaines & Frank for the FY 2015-2016 audit. He noted that last year was the first audit with rotation of the audit shareholder/partner.

Committee Chair Sansom made a motion to approve to accept the Engagement Letter from Berger, Toombs, Elam, Gaines & Frank for the FY 2015-2016 audit. The motion was seconded by Secretary Donaldson. Chair Cuzzo asked for any additional discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 16A. Storm Damage Discussion.

Mr. Crosley stated that staff completed a post-Hurricane assessment of the District's DMMA sties from North (DMMA NA-1) to Central (DMMA V-20). Fortunately, the District DMMA sites sustained only minor damage. DMMA SJ-20A has approximately twelve (12) trees down along the south fence at the following residences 104 Bilboa, 108 Bilboa, and 112 Bilboa where tree tops have crossed the fence. Staff is working on this and the damages are expected to be within his spending authority of \$10,000.00. If the damages go over \$10,000.00, he will bring it to the board for approval at the District's November meeting. Staff has not surveyed Rattlesnake Island in New Smyrna but, he is not anticipating problems at this site. He asked for questions. There were none.

Mr. Crosley stated the Vilano Beach residents north of the St. Augustine Inlet have contacted the District regarding erosion damages from Hurricane Matthew. He then referred to a letter from the St. Augustine Port, Waterway and Beach District regarding an urgent request to pursue waivers to the current permits for placement of flood-shoal dredging material south of the inlet in Anastasia State Park, north to the Inlet. This request originated because of critical damage due to Hurricane Matthew.

Commissioner Blow stated the Port, Waterway District may be able to pay for the additional costs to put sand north on Vilano Beach. They are requesting that the Board authorize Mr. Crosley to write a letter to Colonel Kirk at the United States Army Corps of Engineers (USACE), Colonel Kirk requesting the USACE change the location of the sand placement to Vilano Beach with the location of the sand placement being worked out between the county and USACE.

Commissioner Blow stated that he will go back to the county and request that the county also write a letter to Colonel Kirk and include the language that the county will be responsible for the easements above mean high water and request an expedited permit or permit waiver from USACE. The Board would authorize Taylor Engineering to coordinate with the USACE.

Mr. Crosley noted that this is officially a USACE project. This area may qualify for federal funding relief. The community would take care of the problem and then would ask the federal funding relief program for reimbursement.

Mr. Millan Mora with the USACE, stated that he does not believe that the USACE opposes the idea of placing sand to the north of the St Augustine Inlet, but there are some hurdles to overcome: (1.) The District does not have the easements to place sand to the

north. Real estate easements need to be secured; (2.) Permit modification or a new permit needs to be submitted; (3.) There may be additional costs above the federal standard to place the sand further to the north (increased distance and possible booster pumps) therefore this would not be the least cost option and some entity would need to pay the difference. There would also need to be a modification to the Weeks Marine contract. The contract has already been awarded and the Notice to Proceed will be issued around the end of the month (October 29/30th), with commencement being mid-November. The dredging contractor will be using a 24" dredge and will be in and done with the project very quickly. Those three (3) items mentioned must be completed very quickly.

Commissioner Blow requested that FIND ask the dredger to start with Matanzas and then complete St Augustine. He stated that he will attend Saturday's South Ponte Vedra/Vilano Beach Preservation meeting and let them know that FIND is willing to make efforts to get sand to Vilano Beach, but it is ultimately up to the USACE to make the decision as FIND's partner. The county and the state will need to work out the necessary items as well. Also, the project is only 200,000 cubic yards of material and the county will need much more to begin to fix their beach erosion problem. The county will be responsible to obtain the project easements, and citizens need to sign the easement documents ASAP.

Secretary Donaldson made a motion to approve the Board modify the project permit to allow beach disposal north of the inlet and the local government be the co-applicant or applicant for the beach project, securing all easements and property rights. The motion was seconded by Commissioner Williams. Chair Cuzzo asked for discussion.

Commissioner Blow requested his fellow commissioners support this request and help the citizens in his county.

Secretary Donaldson amended the motion to include authorization for Mr. Crosley to: write a letter to Colonel Kirk requesting the USACE modify the St Augustine ICW dredging project to allow the placement of sand to the north of the St Augustine Inlet; request St Johns County get the required emergency permits with FIND as the co-applicant if necessary; request St Johns County be responsible for quickly securing all easements necessary for placement of the sand to the north; and authorize staff to issue a task order to Taylor Engineering for up to \$25,000 for any work required on behalf of FIND. The amended motion was seconded by Commissioner Williams. Chair Cuozzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

ITEM 16B. Request to Use District Site DMMA BV-11 for a Storm Staging Area, Brevard County.

Attorney Breton stated that the recommendation is that the Interlocal Agreement between FIND and Brevard County should be changed to a simple six (6) month Lease Agreement.

Mr. Crosley stated that he is asking for authorization for staff to extend the Lease Agreement an additional six (6) months, if necessary.

Commissioner Sansom made a motion to approve a six (6) month Lease Agreement with Brevard County to use District Site DMMA BV-11 for material placement, with the ability for the FIND Executive Director to authorize an additional six (6) months use. The motion was seconded by Commissioner Blow. Chair Cuozzo asked for discussion. Hearing none, a vote was taken and the motion passed unanimously.

Commissioner Sansom stated that he wanted to make all commissioner aware that FIND has a \$1 million for emergency funding for damage to previously funded FIND projects.

Ms. Zimmerman stated that she has had heard from Fernandina Beach, the City of Jacksonville, St Johns and Indian River Counties regarding storm-related damage.

ITEM 17. Additional Staff Comments and Additional Agenda Items.

Chair Cuozzo asked if there were any additional staff comments or agenda items.

Mr. Crosley stated that the Okeechobee Waterway (OWW) Trip scheduled for October 25, 2016 through October 27, 2016 has been cancelled.

Mr. Crosley stated that a thank you letter has been sent to the USACE and Congresswoman Frankel for their support of the Intracoastal Waterway.

ITEM 18. Additional Commissioners Comments.

Chair Cuozzo asked if there were any additional commissioner comments.

Commissioner Sansom noted that last evening's Community Outreach Event was well-attended and very nice. The IWW location was beautiful.

Commissioner Blow stated that he was speaking with Ms. Cohn of the Marine Industries Association of South Florida last evening and she mentioned that 30 large super-yachts, of which five (5) are over 300-feet long and have never been a part of the Fort Lauderdale Boat Show, will be able to participate this year due to the Broward County Deepening project.

Commissioner Sansom stated that Ms. Cohn also mentioned that several yachts are coming to Fort Lauderdale for maintenance for the first time because of the Broward County Deepening project.

Chair Cuozzo stated that because of the public records laws, FIND is responsible for keeping all commissioner e-mails. Commissioners were recently the recipients of a barrage of e-mails that must be kept. He encouraged commissioners to move those e-mails

into a FIND folder. He stated that he is requesting that commissioners have a FIND e-mail address that is posted on the District's web site. Also, he would like all commissioners to get new FIND business cards with the new e-mail address on them.

ITEM 19. Adjournment.

Chair Cuozzo stated that hearing no further business the meeting was adjourned at 12:34 p.m.



MARTIN COUNTY PROJECT STATUS UPDATE

NOVEMBER 2016

Dredged Material Management Plan

Phase I of the Dredged Material Management Plan (DMMP) for the 21 miles of Intracoastal Waterway in Martin County was completed in 1993. Phase II of the DMMP was also completed in 1993, and all major land acquisition was completed in 2001. *(Please see the attached maps).*

The 50-year dredging projection for the IWW in this area is 1.4 million cu/yds. and the storage projection is 2.7 million cu/yds. Maintenance Dredging in Reach II of the Crossroads area is 85% of the projected dredging volume and occurs approximately every 3 years, including this forthcoming year.

Okeechobee Waterway (OWW) Project:

The District is also the local sponsor for navigation interest for 97 miles of the Okeechobee Waterway (OWW) in Martin and Palm Beach County. Phase I of the Dredged Material Management Plan (DMMP) for the Okeechobee Waterway from the Crossroads to the St. Lucie Lock was completed in 1998. Phase I of the DMMP from the St. Lucie Lock to the western Martin County line was completed in 2007. Phase II of the DMMP from the Crossroads to the St. Lucie Lock was completed in 2001, and Phase II of the plan, from the St. Lucie Lock to the western county line, was completed in 2009. *(Please see the attached maps).*

The 50-year dredging projection for the OWW is 1.5 million cu/yds. and the storage projection is 3 million cu/yds.

Dredged Material Management Area Development - IWW:

To date, one of the seven upland Dredged Material Management Areas (MSA M-5) in Martin County has been fully constructed. Sites MSA 524B and MSA 504 were cleared in 2010. Site DMMA O-7 will be constructed by the U.S. Army Corps of Engineers (USACE) in 2017.

The other sites in Martin County are in various phases of pre-construction, environmental permitting, engineering, or design. An offloading of MSA M-5 by Lucas Marine in association with the Ft. Pierce Waterfront Protection Project was completed in 2013. Approximately 110,000 cu/yds. of material was offloaded, at no expense to the District, to assist with the construction of barrier islands offshore of the Fort Pierce Marina. Some minimal damage to the existing gabion mats at MSA M-5 that occurred during the offloading was repaired by the contractor. A full replacement of these mats will be forthcoming in the next few years.

OWW:

Acquisition of four Dredged Material Management Areas to serve the section of the OWW from the Crossroads to the St. Lucie Lock was completed in 2006. Acquisition DMMA LT-4A, which serves the section of the OWW from the St. Lucie Lock to the western Martin County line was completed in 2008. The initial condemnation of a proposed site near OWW Route 2 (DMMA LT-13) has been suspended to evaluate more cost-effective alternatives for the low dredging volumes associated with this waterway. *(Please see the attached maps).*



MARTIN COUNTY PROJECT STATUS UPDATE

NOVEMBER 2016

Waterway Dredging

Dredging of a portion of Routes 1 & 2 of the Okeechobee Waterway within the Lake was completed in 2012, removing some small but critical shoals totaling approximately 6,700 cu/yds. Dredging of the Crossroads area of the IWW and OWW was completed in 2010, and again in the summer of 2013. This area and the recently permitted channel widener (advance maintenance) is scheduled for dredging in 2017. The District is coordinating with the USACE to determine the feasibility of limited hopper dredging of the area in the near-term. Reach IV of the OWW will be dredged in 2017 following the construction of DMMA O-7.

Waterways Economic Study

The Martin County Waterways Economic Study was completed in 2000 and updated in 2011. The update found that the recessionary depression of the economic output of waterway related businesses in the county decreased by \$443.1 million, employment decreased by 2,601 jobs, and \$18.8 million in waterway related tax revenue was lost. The latest economic output of waterway related businesses is \$639.9 million, with 3,750 jobs, wages of \$156.5 million and \$28 million in tax revenues. Property values were determined to be increased by \$588 million by the presence of the IWW channel. These numbers are expected to increase significantly following the economic updates scheduled for 2016. The initial study shows that these economic benefits would be reduced by over half if maintenance dredging of the waterways in the county ceased. *(Please see the attached study excerpt and business location map).*

Waterways Assistance Program

Since 1986, the District has provided \$9.1 million in Waterways Assistance Program funding to 77 projects in the County having a total constructed value of \$58.1 million. The County, the County Sheriff's Office, the City of Stuart and the Town of Jupiter Island have participated in the program. *(Please see attached listing).*

Notable projects funded include: Manatee Pocket Dredging, Sandsprit Park, Twin Rivers Park, the Stuart Riverwalk, the Southpoint Anchorage, MC-2 Bird Island Restoration, and the South County Boat Ramp.

Cooperative Assistance Program

The District's Cooperative Assistance Program has provided funding assistance for 26 state and regional agency projects with elements in Martin County: the Indian River Lagoon Spoil Island Management Program; Florida Clean Marina Program; Florida Clean Vessel Act Program; Florida Marine Patrol Officer Funding; Manatee Pocket Dredging and Jonathan Dickinson Park Boardwalk and Canoe Launch Improvements. The District's funding assistance for the Martin County portion of these projects was approximately \$4 million.

**MARTIN COUNTY PROJECT STATUS UPDATE****NOVEMBER 2016****Interlocal Agreement Program**

The District's Interlocal Agreement Program has provided funding assistance to 4 projects in Martin County. These include Clean Marina and Clean Vessel Act projects as well as environmental improvements at Peck's Lake Park. The District's funding assistance for these projects was approximately \$175,000 and the projects had a constructed value of \$918,000.

Waterway Clean Up Program

The District has partnered with both Keep Martin Beautiful and the Marine Industries Association of the Treasure Coast for several years to assist with their programs to remove trash and debris from Martin County's waterways. The District provides up to \$10,000 year for this program.

Small-Scale Derelict Vessel Removal Program

Martin County has participated in this program with the removal of several vessels.

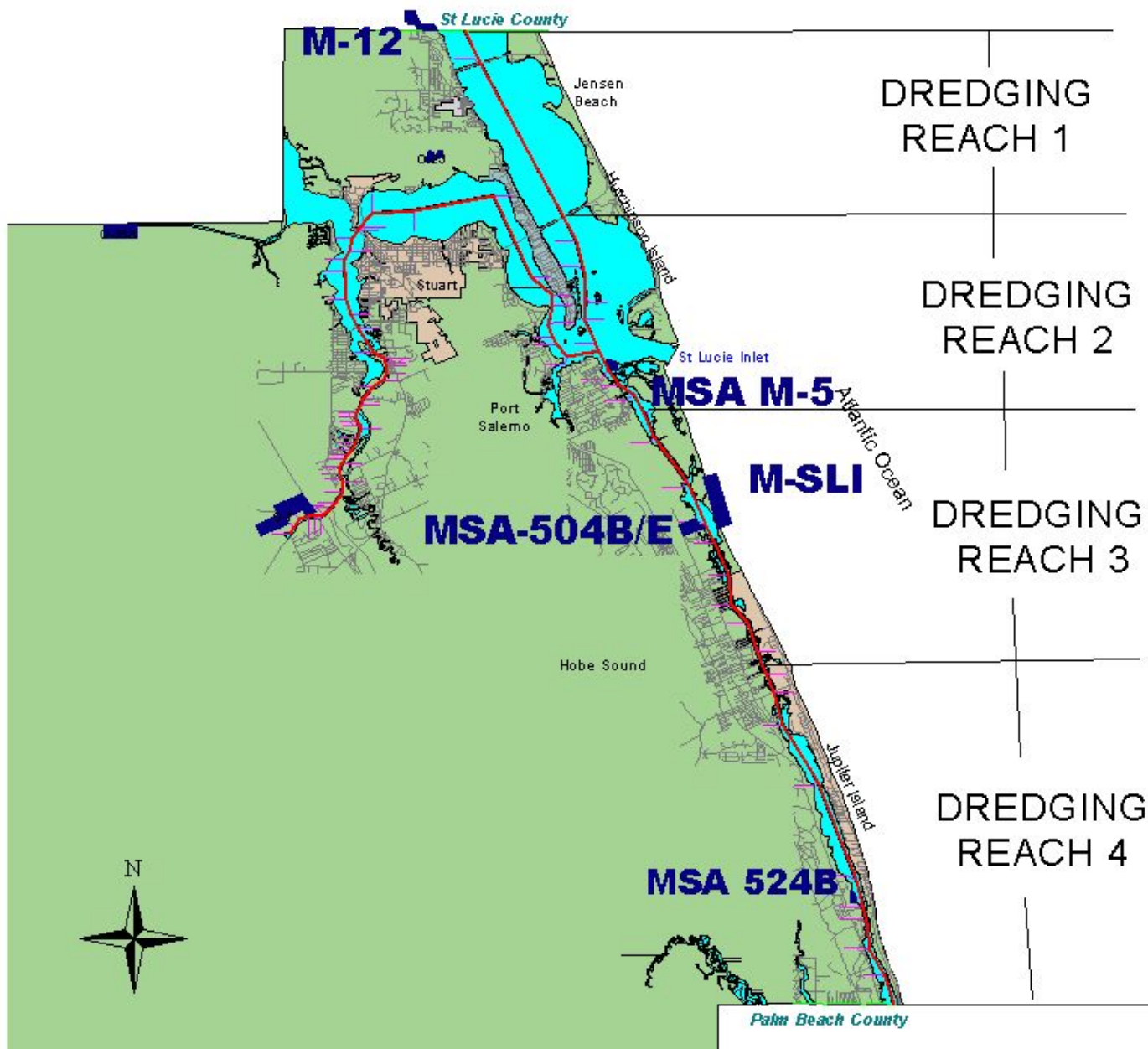
Small-Scale Spoil Island Enhancement and Restoration Program

No projects have been funded yet in Martin County through this program, although restoration to MC-2 (Bird Island) in 2012 was completed through the WAP program.

Public Information Program

The District currently prints and distributes brochures with information pertaining to Martin County Waterways. Additional waterway information and useful links are available on the District's website at <http://www.aicw.org/>.





DREDGED MATERIAL MANAGEMENT PLAN FOR THE INTRACOASTAL WATERWAY IN MARTIN COUNTY

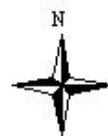


DMMA M-12

43

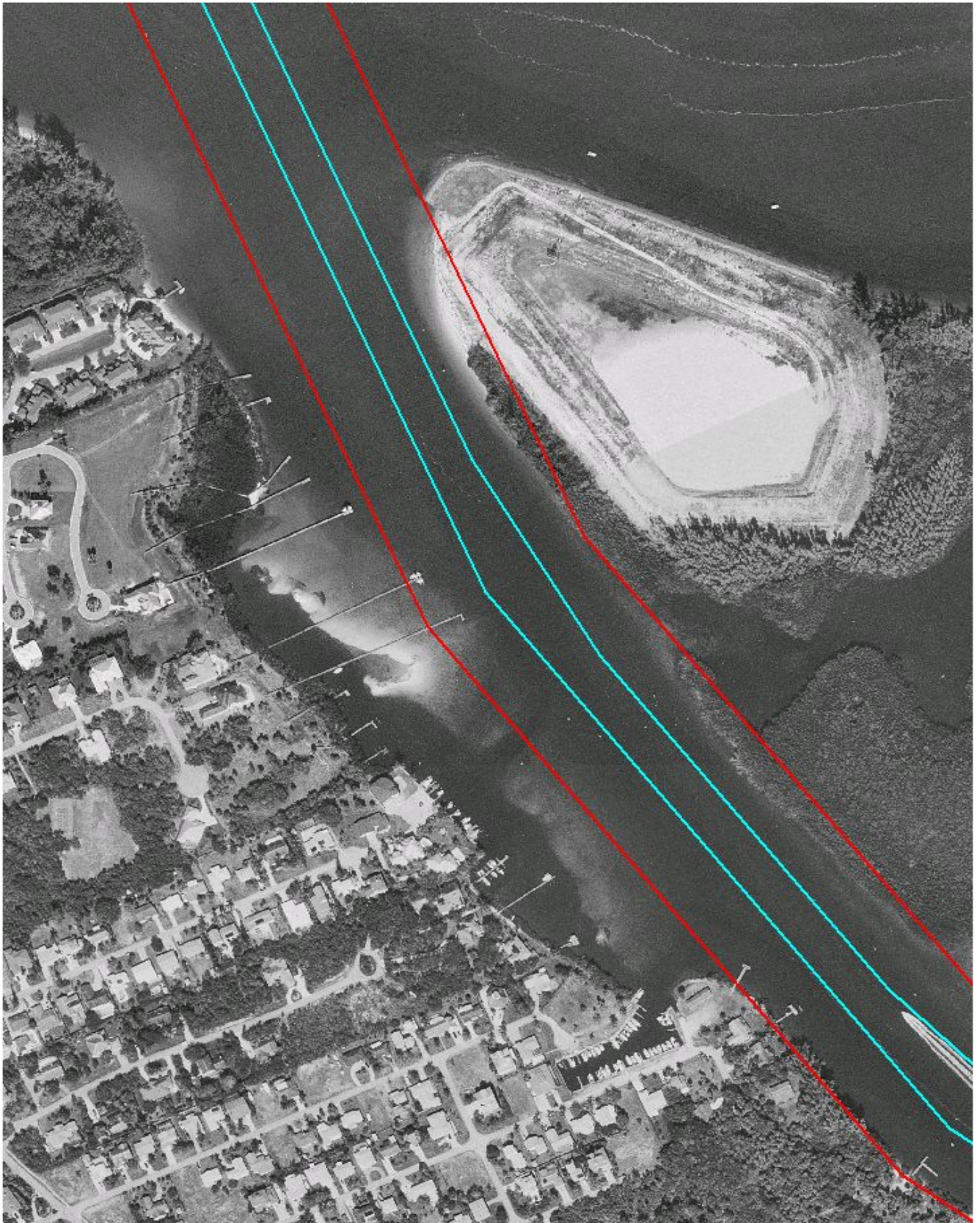


-  FIND Easement
-  FIND Owned
-  Channel
-  ICW Right-of-Way



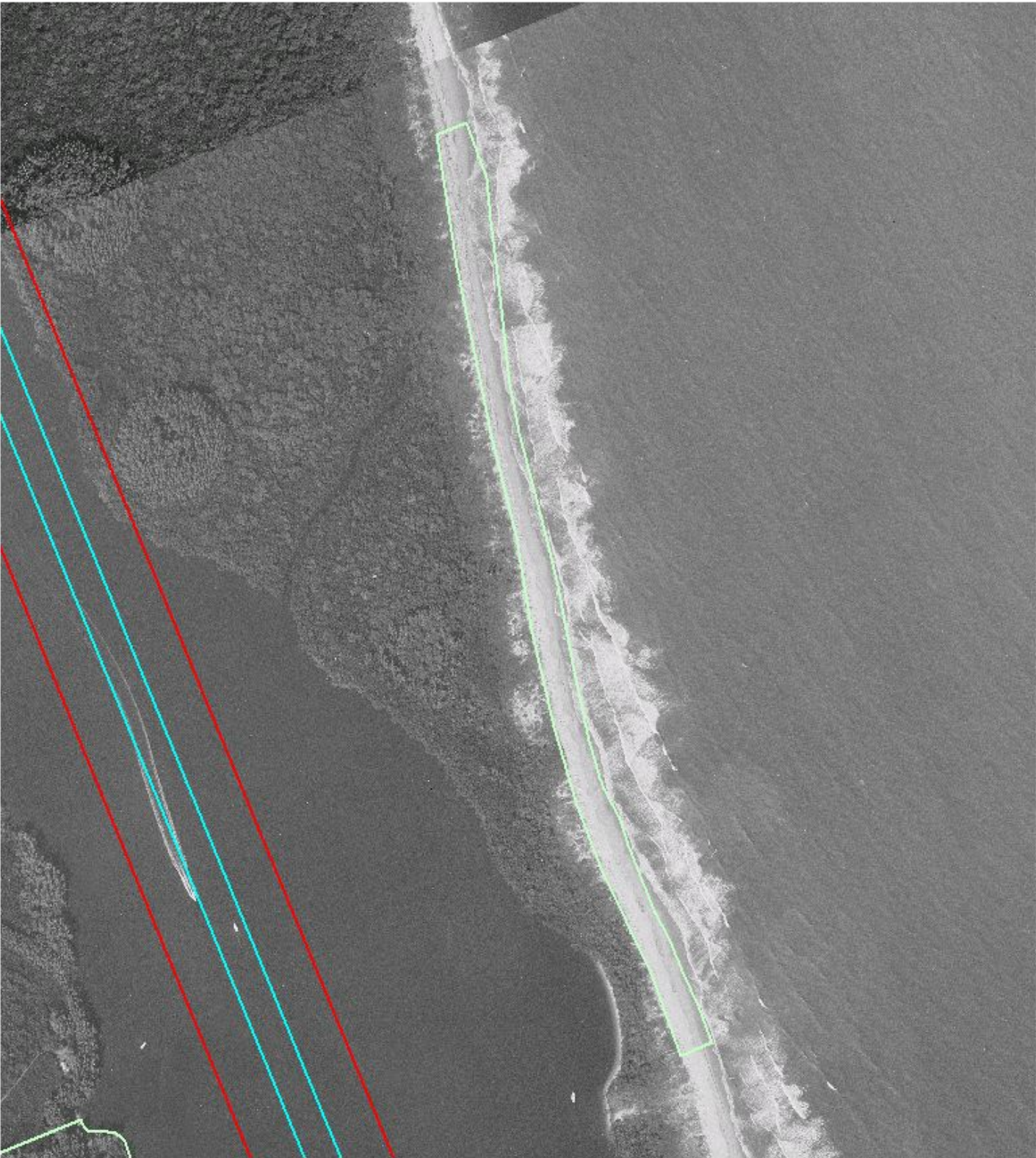
DMMA M-5




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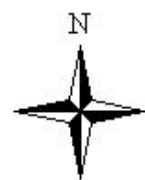


M-SLB

45






 FIND Owned
 Channel
 ICW Right-of-Way



SITE MSA 504B/E







-  FIND Owned
-  Channel
-  ICW Right-of-Way

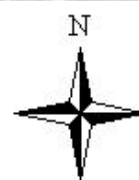


MSA-524B

47



-  Pipeline Easement
-  FIND Owned
-  Channel
-  ICW Right-of-Way







MSA M-5



50

M-SLI

MSA-504B/E

5



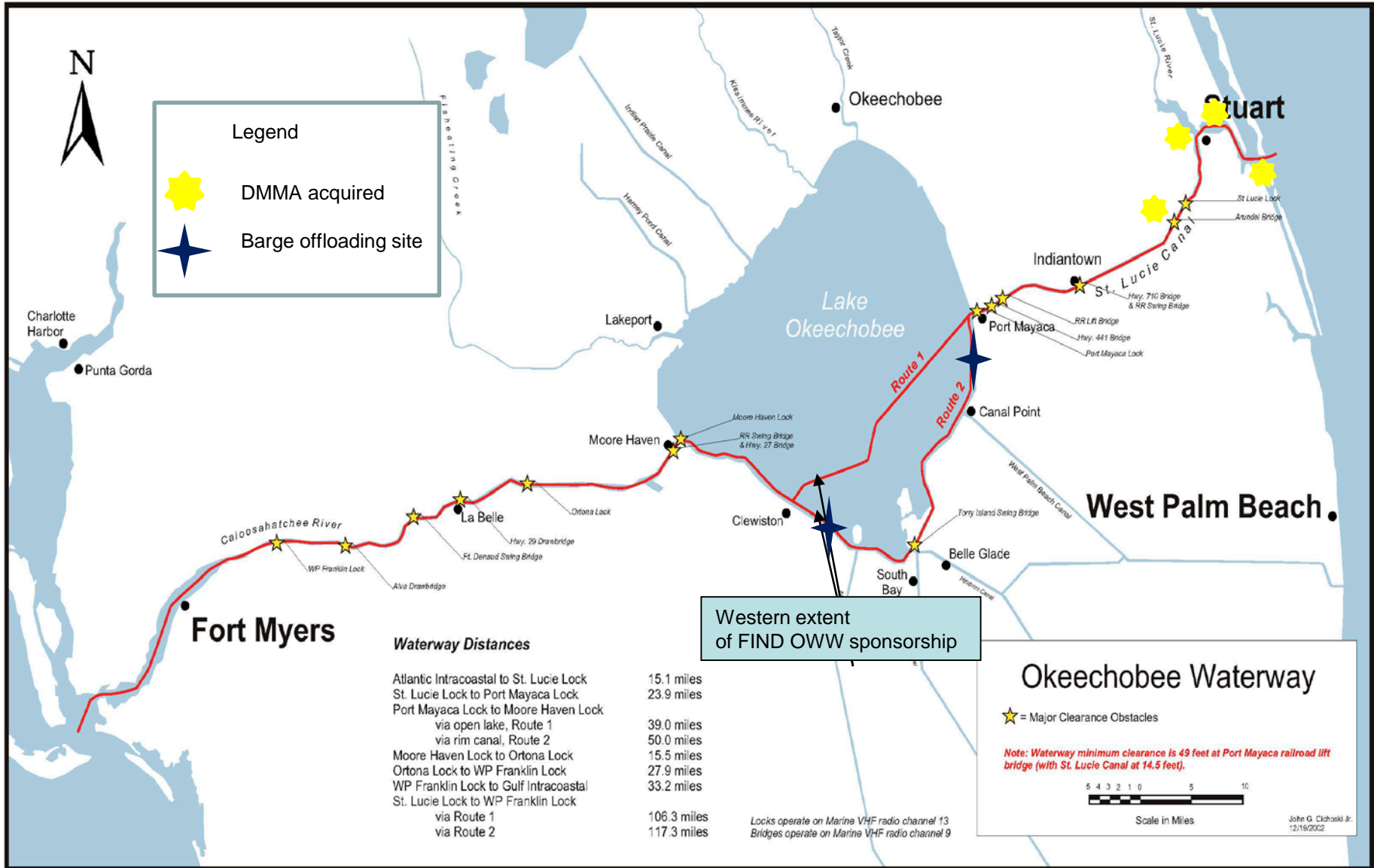
2000 ft

MSA 524B
Martin/PBC border

Legend



MSA 524B



O-23



54

O-23

© 2015 Google

600 t





DMMA O-35A
(DWR)

Legend

55

91

SW Boatramp Ave

O-35A

Google earth

© 2015 Google

600 ft

N

SITE O-7

56



FIND Owned



Okeechobee Waterway





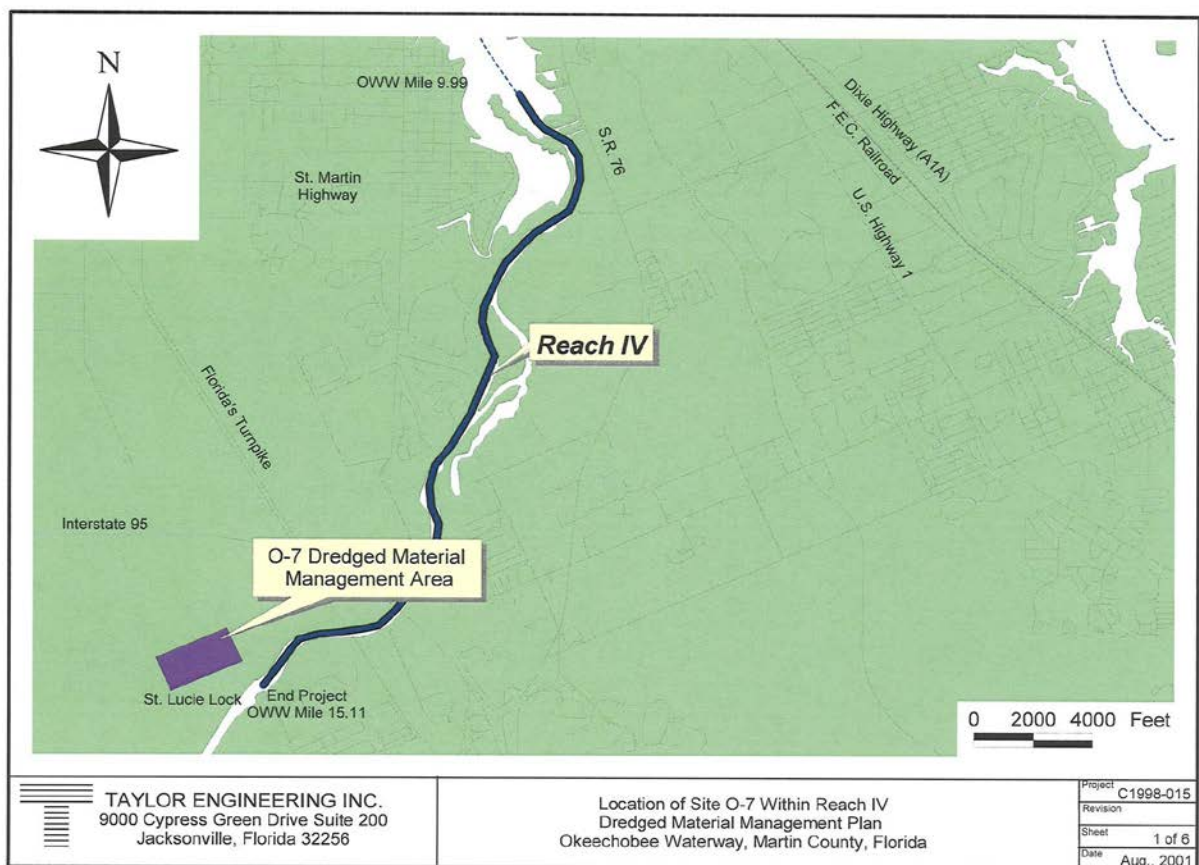
**DMMA O-7 CONSTRUCTION
AND OWW REACH IV DREDGING
STATUS UPDATE
November 2016**

DESCRIPTION OF WORK: Work includes construction of Dredged Material Management Area (DMMA) O-7 and maintenance dredging of shoals in Reach IV of the Okeechobee Waterway (OWW) from north of the Palm City Bridge to the St. Lucie Lock.

PRELIMINARY SCHEDULE:

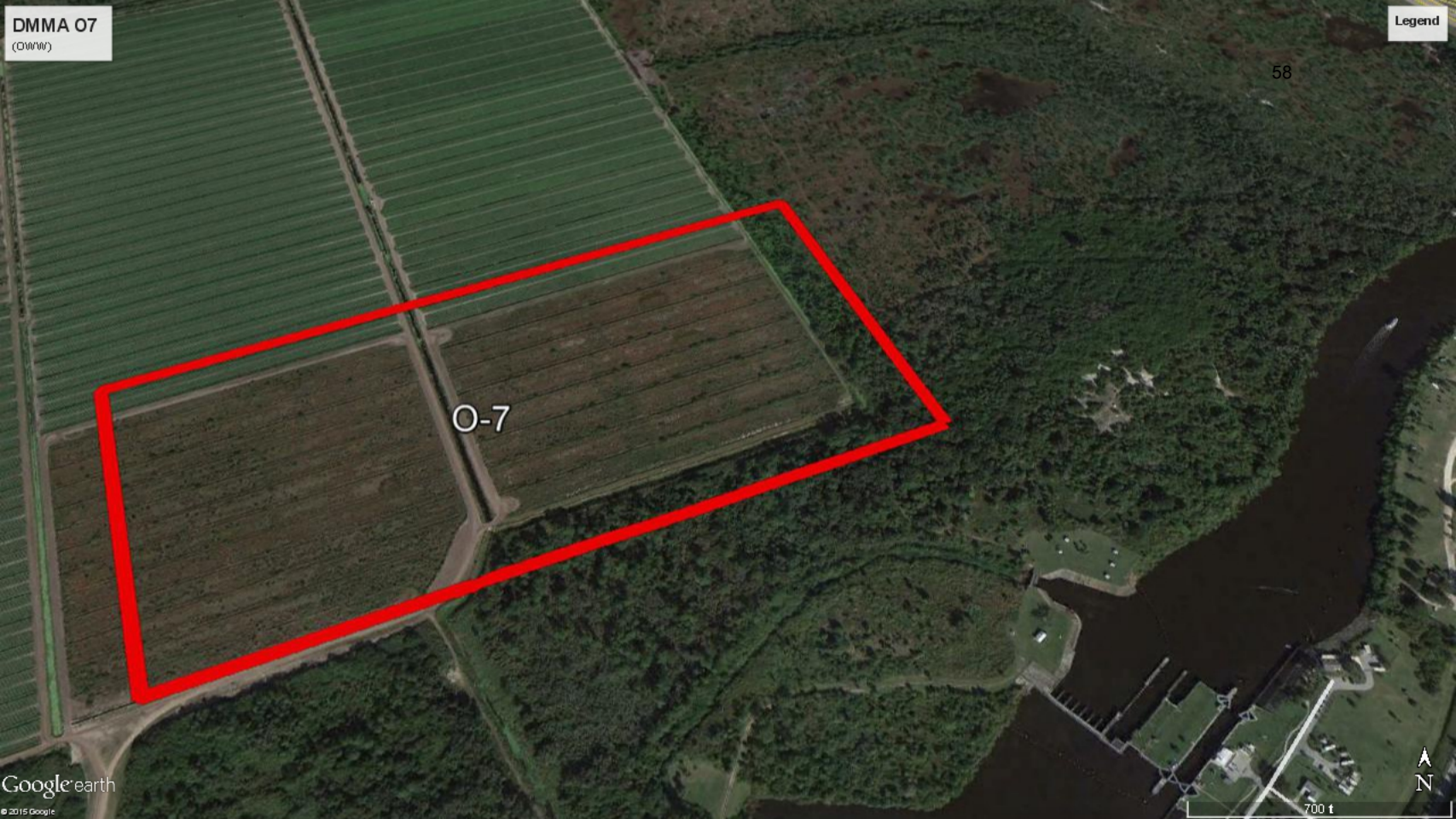
DMMA O-7 Plans and Specs:	Complete (USACE)
Bid O-7 Construction:	2016
O-7 Bid Opening:	November 2016
O-7 Construction:	9 month construction period
Dredging Initiated:	Following O-7 construction

STATUS: Construction pending Work Order execution.



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O-7



ECONOMIC BENEFITS OF THE DISTRICT'S WATERWAYS

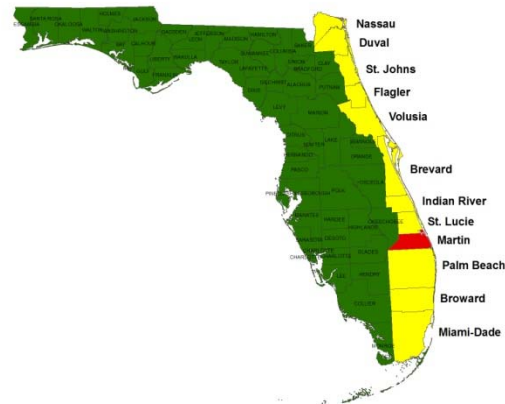


Purpose

To update economic benefits in Martin County of marine-related activities on the District Waterways, as previously estimated in *An Economic Analysis of the District's Waterways in Martin County*, June 2001, and to provide the general public and Federal, State, and local officials with a clear understanding of the importance of maintaining the waterways.

Scenarios Evaluated

1. Current Existing Conditions
2. Cessation of Waterways Maintenance
3. Increase in Waterways Maintenance
4. Estimated impact of the 2007-2009 U.S. economic recession



ECONOMIC IMPACTS

Current Existing Impacts

- \$639.9 million in business volume
- \$156.5 million in personal income
- 3,750 jobs
- \$28.0 million in tax revenue

Impacts of Cessation of Waterways Maintenance

- Decrease of \$354.6 million in business volume
- Decrease of \$80.1 million in personal income
- Decrease of 2,014 jobs
- Decrease of \$15.5 million in tax revenue

Impacts of an Increase in Waterways Maintenance

- Increase of \$160.0 million in business volume
- Increase of \$26.8 million in personal income
- Increase of 664 jobs
- Increase of \$5.2 million in tax revenue

Impact of the 2007-2009 U.S. Economic Recession

- Decrease of \$443.1 million in business volume
- Decrease of \$108.2 million in personal income
- Decrease of 2,601 jobs
- Decrease of \$18.8 million in tax revenue

Economic Benefits as of April 2011

MARTIN COUNTY



ECONOMIC BENEFITS OF THE DISTRICT'S WATERWAYS

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MARTIN COUNTY

The Intracoastal Waterway

The Atlantic Intracoastal Waterway (AICW) is a 1,391-mile channel between Trenton, New Jersey, and Miami, Florida. The Waterway along Florida's eastern seaboard is 406 miles long and follows coastal rivers and lagoons past numerous tourism-oriented communities. The channel is authorized to a depth of 12 feet from Nassau County to Fort Pierce, and a 10 foot depth south through Miami-Dade County. Boating activities on the waterways contribute to the existence of numerous marine-related businesses such as marinas and boatyards and have stimulated development of residential properties on the Waterways.

The Navigation District

The Florida Inland Navigation District, created in 1927, is the local sponsor for the AICW in Florida. In cooperation with the Jacksonville District of the U.S. Army Corps of Engineers, the Navigation District is responsible for maintenance of the AICW in Florida. To maintain navigation, the waterways need to be periodically dredged due to shoaling from currents, upland soil erosion, and the movement of offshore sands through the ocean inlets. Maintenance dredging is projected to cost approximately \$12 to \$16 million annually during the next 50 years, of which 50 percent of the costs are expected to be borne by property owners within the Navigation District's jurisdiction.

The Navigation District also partners with other governments to provide waterway access and improvement facilities for our mutual constituents. These projects include public boat ramps, marinas, side channels, parks, fishing piers, boardwalks, navigation aids, derelict vessel removal, shoreline stabilization, and waterway cleanups.

Source of Data Used in This Analysis

The economic benefits of the Waterways were estimated in June 2001 in *An Economic Analysis of the District's Waterways in Martin County*.

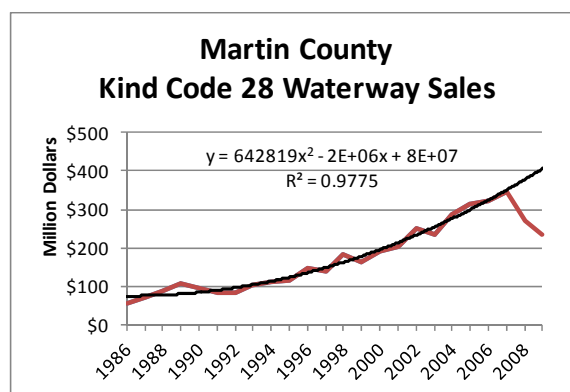
Updating of Previously Estimated Benefits

The benefits presented in this analysis were estimated by updating the direct marine-business

impacts in the original analysis to current values using the change in gross sales reported by boat dealers to the Florida Department of Revenue (FDOR). The updated direct impacts were used in conjunction with an IMPLAN input/output model to estimate total economic benefits.

Estimating the Impact of the Recession

The impact of the recession was estimated by determining the trend in gross sales of boat dealers over the 20-year period prior to the onset of the recession. This trend was used to estimate the theoretical gross sales if sales had continued to increase at the rates previously experienced. The red line in the figure below illustrates reported actual gross sales of boat dealers and the black line illustrates the trend of those sales. From 2007 to 2009 gross boat dealer sales in Martin County decreased by 32 percent; if the recession had not occurred, it is estimated that gross sales from 2007 to 2009 would have increased by 16 percent.



Annual Boater Spending on Gas, Food, and Drinks at Non-Marine-Related Establishments

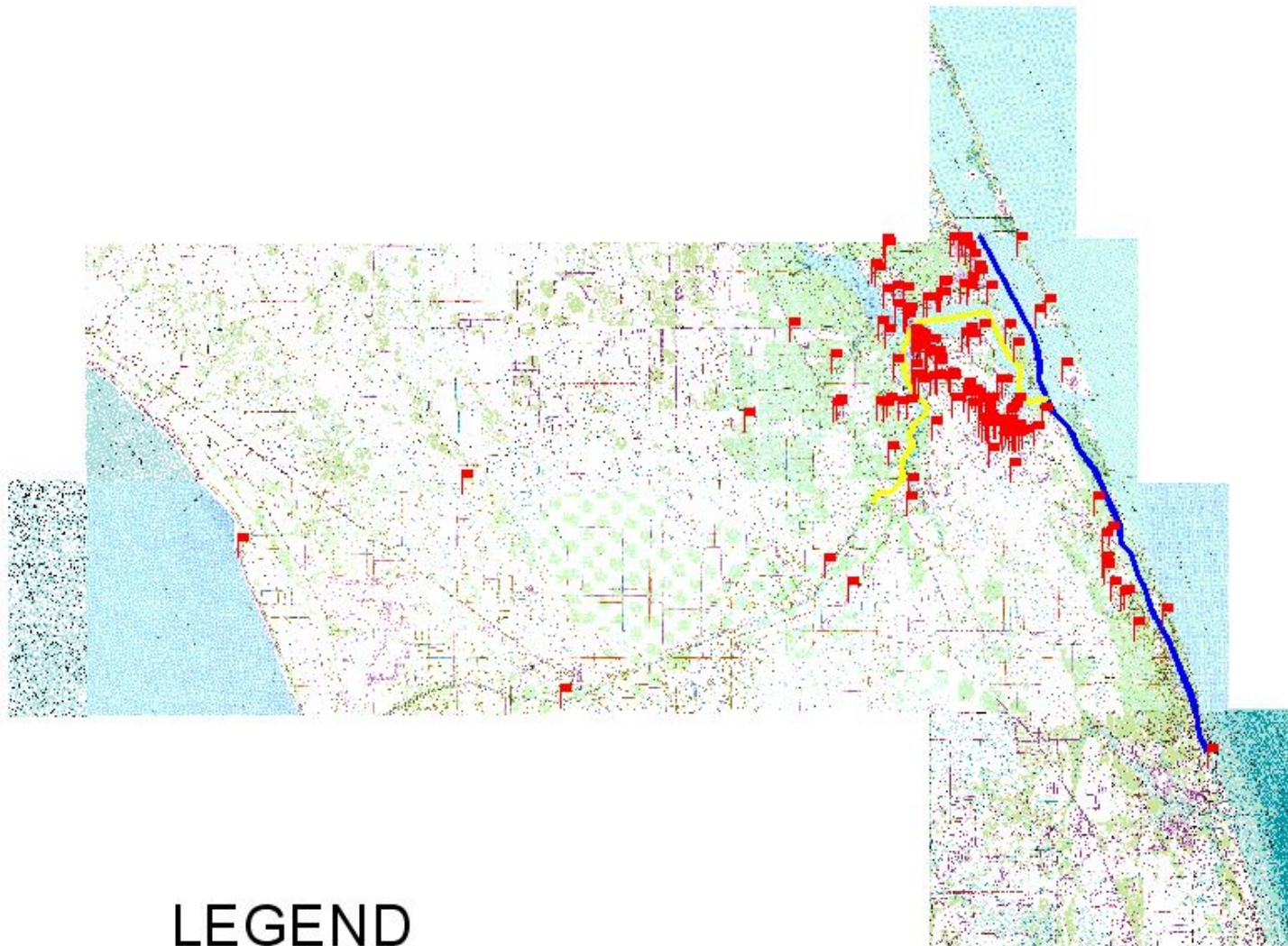
- Current existing conditions: \$24.9 million
- Cessation of maintenance: \$12.7 million
- Increased maintenance: \$24.9 million
- Assuming no recession: \$25.6 million

Vessel Draft Restrictions Assumed for Each Scenario

- Current existing conditions: 6.5 feet MLW
- Cessation of maintenance: 3 feet MLW
- Increased maintenance: 10 feet MLW
- Assuming no recession: 6.5 feet MLW

LOCATION MAP

MARTIN COUNTY WATERWAY RELATED BUSINESSES



LEGEND



**FLORIDA INLAND NAVIGATION DISTRICT WATERWAYS ASSISTANCE PROGRAM
1986-2016**

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PROJECT NAME	NUMBER	PROJECT'S SPONSER	AMOUNT	COST
Twin Rivers Park Shoreline Stabilization	MA-00-38	Martin County	\$299,000	\$626,000
Cross Roads Regional Artificial Reef - Phase I	MA-01-40	Martin County	\$15,000	\$30,000
Indian Riverside Park - Day Use Boat Slip Construction	MA-01-41	Martin County	\$87,500	\$175,000
Sandsprit Park Addition	MA-02-44	Martin County	\$200,000	\$492,000
Cross Roads Regional River Reefs Artificial Reef	MA-03-47	Martin County	\$25,000	\$50,000
Law Enforcement Marine Unit	MA-03-48	Martin County Sheriff's Office	\$33,050	\$66,100
Twin Rivers Park Shoreline Stabilization - Phase I	MA-03-49	Martin County	\$65,000	\$130,000
Twin Rivers Park Shoreline Stabilization - Phase I I	MA-04-51	Martin County	\$240,575	\$850,000
Twin Rivers Park Shoreline Stabilization - Phase I I I	MA-05-53	Martin County	\$300,000	\$650,000
Jensen Beach Boat Ramp Park	MA-06-54	Martin County	\$325,000	\$1,500,000
Manatee Pocket Channel Dredging - Phase I	MA-06-55	Martin County	\$165,000	\$220,000
Law Enforcement Marine Unit	MA-07-56	Martin County	\$30,000	\$85,000
Leighton Park	MA-08-57	Martin County	\$95,000	\$210,000
St. Lucie Inlet North Jetty Improvements	MA-08-58	Martin County	\$300,000	\$5,400,000
Mc-2 (Bird Island) Shoreline Stabilization - Phase I	MA-09-60	Martin County	\$75,000	\$150,000
St. Lucie Inlet Maintenance	MA-09-61	Martin County	\$339,035	\$6,339,035
Manatee Pocket Dredging (Grant Cancelled)	MA-1	Martin County	\$70,000	\$360,000
Manatee Pocket Channel Dredging	MA-10-63	Martin County	\$200,000	\$13,300,000
Manatee Pocket Commercial Dock Repl.-ph I (Expired)	MA-10-64	Martin County	\$12,000	\$24,000
Mc-2 Bird Island Shoreline Stabilization - Phase I I	MA-11-65	Martin County	\$150,000	\$500,000
Jensen Beach Managed Mooring Field (Withdrawn)	MA-12-67	Martin County	\$439,695	\$879,390
St. Lucie Inlet Maintenance	MA-13-69	Martin County	\$386,361	\$780,000
St. Lucie Inlet Maintenance	MA-14-71	Martin County	\$237,605	\$475,210
Phipps Park Shoreline Stabilization & Access	MA-15-73	Martin County	\$167,061	\$334,122
Charlie Leighton Park Accessible Floating Dock	MA-15-74	Martin County	\$60,000	\$120,000
Willoughby Creek Dredging	MA-2	Martin County	\$10,000	\$498,288
Jensen Beach Causeway Fishing Pier	MA-3	Martin County	\$38,000	\$76,050
Pecks Lake Park - Phase I	MA-4	Martin County	\$62,000	\$425,700
Pecks Lake Park - Phase I I	MA-87-5	Martin County	\$75,000	\$258,500
Willoughby Creek Dredging - Phase I I	MA-87-6	Martin County	\$23,500	\$465,000
Pendarvis Cove Park Improvements	MA-88-10	Martin County	\$90,000	\$227,700
Jensen Causeway Relief Channel Dredging	MA-89-11	Martin County	\$5,000	\$36,300

**FLORIDA INLAND NAVIGATION DISTRICT WATERWAYS ASSISTANCE PROGRAM
1986-2016**

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PROJECT NAME	NUMBER	PROJECT'S SPONSER	AMOUNT	COST
Sandsprit Park - Phase I	MA-91-14	Martin County	\$76,500	\$1,000,000
Pecks Lake Park - Phase I I I (Withdrawn)	MA-91-15	Martin County	\$135,000	\$867,000
Sandsprit Park - Phase I I	MA-92-17	Martin County	\$92,000	\$1,000,000
Timer Powers Park	MA-92-18	Martin County	\$217,500	\$435,000
Secondary Channel Marking For Nav. Aid & Res Protection	MA-93-19	Commissioners	\$12,555	\$22,535
Pendarvis Cove Park	MA-93-20	Commissioners	\$34,000	\$68,000
South County Boat Ramp - Phase I	MA-93-21	Commissioners	\$30,000	\$60,000
South County Boat Ramp Park - Phase I (Cancelled)	MA-94-22	Commissioners	\$220,000	\$660,033
St. Lucie Inlet Interior Shoal Dredging	MA-95-23	Martin County	\$75,000	\$160,000
Maritime Hammock Educational Boardwalk	MA-95-24	Oceanographic	\$107,000	\$150,547
Law Enforcement Patrol And Rescue Vessel	MA-95-25	Martin County Sheriff's Office	\$34,000	\$54,919
Jensen Beach Causeway Park	MA-95-26	Martin County	\$77,000	\$197,900
Leighton Park Improvements - Phase I	MA-96-27	Commissioners	\$198,575	\$479,150
Peck Lake Park Environmental Education Signage	MA-96-28	Commissioners	\$33,000	\$33,000
Cove Road Boat Dock (Project Expired)	MA-97-31	Martin County	\$37,000	\$74,000
St. Lucie Inlet Flood Shoal Dredging	MA-97-32	Commissioners	\$250,000	\$4,897,500
Mangrove Marsh Educ. Nature Trail & Observation Tower	MA-97-33	Oceanographic	\$90,450	\$118,600
St. Lucie Inlet Mgmt. Plan - Flood Shoal Dredging	MA-98-34	Commissioners	\$79,336	\$4,897,500
South County Boat Ramp Park - Phase I	MA-98-35	Commissioners	\$220,000	\$940,000
Safe Waterways	MA-JI-01-39	Town Of Jupiter Island	\$12,500	\$25,000
Jupiter Island Marine Patrol Waterway Safety	MA-JI-13-70	Town Of Jupiter Island	\$30,000	\$60,000
Floating Courtesy Dock At City Hall Pier	MA-ST-01-42	City Of Stuart	\$137,500	\$275,000
Rehabilitation Of Anchorage T-dock	MA-ST-01-43	City Of Stuart	\$12,500	\$50,000
Southpoint Anchorage & Marina Seawall Restoration	MA-ST-02-45	City Of Stuart	\$75,000	\$150,000
South Municipal Marina - Phase I (Expired)	MA-ST-02-46	City Of Stuart	\$27,000	\$55,000
Day Markers For Channel At Southpoint (Withdrawn)	MA-ST-03-50	City Of Stuart	\$6,000	\$12,000
Floating Courtesy Dock At City Hall Pier - Phase I I	MA-ST-04-52	City Of Stuart	\$150,000	\$300,000
Courtesy Dock Impr.&riverwalk Enhancements-phase I I I	MA-ST-08-59	City Of Stuart	\$107,973	\$295,945
Shepard Park Boat Ramp Reconstruction & Dredging	MA-ST-09-62	City Of Stuart	\$80,000	\$160,000
Floating Docks Fire Protection	MA-ST-11-66	City Of Stuart	\$25,000	\$50,000
City Of Stuart Riverwalk Expansion - Phase I	MA-ST-13-68	City Of Stuart	\$40,000	\$80,000
Riverwalk Expansion - Phase II	MA-ST-14-72	City of Stuart	\$232,334	\$464,668

**FLORIDA INLAND NAVIGATION DISTRICT WATERWAYS ASSISTANCE PROGRAM
1986-2016**

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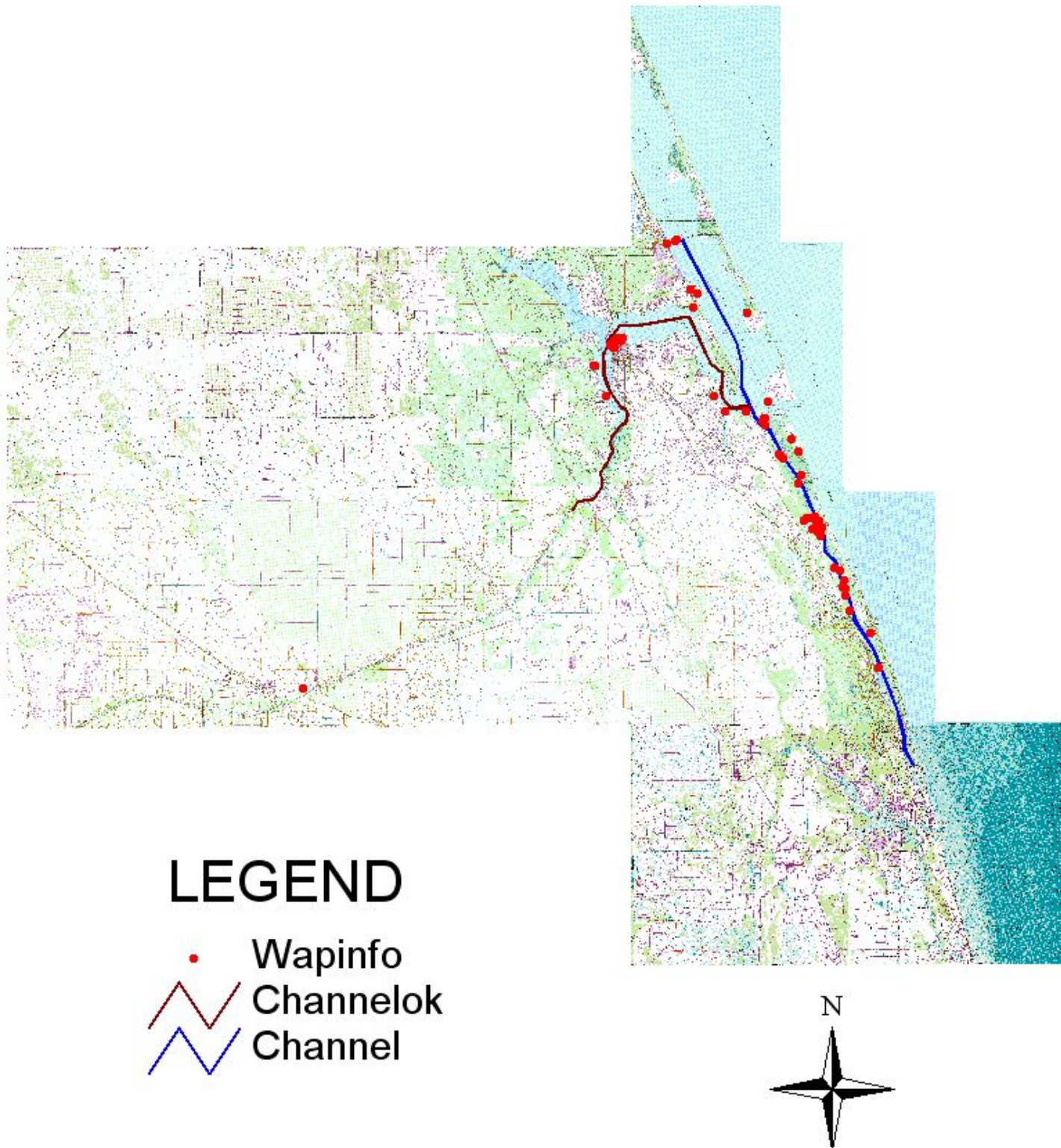
PROJECT NAME	NUMBER	PROJECT'S SPONSER	AMOUNT	COST
Shepard Park Improvements	MA-ST-15-75	City of Stuart	\$232,158	\$494,000
Riverwalk & Pier	MA-ST-87-7	City of Stuart	\$50,000	\$191,400
Shepard Park Fishwalk	MA-ST-87-8	City of Stuart	\$20,000	\$40,000
Stuart Riverwalk	MA-ST-88-9	City of Stuart	\$50,000	\$329,400
Riverwalk	MA-ST-89-12	City of Stuart	\$200,000	\$917,270
Stuart Riverwalk	MA-ST-90-13	City of Stuart	\$175,000	\$350,000
River Boardwalk - Phase VI	MA-ST-91-16	City of Stuart	\$67,500	\$150,000
Courtesy Dock At City Hall Pier (Withdrawn)	MA-ST-96-29	City Of Stuart	\$11,000	\$22,000
Construction Of Stuart Anchorage - Phase I	MA-ST-98-36	City Of Stuart	\$25,000	\$50,000
Stuart Southpoint Anchorage - Phase I I	MA-ST-99-37	City Of Stuart	\$312,750	\$625,500
Shepard Park Improvements Part 2	MA-ST-16-76	City of Stuart	\$137,750	\$275,500
Phipps Park Shoreline	MA-16-77	Martin County	\$281,771	\$700,000
Martin County Sheriff's Marine Patrol Vessel	MA-16-78	Martin County Sheriff's Office	\$60,000	\$150,000

TOTALS:

\$9,198,034

\$58,121,762

**WATERWAYS ASSISTANCE PROGRAM PROJECTS
IN MARTIN COUNTY**





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Jacksonville District

**IWW STATUS UPDATE
FIND Board of Commissioners Meeting
November 19, 2016**



WORK ACTIVITIES IN FY 16:

1. IWW: St. Augustine and Matanzas (St. Johns County)
2. DMMA O-7 (Martin County)
3. DMMA O-23 (Martin County)
4. IWW: Crossroads (Martin County)
5. IWW: Broward Reach 1 (Broward County)
6. IWW: Bakers Haulover (Miami Dade County)



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IIWW STATUS UPDATE FIND Board of Commissioners Meeting November 19, 2016



AIWW = Atlantic Intracoastal Waterway Norfolk to St. Johns
IWW = Intracoastal Waterway Jacksonville to Miami (12' and 10' projects)
DMMA = Dredge Material Management Area

1. WORK ACTIVITY: IWW St. Augustine / Matanzas (St. Johns County)

CONTRACT AMOUNT: TBD

DESCRIPTION OF WORK: Completion of plans and specifications and administration of the contract for O&M dredging in the St. Augustine and Matanzas reaches of the IWW located in St. Johns County.

SCHEDULE:

- | | |
|--|---------------|
| • Obtain Survey | 6 Nov 2015A |
| • Initiate P&S | 16 Nov 2015A |
| • Verify NEPA/FDEP permit | 16 Nov 2015A |
| • Complete Draft P&S including reviews | 16 May 2016A |
| • Advertise Contract | 28 July 2016A |
| • Bid Opening | 30 Aug 2016A |
| • Contract Award | 30 Sept 2016A |
| • Notice to Proceed | 2 Nov 2016A |

FIND WORK ORDER: P&S were funded 100% with Federal funding in 2016. A work order was presented to and approved by the FIND Board in April 2016 to fund a portion of the dredging contract. Dredging will be paid for with a combination of FIND Contributed Funds and COE federal funding.

NAME OF CONTRACTOR: Contract was awarded to Weeks Marine on 30 Sept 2015 in the amount of \$8,896,500.

STATUS:

St. Augustine: Contract was advertised on 28 July 2016 and awarded to Weeks Marine on 30 Sept 2016. NTP was issued on 2 November 2016. Based on damages incurred in the Vilano/South Ponte Vedra beach area from Hurricane Matthew, FIND has requested that the Corps investigate the cost of changing our placement area for the St. Augustine reach to the beach north of the Inlet in lieu of placing on the State Park south of the Inlet. Cost Estimating completed the task on 4 Nov 2016 and PM requested the official RFP be sent over to Weeks on 7 Nov 2016. If FIND agrees to fund the change, the IWW FDEP permit will be modified to add a placement area and the County will finalize required easements. Weeks Marine has confirmed that they would not seek monetary compensation for delaying commencement of dredging operations until February 2017, in an effort to allow a decision to be made on the shift in placement area and the need to obtain the permit mod and real estate interests.

IWW Matanzas: The new breach in the Summer Haven area is located approximately 200 yards north of our placement area, and at the exact location where we had envisioned the pipeline to come over to the beach. Taylor Engineering is currently under contract with the County for the Summer Haven River restoration and they have also been contracted to close the breach. The breach should be closed before commencement of dredging in February 2017.



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**IWW STATUS UPDATE
FIND Board of Commissioners Meeting
November 19, 2016**



2. WORK ACTIVITY: DMMA O-7 (Martin County)

CONTRACT AMOUNT: TBD

DESCRIPTION OF WORK: Finalization of plans and specifications, environmental coordination, procurement and administration of the construction contract for DMMA O-7.

SCHEDULE O-7:

- Contract Advertisement Initiated: 13 May 2016A
- Bid Opening: 14 Oct 2016
- Contract Award: 22 Nov 2016

FIND WORK ORDER: Funding for completion of P&S was funded with 100% Federal funding. A work order for construction funds was presented and approved at the March 2016 FIND Board.

NAME OF CONTRACTOR: TBD

STATUS: The contract for construction of DMMA O-7 was advertised on 13 May 2016 with the contracting tool being small business set aside least cost technically acceptable. Due to the need to amend the contract a multitude of times to address issues associated with the access road, bids were delayed until 14 Oct 2016. The source selection team was convened on 7 Nov 2016 to begin the review process of bids received. Award is currently tracking for 22 Nov 2016.



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**IWW STATUS UPDATE
FIND Board of Commissioners Meeting
November 19, 2016**



3. WORK ACTIVITY: DMMA O-23 (Martin County)

CONTRACT AMOUNT: TBD

DESCRIPTION OF WORK: Development of Plans and Specifications for the construction of DMMA O-23 which is located in Martin County, Florida.

SCHEDULE: (Tentative)

- | | |
|---------------------------------|--------------|
| • Initiate P&S | 1 Dec 2016 |
| • Complete NEPA | 30 May 2017 |
| • Complete P&S with all reviews | 25 July 2017 |
| • Advertise Contract | 1 Aug 2017 |
| • Receive Bids | 1 Sept 2017 |
| • Contract Award: | 1 Oct 2017 |

FIND WORK ORDER: P&S will be funded 100% with Federal funding. Construction of DMMA O-23 will be with FIND Contributed Funds.

NAME OF CONTRACTOR: TBD

STATUS: P&S will kick off once DMMA O-7 is awarded. O-23 will utilize the same weir system as O-7. There is a federally listed plant, reindeer lichen, which grows in scrub areas which is present on the site. Probably 10-20 sf of the species is estimated to be present which will have to be relocated out of the construction area.

Anticipate a work order being presented to the FIND Board in June 2017 for construction funding.



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IWW STATUS UPDATE
FIND Board of Commissioners Meeting
November 19, 2016



4. WORK ACTIVITY: IWW Crossroads (Martin County)

CONTRACT AMOUNT: TBD

DESCRIPTION OF WORK: Staff has identified a small problematic shoal within IWW Crossroads and has asked that the Corps investigate the most efficient way to remove it.

FIND WORK ORDER: Depending on the order of magnitude for dredging, a FIND work order may be required. Amount to be determined.

NAME OF CONTRACTOR: TBD

STATUS: There is approximately 10k cubic yards of material in a problematic shoal within the Crossroads reach of the IWW. The plan is to utilize a Corps dredge to remove this small quantity. An EA and FONSI have been completed that evaluated material being dredged from the IWW and placed within the settling basin at St. Lucie Inlet.

After coordination with FDEP, it was determined that the best path forward is to proceed with a MINOR modification to the existing IWW Crossroads permit instead of modifying Martin County's permit. A modification to FDEP permit No. 0296970-001-JC has been submitted. RAI #1 was received on 29 July 2016 and responded to on 17 August 2016. The application as deemed complete and we anticipate receiving the modification by end of Dec 2016.

Once the permit modification is received, project management will coordinate with the Wilmington District to schedule a time for them to proceed with removal of this shoal. May look to combining the effort with 2-3 other small jobs so that mobilization can be split and minimized.



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**IWW STATUS UPDATE
FIND Board of Commissioners Meeting
November 19, 2016**



5. WORK ACTIVITY: IWW Broward Reach 1 (Broward County)

CONTRACT AMOUNT: TBD

DESCRIPTION OF WORK: A hydro survey was performed by Morgan and Ecklund and provided to the Corps on 26 June 2014. There is approximately 50k cy of material located within the federal channel down to 10' and 80k cy down to 10'+2'. Even at 80k cy, given the small quantity, the most cost effective way to pursue the dredging would be utilization of a Corps of Engineers dredge, either the Currituck or Murden, and dispose of in the nearshore.

SCHEDULE Broward Reach 1:

- | | |
|---|---------------|
| • Complete Environmental Assessment (incl public reviews) | 27 March 2017 |
| • Obtain Water Quality Certification | 27 March 2017 |
| • Obtain updated Survey | 1 Dec 2016 |
| • Provide Dredge Orders to SAW | 1 April 2017 |

FIND WORK ORDER: Current path forward is to proceed with dredging with a Wilmington Hopper dredge unless it is determined that the quantity is large enough to justify a standard procurement. Anticipate a work order being presented to the FIND Board in February 2017 for dredging funding.

NAME OF CONTRACTOR: TBD

STATUS: Based on preliminary discussions with the Hillsboro Inlet Chairman, Mr. Jack Holland, it has been suggested that we consider an alternative that requires us dredging material from the IWW and placing in within the existing Hillsboro Inlet Settling Basin. This would alleviate the need to obtain NEPA on a new nearshore placement area. The Hillsboro Inlet dredge would then move the material to the dry beach south of the Inlet. In order to do this we would need to modify Broward County's permit (JCP 0229394-001-JC) or obtain a new stand alone permit. NEPA would also need to be addressed since IWW material has never been placed in this disposal location. Staff and the Corps plan to meet with Mr. Holland before the December Board meeting.



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IWW STATUS UPDATE
FIND Board of Commissioners Meeting
November 19, 2016



6. WORK ACTIVITY: IWW Bakers Haulover (Miami Dade County)

CONTRACT AMOUNT: TBD

DESCRIPTION OF WORK: Bakers Haulover continues to be one of the highest frequency dredging needs within the IWW. The current plan is to take advantage of regional sediment management practices and dredge Bakers Haulover as a borrow source for the Miami Dade County shore protection project.

FIND WORK ORDER: N/A

NAME OF CONTRACTOR: TBD

STATUS: Dredging of the Bakers Haulover Reach of the IWW will be accomplished in conjunction with the Miami Dade SPP. Material from the IWW will be placed on the beach in Sunny Isles. This is one of the 2 highest shoaling areas in the IWW and is in need of dredging asap. Permit modification was received on 28 Sept 2016 which extends the existing permit until 21 Oct 2020; however, another FDEP permit modification will be needed to add Sunny Isles as a placement area for our O&M material. Discussions are underway on the contracting tool since the contract could involve both truck haul of material and pipeline dredging. Anticipate a contract advertisement for Sunny Isles in April 2017.



November 4, 2016

Mr. Mark Crosley
Executive Director
Florida Inland Navigation District
1314 Marcinski Rd
Jupiter, FL 33477

RE: Scope of Professional Engineering Services
Final Design and Bid Administration for Crossroads Maintenance Dredging
Martin County, Florida

Mr. Crosley:

Per your request, Taylor Engineering has prepared the attached Scope of Services (Attachment A) and Cost Proposal (Attachment B) for the following services related to the proposed maintenance dredging of Martin County Crossroads, including newly permitted wideners and adjacent areas of the Intracoastal Waterway:

Task 1 – Final Design and Bid Documents
Task 2 – Bid Administration

Dredged material disposal for the project will occur within the FIND's Dredged Material Management Area (DMMA) M-5 facility. This proposal includes costs for coordination of burrowing animal removal and backfill of burrows, specifications for clearing and grubbing the interior of the DMMA, and an engineer's assessment of stabilization measures for the deficient DMMA outfall structure. A figure depicting the project area has been included with this proposal.

Taylor Engineering will complete the work described herein for a lump sum fee of \$39,357.60. Please contact me with any questions you have regarding this effort. We can begin work upon your notice to proceed.

Sincerely,

Jerry Scarborough, P.E.
Senior Advisor, Waterfront Engineering
Attachments (2)
Figure (1)

**SCOPE OF PROFESSIONAL ENGINEERING SERVICES
FINAL DESIGN AND BID ADMINISTRATION FOR
CROSSROADS MAINTENANCE DREDGING
MARTIN COUNTY, FLORIDA**

INTRODUCTION

This scope of professional engineering services describes Taylor Engineering's effort to support a maintenance dredging the Martin County Crossroads, including initial dredging of recently permitted wideners on the eastern end of Okeechobee Waterway (OWW) Cut 1 (OWW-E) in Martin County, Florida (Figure 1). Dredged material disposal will occur within the FIND's Dredged Material Management Area (DMMA) M-5 facility.

In July 2015 Taylor Engineering, under contract to the FIND, completed the *Okeechobee Waterway Cut 1 Sediment Basin Feasibility Study*. That study identified and analyzed the existing features, hydrodynamics, and sediment transport characteristics of the OWW Cut 1 area to determine whether alternatives exist that would reduce sediment inflow into OWW Cut 1. The conclusions in that report supported proceeding with the permitting, engineering, and design of a sediment basin, or channel wideners, as shown on Figure 1.

In December 2015, FIND authorized Taylor Engineering to complete preliminary design, submerged aquatic vegetation survey coordination, and permitting for the channel wideners. During the preliminary design effort, Taylor Engineering completed a reconnaissance visit to DMMA M-5. This visit documented some deficiencies with DMMA M-5 and recommended investigation of measures to stabilize the DMMA outfall structure, trapping and removing gopher tortoises and mammals that have burrowed into the DMMA embankments and interior, backfilling and repairing the excavated burrows, and removing a substantial amount of woody vegetation from the DMMA embankments.

This proposal is for final design through bid administration services in support of initial dredging of the newly permitted wideners as well as maintenance dredging of the previously permitted cuts of the OWW and Intracoastal Waterway (ICWW) at Martin County Crossroads. This proposal includes coordination of burrowing animal removal and backfill of burrows, specifications of clearing and grubbing the interior of the DMMA, and an engineer's assessment of temporary stabilization measures for the DMMA outfall structure.

ASSUMPTIONS

Taylor Engineering has developed this scope of services based on the following assumptions:

- Recent survey data received from the U.S. Army Corps of Engineers is sufficient for design level drawings of ICWW and OWW in the project area.
- Previously suggested vegetation removal at DMMA outside of the containment basin and embankments will not occur in conjunction with this Crossroads dredging project.
- Permanent weir outfall replacement at DMMA M-5 will not occur as part of this Crossroads dredging project. A temporary measure may be suggested based on engineer assessment.
- FIND will contract directly with a contractor to remove and relocate gopher tortoises. Taylor will coordinate scopes of work, permitting, and other related tasks for FIND.
- The interior of the DMMA will need to be cleared and grubbed prior to placement of dredged material during this upcoming maintenance dredging event.

If any of these assumptions prove incorrect, Taylor Engineering will work with the FIND to develop an appropriate additional scope of services and cost. Finally, this proposal excludes permit fees, bid and contract administration, and construction administration and observation services.

TASK 1 FINAL DESIGN AND BID DOCUMENTS

1.1 Final Design and Construction Drawings

We will prepare final design documents and construction drawings for the various site elements. Construction drawings will provide plan, cross-section, and detail views of the ICWW and OWW dredging areas and pertinent details (e.g., pipeline corridor, contractor staging area, weir location and discharge area, etc.) of the DMMAs. We will coordinate with FIND to secure a subcontractor, under direct contract to FIND, to remove and relocate burrowing mammals and tortoises. The excavated burrows will be repaired by the selected dredging contractor. Technical specifications will include provisions for backfill and re-compaction of the excavations. We will provide a record set of construction drawings in appropriate hardcopy format and in digital (AutoCAD) format, as well as record drawings signed and sealed by a Florida Registered Professional Engineer.

1.2 Contract Documents and Technical Specifications

We will update the Division 00 and 01 Contract Documents and prepare all other Technical Specifications for the project. We will follow the Engineers Joint Contract Documents Committee (EJCDC) and Construction Specification Institute (CSI) standards and guidelines to prepare the specifications.

1.3 Bid Package Submittal

We will prepare a bid package (including contract documents, technical specifications, and bid schedule) with estimated quantities for all bid items. To give interested bidders immediate access to the bid package, we will provide an electronic copy of the final drawings and specifications for the FIND to upload onto its FTP site.

1.4 Opinion of Probable Cost

We will prepare and submit an opinion of probable cost for the project.

TASK 2 BID ASSISTANCE

Taylor Engineering will help the FIND administer the bidding process and select the contractor. We will remain available at our Jacksonville and West Palm Beach offices to clarify and interpret project documents and prepare addenda, if required. We will coordinate and attend a pre-bid meeting to answer questions concerning elements of the project for which Taylor Engineering is responsible.

We will review the bids received and provide the FIND with our recommendations for contractor selection. This work includes reviewing the submitted bid documents, checking technical references of the responsive bidders, and preparing and transmitting a written recommendation for contractor selection. Taylor Engineering will limit its review and recommendations to engineering and technical issues. The FIND will take responsibility for legal review and evaluation of contractors' financial condition, business licenses or authorizations, bonding, contractual requirements, and any other non-engineering or non-technical information.

ESTIMATED SCHEDULE

We anticipate completing this scope of work within four months from notice to proceed, as shown in the table below.

No.	Task	Months from Notice to Proceed			
		1	2	3	4
1	Final Design and Bid Documents				
2	Bid Assistance				

TAYLOR ENGINEERING, INC.
COST SUMMARY BY TASK
P2016-059: FIND CROSSROADS

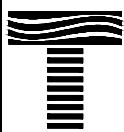
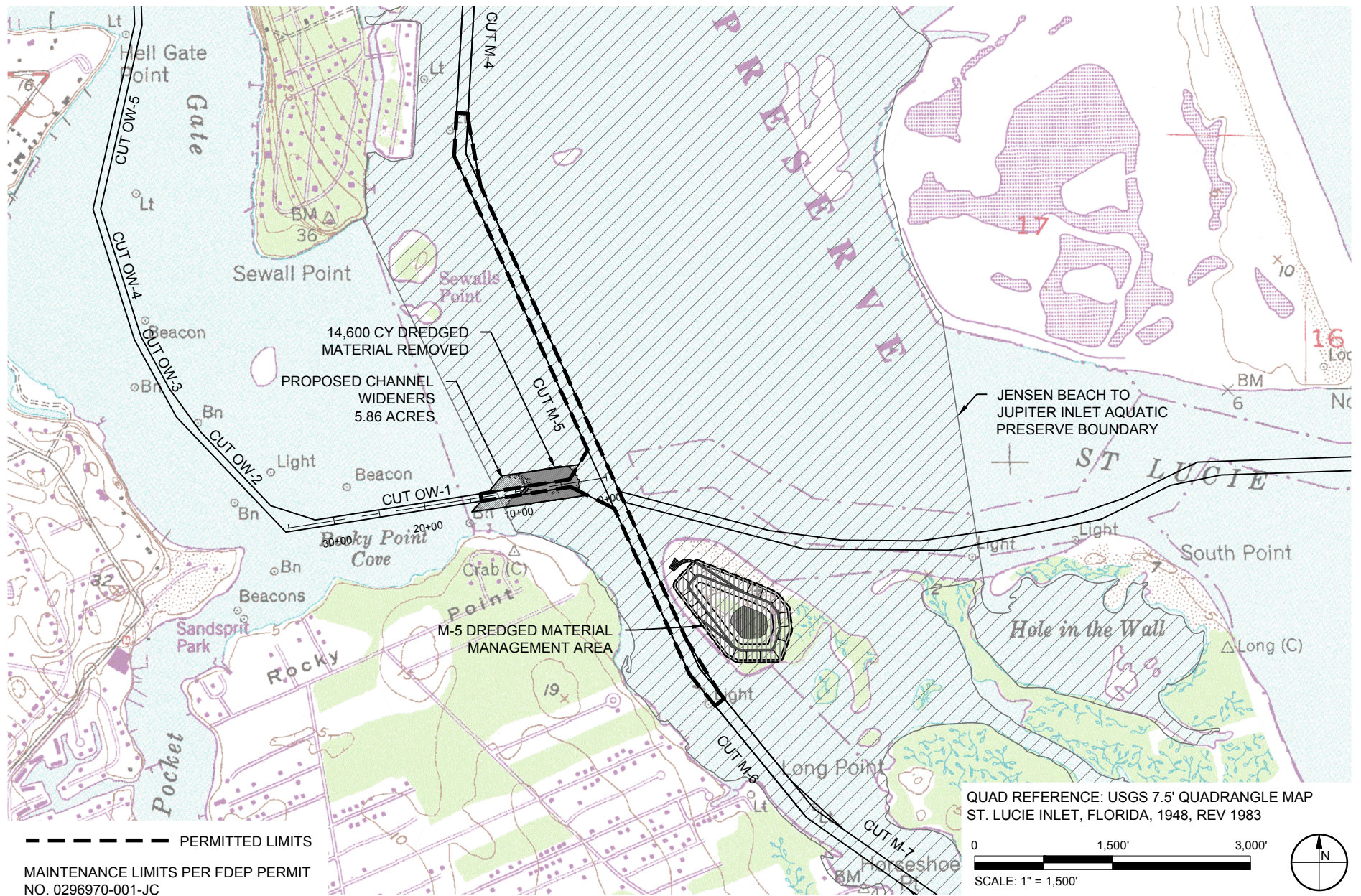
TASK 1: FINAL DESIGN AND BID DOCUMENTS

<i>Labor</i>	Hours	Cost	Task Totals
Vice President	4.0	748.00	
Senior Advisor	8.0	1,504.00	
Director	20.0	3,320.00	
Senior Professional	76.0	11,248.00	
Project Professional	40.0	4,360.00	
Technical Editor	6.0	612.00	
Senior CAD Designer	48.0	5,424.00	
Administrative	12.0	624.00	
Total Man-Hours	214.0		
Labor Cost			27,840.00
<i>Non-Labor</i>	Units	Cost	
DMMA Site Visit car milage	500.0	270.00	
DMMA Site Visit per-diem	2.0	128.00	
DMMA Site Visit lodging	2.0	220.00	
Non-Labor Cost		618.00	
Fee @ 10.0%		61.80	
Total Non-Labor Cost			679.80
<i>Total Task 1</i>			28,519.80

TASK 2: BID ADMINISTRATION

<i>Labor</i>	Hours	Cost	Task Totals
Senior Advisor	6.0	1,128.00	
Director	5.0	830.00	
Senior Professional	32.0	4,736.00	
Project Professional	28.0	3,052.00	
Technical Editor	2.0	204.00	
Administrative	4.0	208.00	
Total Man-Hours	77.0		
Labor Cost			10,158.00
<i>Non-Labor</i>	Units	Cost	
Pre-app meeting car milage	500.0	270.00	
Pre-app meeting per-diem	2.0	128.00	
Pre-App meeting lodging	2.0	220.00	
Non-Labor Cost		618.00	
Fee @ 10.0%		61.80	
Total Non-Labor Cost			679.80
<i>Total Task 2</i>			10,837.80

Project Total \$ 39,357.60



TAYLOR ENGINEERING INC.

10151 DEERWOOD PARK BLVD.
BLDG. 300, SUITE 300
JACKSONVILLE, FL 32256
CERTIFICATE OF AUTHORIZATION # 4815

FIGURE 1
PROJECT OVERVIEW
FIND OWW CROSSROADS CHANNEL WIDENER
MARTIN COUNTY, FLORIDA

PROJECT	C2015-064
DRAWN BY	CAS
SHEET	1 of 5
DATE	FEBRUARY 2016

SEAL

DRAFT

JOHN F. ADAMS P.E. # 20298

DATE

CESAJ (1145)

18 DEC 2013

MEMORANDUM FOR RECORD

SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

1. **PURPOSE:** This memorandum updates the U.S. Army Corps of Engineers, Jacksonville District, (Corps) guidance regarding setback policy (Guidance) for structures along the Federal channels, established by memorandum dated 23 November 1998. On 18 July 2013, updated Guidance was established which is revised herein as a result of public comments received. In accordance with Section 10 of the Rivers and Harbors Act of 1899 (R&HA Section 10), the Corps has the authority to regulate any obstruction to the navigable capacity of waters of the United States not affirmatively authorized by Congress. Furthermore, in accordance with Section 14 of the Rivers and Harbors Act of 1899 (33 USC § 408) (Section 408), the Corps has the authority to review requests for modifications to Federal projects by non-Federal interests. The purpose of this Guidance is to advise the public and Jacksonville District personnel of the manner in which Jacksonville District intends to exercise its authority under the R&HA to (1) maintain the navigable capacity of the Federal channels by minimizing both potential hazards to navigation and impediments to maintenance dredging created by structures located along the edges of the channels and (2) ensure that proposed modifications do not impair the usefulness of a Federal project and are not injurious to the public interest. It is the Corps' intent to equitably manage the construction of structures along the Federal channels so that, to the extent practicable, property owners achieve some form of structural access while ensuring the Government's ability to maintain such waterways and the public's rights to safe navigation.

2. **DEFINITIONS:** The following terms are defined for the purpose of this memorandum. See Appendices for diagrams depicting some of the following terms.

a. **channel** – Those areas within the waterways listed in Paragraph 3 of this Guidance that are part of a Congressionally authorized federal navigation project. This refers to the area located between the design edges.

b. **currently serviceable** – A structure is considered to be currently serviceable if it is usable as is but not so degraded or damaged as to essentially require reconstruction.

c. **design depth** – The depth to which the Corps has been authorized to maintain the Federal channel by the enabling legislation for that Federal project. This defines the depth parameter of the area between the design edges of the Federal channel with respect to mean lower low water elevations.

d. **design edge** – The surveyed edges of a Federal channel which the Corps has been authorized to maintain by the enabling legislation for that Federal project. The distance between the design edges defines the width of the Federal channel.

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SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

- e. hydrographic survey – A survey designed to present an accurate, measurable description of the submerged terrain. The survey must show a plan view of the entire project area, including all areas where structures and mooring will be located, arranged in such a manner as to show lines of equal depth with respect to mean lower low water elevation. See Appendix A for more information on this requirement.
- f. mean lower low water (MLLW) – The average of the lower low water height of each tidal day observed over the National Tidal Datum Epoch (NTDE). The NTDE is actively considered for revision every 20 to 25 years and the present NTDE is 1983 through 2001.
- g. most waterward – This is considered to be that portion of a structure which lies closest to the near design edge of the federally authorized navigational channel. For purposes of measuring the setback of a structure, this point could be, but is not limited to, mooring pilings, decking, boat hoist/lifts or the roof line.
- h. near design edge - The design edge of the federally authorized navigational channel that is closest to the work/site.
- i. riprap – For this document, riprap is considered to be a type of revetment. Revetments are generally sloping structures placed along a shoreline in such a way as to absorb the energy of incoming waves. A riprap revetment is generally constructed of concrete or stone rubble of varying sizes.
- j. setback – The area between the near design edge of a federal channel extending horizontally 100 feet landward.
- k. side slope – The inclined area located adjacent to and outside of the established design edges of the Federal channel going from the design depth at the near design edge to the top edge of slope.
- l. structures – Any pier, boat dock, boat ramp, wharf, dolphin, weir, boom, breakwater, bulkhead, revetment, riprap, jetty, artificial island, artificial reef, permanent mooring structure, power transmission line, permanently moored floating vessel, piling, aid to navigation, or any other obstacle or obstruction.
- m. submerged bulkhead – Any structure constructed such that it extends into the bottom contour (natural or dredged) of the waterbody and is partially or completely submerged at mean low water, which is designed to stop/reduce shoaling within the boat mooring area. Submerged bulkheads are not considered to be essential to support the integrity of the associated dock structure.

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SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

n. top edge of slope – The reference point determined by multiplying the design depth of the channel by the channel slope; a slope ratio of 3:1 will be utilized for this guidance. The resulting value gives the horizontal distance from the design edge to the top edge of slope. For example, when the design depth of a channel is 10 feet, then design edge will be 30 feet from the top edge of slope.

o. terminal platform - A terminal platform is that part of a structure located at or near the waterward terminus of the structure which is designed to secure and load or unload a vessel or conduct other water dependent activities but which may also be used for other non-water dependent activities.

p. wave break - Wave breaks are defined as boards, sheet piling or any materials that are constructed as part of docks, boat and anchoring locations, walkways, and fishing piers that are not necessary for the integrity of the structure. The purpose of these structures is to stop/reduce shoaling within the boat mooring area and minimize wave damage.

q. X&Y State Plane Coordinates (X&Ys) - A set of two coordinates which describe a unique point based on the State Plane Coordinate System. The X&Ys are utilized to identify the location of the most waterward corners of a structure, proposed or existing. These coordinate points are used by the Corps to determine the horizontal distance between the most waterward portion of the structure and the near design edge of the Federal channel. See Paragraph 4.b. (page 4) and Appendix A for more information X&Ys as they pertain to this Guidance.

3. APPLICABILITY:

a. This Guidance supersedes all previous setback guidance and memoranda for structures issued by the Corps' Jacksonville District and will be applicable to all structures located within the setback along the following Federal navigation project waterways:

Atlantic Intracoastal Waterway from the Florida State line south to the St. Johns River,
Intracoastal Waterway on the east coast of Florida, from the St. Johns River south to Miami,

Intracoastal Waterway on the west coast of Florida, from the Caloosahatchee River north to the Anclote River,

Okeechobee Waterway, those portions not covered by Regional General Permit number 67. This would include the waterway from the W.P. Franklin Lock and Control Structure west to

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SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

Punta Rassa and from the St. Lucie Lock and Dam east to its intersection with the east coast Intracoastal Waterway.

- b. This Guidance may be modified as necessary to ensure the navigable capacity of the above-listed waterways.
- c. This Guidance does not create any rights or obligations.
- d. Nothing in this Guidance shall limit the ability of the Corps to issue, modify, suspend, revoke, or deny any individual permit or general permit nor shall this Guidance limit the Corps' ability to exercise its enforcement authority under the Rivers and Harbors Act of 1899.

4. GENERAL INFORMATION:

a. All applications seeking authorization for the construction of structures at or within the setback of a Federal channel will indicate the mooring locations of vessels expected to use the structure. If mooring to the waterward side of the terminus of the structure is proposed and no mooring pilings are included in the application, a minimum vessel beam width of 10 feet will be applied to establish the setback distance of the structure. A larger beam width may be utilized if information is provided or obtained which indicates larger vessels will be docked at the structure.

b. All applications seeking authorization for the construction of structures to be located at or within the setback of a Federal channel will be required to supply X&Ys for the most waterward points of the structure. Additionally, all authorized projects will be required to submit an as-built survey with X&Ys upon completion of the work for review and verification by the Corps' Regulatory Division. See Appendix A for further information regarding the requirements associated with the submittal of X&Ys.

c. All applications seeking authorization for the construction of docking and mooring structures to be located at or within the setback (as defined in Paragraph 5 below) of a Federal channel will be required to supply a hydrographic survey of the location of the structure. See Appendix A for further information regarding the requirements associated with a hydrographic survey. Generally, vertical bulkheads, information signs, and aids to navigation will not require a hydrographic survey; however, on a case-by-case basis the Corps may require a hydrographic survey for other structures to be located at or within the setback.

d. Provided structures along those Federal channels identified in Paragraph 3 above comply with this Guidance, no project-specific approval under Section 408 will likely be required.

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SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

d. Provided structures along those Federal channels identified in Paragraph 3 above comply with this Guidance, no project-specific permit under Section 408 will likely be required.

5. SETBACK GUIDANCE: The setback for all structures constructed along the channel of the Federal navigation project waterways identified in Paragraph 3 of this guidance is 100 feet from the near design edge of the channel. All permit applications seeking authorization for the construction of structures within 100 feet of the near design edge of the Federal channel will be reviewed by the Corps and are subject to review by the non-Federal, local sponsor of the affected portion of the Federal channel. During the review and evaluation of all applications seeking authorization for the construction of structures within the setback, every effort will be made to locate any structure authorized as far from the near design edge of the channel as practicable.

6. VARIANCES TO SETBACK:

a. All applicants requesting a variance to the 100-foot setback must provide justification for allowance of a potential impact to both the navigable capacity of the channel and the Federal Government's ability to maintain the channel. The information supplied to provide justification may include, but is not limited to, the horizontal distance between the shoreline and the edge of the federal channel, the actual water depth at MLLW, the presence of submerged vegetation or shoreline vegetation (i.e. sea grasses or mangrove fringe), public health and safety issues, and the proposed use of the structure. All of the information supplied to justify authorization of the structure will be used during the review and evaluation of the work proposal.

b. If the applicant is seeking authorization to place a structure such that any portion of it will be located within 62.5 feet of the near design edge of the channel and the justification provided is inadequate water depth, the applicant must show that dredging an access channel to allow placement of the structure more than 62.5 feet from the near design edge of the channel has been denied by a local, state or federal agency with the authority to regulate the dredging activity.

c. In no case will any structure be allowed to be constructed such that any portion of it, or any vessel utilizing it, is located closer to the near design edge of the Federal channel than the top edge of the slope plus 2 feet. For example, if the top edge of slope is located 30 feet from the near design edge of the federal channel, no structure will be authorized such that any portion of that structure is located closer than 32 feet (30 feet + 2 feet) from the near design edge of the channel. The following table provides information on the design depth and minimum clearance requirements for each of the Federal Channels listed in Paragraph 3.a.

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SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

Federal Channel	Limits	Design Depth	Minimum Clearance Requirement ((Design DepthX3) + 2)
Atlantic Intracoastal Waterway	Florida State line to St. Johns River	12	38
Intracoastal Waterway	St. Johns River to Ft. Pierce.	12	38
Intracoastal Waterway	Ft. Pierce to Miami	10	32
Okeechobee Waterway	IWW (at St. Lucie Inlet) to St. Lucie Lock and Dam	8	26
Okeechobee Waterway	W.P. Franklin Lock and Dam to RR Bridge at Tice	8	26
Okeechobee Waterway	RR Bridge at Tice to Punta Rassa	10	32
Intracoastal Waterway	Caloosahatchee River to Anclote River (including Sunshine Skyway Channel)	9	29
Intracoastal Waterway	Cats Point Channel (eastern side of Ciega Bay)	6	20

7. RESTRICTIONS ASSOCIATED WITH VARIANCES TO SETBACK:

Applications for structures which are seeking authorization to be located closer than 62.5 feet from the near design edge of the Federal channel will be required, to the maximum extent practicable, to adhere to the design guidance provided below to reduce their potential impact to both the navigable capacity of the channel and the Federal Government's ability to maintain the channel. Site specific information will be reviewed when determining if there is a need to deviate from the design guidance provided below. See Appendix C for more information on these design restrictions.

a. The width of the structure may not exceed 1/3 of the width of the waterward edge of the lot, up to a maximum width of 40 feet. Any proposed structure larger than 40 feet in width which is located within 62.5 feet of the near edge of the federal channel is generally not approved; however, any exceptions to the 1/3 width will be evaluated on a case-by-case basis. The width of the structure will be determined by including any hoist, lift, mooring piling, roof overhang or any other structure used in conjunction with the mooring structure. Additionally, the terminal platform portion of the structure must be centered on the waterward edge of the lot to the

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SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

maximum extent practicable. The access pier may be located at any point along the landward side of the terminal platform.

b. Where local restrictions require that two adjacent lots share an access pier, the width restriction noted in Paragraph 7.a. will apply to each half of the structure. In no case will the total width of the shared structure be allowed to exceed 80 feet. The access pier must be centered on the shared property line.

c. A submerged bulkhead or wave break may not be allowed to be constructed such that any portion of the structure is located closer than 62.5 feet from the near design edge of the channel.

d. Riprap may not be allowed as a shoreline stabilization method when it is to be located closer than 62.5 feet from the near design edge of the channel.


8. Existing Structures: Generally, previously authorized, currently serviceable structures may be repaired or rehabilitated, in accordance with their Department of the Army authorization, such that they maintain the authorized footprint and do not extend beyond their authorized distance from the near design edge of the Federal channel provided that the structure is not to be put to uses differing from those uses specified or contemplated for in the original permit or the most recently authorized modification. A pre-construction notification is required and the Corps will make the determination of whether an existing structure is currently serviceable. Any modification, including minor deviations, will be reviewed pursuant to the setback guidance in effect at the time of the request.

In the event that a previously authorized, currently serviceable structure is destroyed by an act of nature or other event, a new application will be required which will be reviewed pursuant to the setback guidance in effect at the time of the request. It is possible that any authorized reconstruction will not be the same size or design as the original structure.

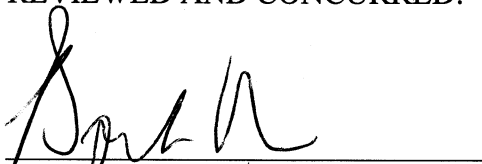
CESAJ (1145)

SUBJECT: U.S. Army Corps of Engineers, Jacksonville District, Setback Guidance for Structures along Certain Federal Channels

REVIEWED AND CONCURRED:


DONALD W. KINARD
Chief, Regulatory Division

REVIEWED AND CONCURRED:


DAVID S. HOBBIE
Chief, Project Management Division


REVIEWED AND CONCURRED:


AUDREY C. ORMEROD
Chief, Real Estate Division

REVIEWED AND CONCURRED:


LAUREEN A. BOROCHANIER
Chief, Engineering Division

APPROVED BY:


ALAN M. DODD
Colonel, Corps of Engineers
Commanding

Appendix A-1 Parameters for Horizontal (X&Y) and Hydrographic Surveys:

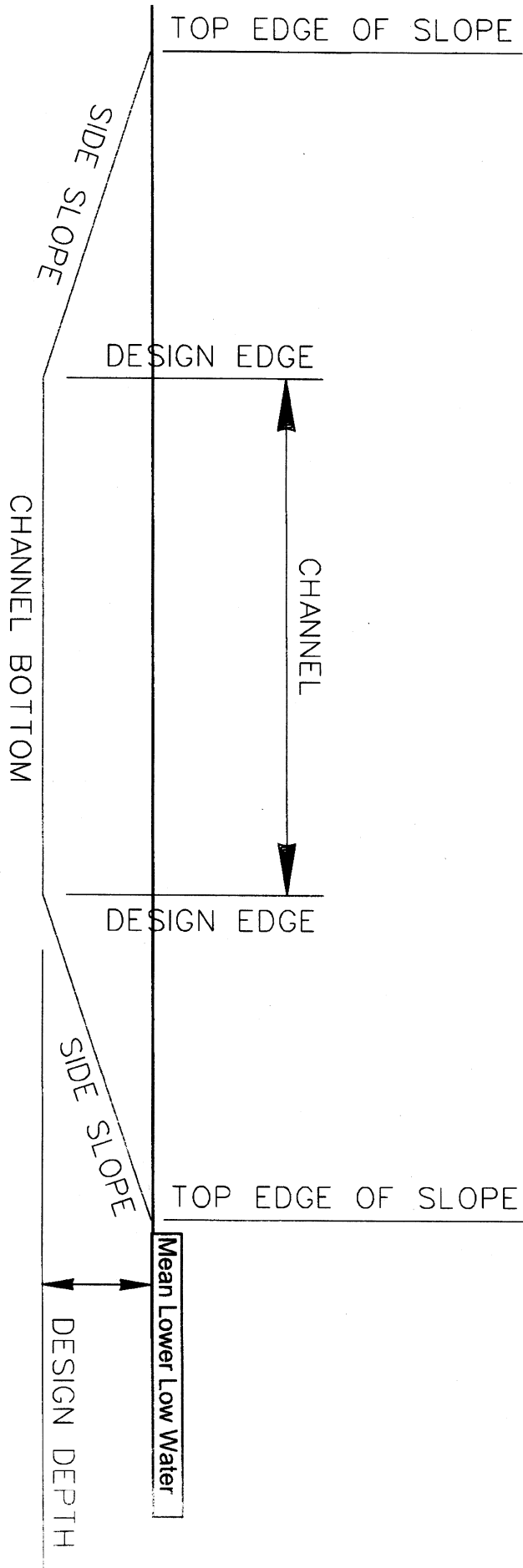
Horizontal (X&Y) Surveys:

1. All surveys must be conducted under the direction of a surveyor licensed under Florida Statute 472.
2. The survey shall include the following information:
 - The datum used to perform the survey must be clearly identified. As of the date of this document the Corps is requiring that the X&Ys be determined utilizing the High Accuracy Reference Network upgrade of the North American Datum of 1983 (NAD83(HARN)).
 - A description of the survey methodology used for the survey. This will include information pertaining to the equipment used (e.g. GPS units versus mechanical, type of GPS unit/s used, etc.) and any information which may affect the accuracy of the survey.
 - A statement of the degree of accuracy of the survey.
 - Any point provided in the survey shall be located with a minimum accuracy of ± 1 -foot horizontal.
 - A clear indication of the benchmark used to perform the survey. The benchmark must be a published National Geodetic Survey (NGS) monument and the coordinate values must be provided for the monument used.
 - A clear description of the physical location of the coordinate points. For existing structures, this needs to define where the coordinate point was determined. For proposed structures, this should reference clearly identifiable features of the proposed structure.
 - The survey must include at least two Points of Intersection (PI) points within the adjacent Federal project. The PI points should be located such that one is on either side of the proposed structure.
 - The survey must be dated and signed by the surveyor or the survey firm.

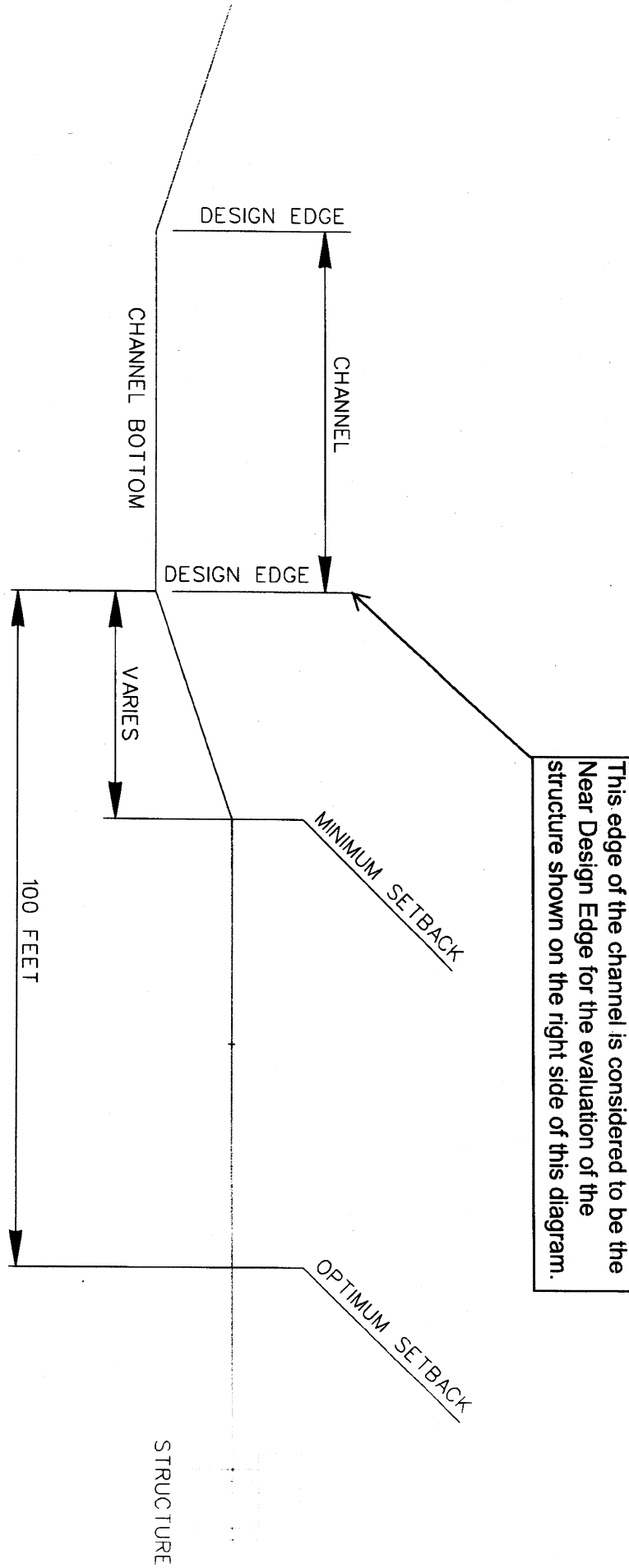
Appendix A-2 Parameters for Horizontal and Hydrographic Surveys:

Hydrographic Surveys:

1. All hydrographic surveys must be conducted by a professional engineer licensed under Florida Statute 471 or surveyor licensed under Florida Statute 472.
2. The survey should be of sufficient detail to determine the landward most area where water depth would be adequate to moor a vessel.
3. Soundings shall be provided in MLLW datum and have an accuracy of ± 0.5 foot vertical. Horizontal accuracy shall be ± 6 feet and clearly identify the 3-foot depth contour.
4. Surveys should be conducted perpendicular to the near design edge of the federal channel and should cover the distance between the highwater mark and the waterward most point of the new or rehabilitated structures.
5. Survey lines should be a maximum of 50' apart and soundings should be taken 10' apart along these survey lines. This survey should cover all areas that will be affected by new or rehabilitated structures.
6. Acoustic (fathometer) or mechanical (lead line/sounding pole) soundings will be acceptable if the accuracy requirements described above can be verified.
7. Surveyed points should be plotted in plan view.



APPENDIX B-1
SETBACK GUIDANCE MEMORANDUM
DIAGRAM OF TERMS



APPENDIX B-2
SETBACK GUIDANCE MEMORANDUM
DIAGRAM OF TERMS

NEAR
DESIGN
EDGE OF
channel

TOP
EDGE OF
SLOPE

62.5-foot
setback line

PROPERTY EDGE

50ft

33ft

PROPERTY EDGE

100ft

40ft

200ft

WATERWARD
EDGE OF LOT

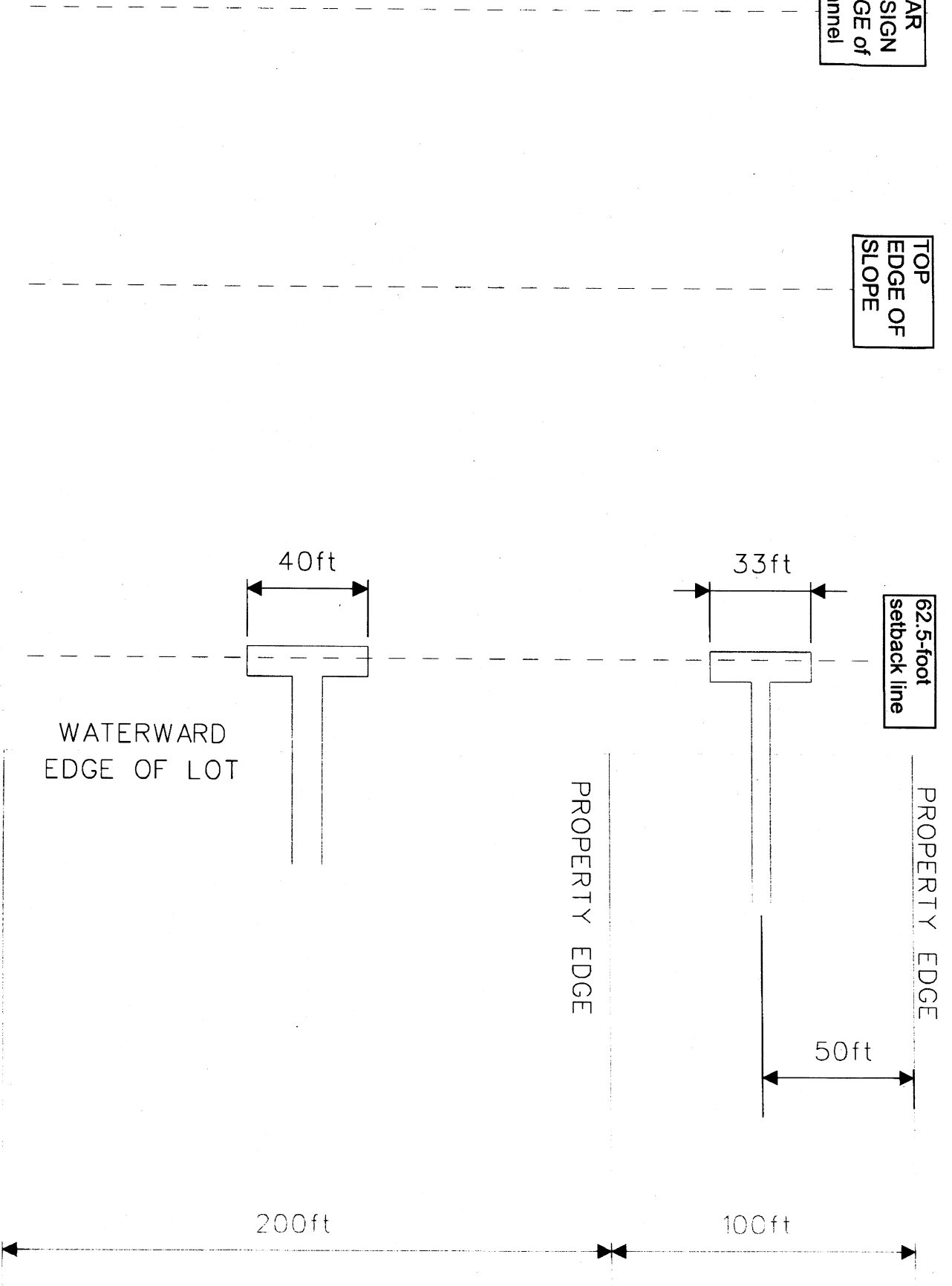
APPENDIX C

General example for structures that are to be located less than 62.5 feet from the Near Design Edge of the channel.

These structures **MUST** be centered on the property to the maximum extent practicable

The width is limited to 1/3 the property width up to a maximum of 40 feet.

While these examples show the access piers centered on the dock structures, they may be offset closer to one side or the other as long as they do not extend beyond the edge of the allowed width.



November 4, 2017

Mr. Mark Crosley
Executive Director
Florida Inland Navigation District (FIND)
1314 Marcinski Road
Jupiter, FL 33477

Re: Scope of Professional Engineering Services
Jupiter ICWW and Jupiter Inlet District (JID) Sand Trap Maintenance Dredging and Beach Fill
Placement Project; Palm Beach County, Florida

Mr. Crosley:

Taylor Engineering is pleased to submit the attached scope of services and cost proposal (Attachment A) for the above-referenced project. This proposal describes Taylor Engineering's effort to support maintenance dredging of Intracoastal Waterway (ICWW) Cuts P-1, P-2, P-3, and P-4 performed in conjunction with the JID annual dredging of the Jupiter Inlet sand trap. Through an interlocal agreement with FIND, the JID will solicit bids to enter into contract with a dredging contractor to excavate both the sand trap and the ICWW.

The Interlocal Agreement approved by the FIND Board on October 14, 2016, dictates that JID will act as the lead agency for the proposed dredging. Taylor Engineering will primarily coordinate with JID throughout bidding and construction of the project and JID will settle final costs with FIND at completion of the project, per the allocation in Attachment A. In Attachment A we present independent costs for JID- and FIND-related work efforts. The costs for Tasks 1 and 3 were divided proportionate to volume of sand to be dredged from the ICWW (FIND) and sand trap (JID). Based on historic and current survey volumes costs were allocated to FIND and JID at 56% and 44%, respectively. Table 1 below summarizes proposed tasks and corresponding costs to JID and FIND.

Table 1. Jupiter Sand Trap and ICWW Proposed Cost Summary

TASK	JID COST	FIND COST	TOTAL TASK COST
TASK 1 – CONSTRUCTION PLANS, SPECIFICATIONS, AND AGENCY COORDINATION	\$11,500	\$ 14,500	\$ 26,000
TASK 2 – BID ASSISTANCE	\$ 4,500	\$ 4,500	\$ 9,000
TASK 3 – CONSTRUCTION-PHASE SERVICES AND REPORTING	\$ 17,500	\$ 32,500	\$ 50,000
TOTAL	\$ 33,500	\$ 51,500	\$ 85,000

We propose to perform the FIND portion of the services detailed in Attachment A for a lump sum fee of \$51,500. Please contact me with any questions.

Sincerely,



Jerry Scarborough, P.E.
Senior Advisor, Waterfront Engineering

Attachments (1)
Figure (1)

ATTACHMENT A**2017 Jupiter Inlet Sand Trap and Intracoastal Waterway Dredging****Scope of Services and Cost Proposal**



P2016-129
C2016-049

August 10, 2016

Mr. Michael J. Grella
Executive Director
Jupiter Inlet District
400 North Delaware Blvd.
Jupiter, FL 33458

RE: 2017 Jupiter Inlet Sand Trap and Intracoastal Waterway Dredging Proposal

Dear Mr. Grella:

We are pleased to present the enclosed scope of services and cost proposal for the work referenced above. We propose to perform this work on a lump sum total of \$85,000.

Please let me know if you have any questions.

Sincerely,

Kenneth R. Craig, P.E.
Vice President of Coastal Engineering

Attachments



ATTACHMENT A

**SCOPE OF SERVICES
ENGINEERING SERVICES IN SUPPORT OF
MAINTENANCE DREDGING THE JUPITER INLET SAND TRAP AND INTRACOASTAL
WATERWAY
P2016-129**

INTRODUCTION

This scope of professional engineering services describes Taylor Engineering's effort to support the annual dredging of the Jupiter Inlet sand trap. The 2016 dredging project will also include a section of the Florida Inland Navigation District's (FIND) Intracoastal Waterway (ICWW). Through an interlocal agreement with FIND, the Jupiter Inlet District (JID) will solicit bids to enter into contract with a dredging contractor to excavate both the sand trap and the ICWW.

Taylor Engineering has developed this scope of services based on the following assumptions:

- JID and FIND will develop an interlocal agreement such that FIND will reimburse JID for all dredging contract work related to the ICWW.
- The ICWW dredging project extends between CR 707 and the U. S. Highway 1 bridges (in vicinity of Jupiter Inlet, including portions of cuts P-1, P-2, P-3, and P-4 (Section 23, Township 40 South, and Range 43 East). The contractor will deposit the dredged material on the shoreline between Florida Department of Environmental Protection (FDEP) reference monuments R-13 and R-19. Dredged material will meet beach placement requirements.
- The ICWW maintenance dredging portion of the project will be authorized by the U.S. Army Corps of Engineers (USACE) under FIND's Regional General Permit 93 (RGP 93) for maintenance dredging of the ICWW.
- FIND holds an FDEP permit to complete the ICWW dredging. We assume submittal requirements in this permit are comparable to JID's FDEP permit submittal requirements.
- Contractor will conduct all contractor-responsible environmental monitoring and surveys.
- Taylor Engineering will not conduct sea turtle or shorebird monitoring under the umbrella of this authorization.
- The existing JID contract will provide for all required beach placement area surveys.
- The beach placement template in JID's permits is the same as that in FIND's permits. We further assume that JID's beach placement template has enough capacity to hold all the material dredged from the ICWW in addition to all the material dredged from the sand trap.
- JID's contractor will conduct all necessary pre-, interim-, and post-construction bathymetric surveys required for objective review of contractor pay applications and for FDEP and USACE certification upon project completion.
- FIND will provide bathymetric and natural resources surveys within the ICWW.
- JID will act as the lead agency for the proposed dredging. As such, Taylor Engineering will primarily coordinate with JID throughout bidding and construction of the project. We have assumed only minimal coordination with FIND throughout the project.
- We present independent costs for JID- and FIND-related work efforts.

Should any of these assumptions prove incorrect, Taylor Engineering will work with JID to develop an appropriate additional scope of work and cost scope.



TASK 1 – CONSTRUCTION PLANS AND SPECIFICATIONS AND AGENCY COORDINATION

We will prepare contract documents and technical specifications for the dredging and beach placement work in Engineers Joint Contract Documents Committee (EJCDC) format. We will provide these documents to the Jupiter Inlet District (JID) in both paper and electronic media formats. Technical specifications will address dredging, beach placement, and environmental monitoring efforts required by the permits.

Taylor Engineering will use 2016 survey data provided by JID (collected by Lidberg Land Surveying, Inc.) and FIND to develop a digital terrain model and estimate quantities of dredged material available within the Jupiter Inlet sand trap and neighboring ICWW. Taylor Engineering will then prepare dredging construction drawings that incorporate plan and cross section views and show the location, extent, and depth of dredging required. Taylor Engineering will design the beach placement template to accommodate the calculated volume of dredged material from the trap and the ICWW. We will specify the berm elevation, berm width, and beach slope to maximize the beneficial use of sand for the present conditions while maintaining a fairly uniform template. Taylor Engineering will produce construction plan and cross section views of the beach placement area.

Every maintenance dredging event requires a Notice to Proceed (NTP) from the Florida Department of Environmental Protection (FDEP). As per Specific Condition Nos. 1 and 2 of FDEP's Joint Coastal Permit (JCP) No. 0134395-001-JC, JID must submit the following information to receive this NTP.

- One copy of final, signed and sealed plans, specifications, and construction operations plan (11" x 17" or larger)
- Final plans and specifications – electronic copies (AutoCAD format)
- Turbidity monitoring qualifications
- Hydrographic monitoring plan
- Written request for FDEP to review proposed dredging event
- Grain size analyses of surface grab samples within the sand trap

The construction plans will include mapping of any significant natural resources (e.g., hardbottom, rock outcroppings, and algal and seagrass beds) and a description of beach construction method. Taylor Engineering will collect three surface sediment samples from within the sand trap and send them off for grain size/carbonate analysis. We will obtain the contractor's turbidity monitoring qualifications and submit those to the FDEP. We will prepare a hydrographic monitoring plan that meets JCP Modification No. 0134395-008-JN, Monitoring Condition No. 2. Finally, we will respond to questions received by FDEP.

Taylor Engineering will revise the draft documents as required based on JID's review and comments and provide an electronic and up to five hard copies (signed and sealed by a registered Florida Professional Engineer).

JID COST: \$11,500
 FIND COST: \$14,500
 TOTAL COST: \$26,000

**TASK 2 – BID ASSISTANCE**

Taylor Engineering will administer bidding and contractor selection on the behalf of JID. From our offices in West Palm Beach and Jacksonville, we will assist with administration of the bidding process. We will prepare, advertise, and distribute bid packages, make clarifications and interpretations of the project documents, and prepare addenda, if required. One of our engineers will attend the bid opening. We will assist with review of the bids received and provide JID with our recommendations for contractor selection. This work includes reviewing the submitted bid documents in conjunction with JID's legal counsel, checking work experience references of the responsible bidders, and preparing and transmitting a written recommendation for contractor selection. Our review of bid documents will focus on contractors' technical qualifications. Accordingly, it excludes review of contractors' financial or contractual arrangements.

JID COST: \$4,500
FIND COST: \$4,500
TOTAL COST: \$9,000

TASK 3 – CONSTRUCTION-PHASE SERVICES AND REPORTING

We assume the construction period will last about 110 calendar days (including mobilization and demobilization) and occur between January and April 2017. The contractor will dredge the ICWW first. We assume the contractor will have responsibility for all required material testing, all construction layout surveys and preparation of as-built drawings of the completed work. Additionally, we assume the construction contractor will have responsibility for water quality monitoring; monitoring of endangered, threatened, and other species of special concern; and all other activities required for compliance with permit conditions. Therefore, our scope of services and respective fees excludes any of these services. We propose to provide the following services:

Task 3a. Pre-Construction Services

This subtask includes the following work:

- Preparing for and conducting a pre-construction meeting;
- Reviewing and approving contractor pre-construction submittals;
- Making recommendation to JID for issuance of notice to proceed;
- Preparing for and conducting an onsite coordination meeting; and
- Preparing and submitting two agency work commencement notices.

Taylor Engineering staff will attend and conduct the preconstruction conference with the contractor, JID, and agency representatives (as per Specific Condition No. 4 of FDEP's JCP No. 0134395-001-JC). We also expect FIND to participate in this meeting. The preconstruction conference will focus on describing the project's permitting requirements and answering the contractor's questions concerning the technical aspects of the work. In addition, we will discuss the "ground rules" and other issues including lines of engineer and contractor authority, contract general and specific conditions, contract administration, progress payment, correspondence procedures, project schedule, submittal register, labor requirements, and general site safety. We will take detailed minutes of the preconstruction conference discussions. We assume that the preconstruction conference will occur at JID's office. This meeting will occur before the contractor commences with physical construction.

We will also attend and conduct a coordination meeting after the preconstruction conference and before starting physical construction. We will review any updated contractor submittals required before the contractor commences with construction. Updated submittals may include schedule of values, list of subcontractors, signature authority, construction schedule, submittal register, environmental protection plan, quality control plan, and accident prevention plan. The purposes of the meeting include achieving a mutual understanding with the contractor of required quality control; jointly reviewing submitted draft plans and resolving issues of concern; discussing project plans and specifications, schedule, and documentation; and establishing a good working relationship between the contractor quality control staff and quality assurance representatives.

Finally, we will prepare and submit two written commencement notices — to the FDEP (as per General Condition No. 9 of JCP No. 0134395-001-JC) and the U.S. Army Corps of Engineers (USACE) (as per Department of Army Permit No. SAJ-1989-506 [IP-LAO]) — that indicate expected construction start and end dates.

Task 3b. Observation during Construction

This task includes the following subtasks:

- Performing up to 15 field visits;
- Submitting weekly summary turbidity monitoring reports to the FDEP;
- Reviewing and approving revised contractor submittals; and
- Reviewing and approving pay applications.

We will assist JID with administration of the construction contract from our offices in West Palm Beach and Jacksonville and from the project site. In-office duties will include reviewing the contractor's shop drawings and submittals, reviewing contractor's daily dredge reports, reviewing pay applications, providing oversight of the progress of the work, and assisting with the preparation of change orders, if required. Additionally, we will review the releases of lien, contractor affidavits, and contractor certifications.

We will remain available throughout construction to provide advice and consultation to JID through site visits and teleconferences. In that role, we will address questions pertaining to engineering, design, permitting issues, and any proposed changes to the project design.

Taylor Engineering's project engineers will also make up to 15 visits (average of 1 per week) to the project site to ascertain whether work is proceeding in general conformance with permit conditions and plans and specifications. Our visits will include observation of the work and monitoring of the contractor's means, methods, and sequences. As a part of the daily observations, Taylor Engineering will review the contractor's daily construction report, which will become part of the project record. The report indicates the observer, weather conditions, date, personnel/visitors on site, the contractor's manpower (e.g., superintendent, field engineer, and laborers) and equipment (e.g., backhoe, bulldozer, and dredge), daily summary of events, and contractor representative and observer signature. These reports provide a log of construction progress.

During our field visits, we will discuss with the contractor the project progress and address questions pertaining to engineering, design, permitting issues, any proposed changes to the project design, and any conflicts. In addition, we will review changes to the construction schedule, site safety, problem prevention, maintaining contract quality for materials and workmanship, pending modifications, changes and substitutions, and other business, as appropriate. Finally, we will review the contractor's daily dredge reports to ensure reporting/monitoring of dredge cutterhead's vertical and horizontal positioning (Specific Condition No. 5 of JCP No. 0134395-001-JC).



We will notify JID should any permit violations, work stoppages, or conflicts occur and provide recommendations to JID as to how to proceed with the project. However, we will not direct the contractor's means and methods of construction. Taylor Engineering assumes no responsibility for jobsite safety.

Note that we may require additional fees if the project requires additional onsite visits to solve unanticipated problems or if construction goes longer than the assumed 110-day construction period.

Task 3c. Project Close-Out and Certification

This task includes the following subtasks:

- Developing preliminary and final punch lists;
- Certifying the project substantially complete;
- Attending final inspection and closeout meetings;
- Conducting final review/acceptance of field data;
- Certifying final completion of the project; and
- Preparing and submitting FDEP engineer's report

Upon JID receiving from the contractor a request to certify the project substantially complete, we will make an observation of the project to determine if substantially complete. If it is not, we will develop preliminary and final punch list items for the contractor to complete or correct. With concurrence from JID, we will transmit this list to the contractor for completion. Upon completion of outlined items, we will certify the project substantially complete.

We will collect and review the following information from the contractor before project closeout.

- Final pay application
- Final contractor certification
- Final as-built survey

We will coordinate and prepare permit-related submittals to the FDEP and USACE after completion of the project. These submittals include

- Turtle monitoring summary report (as per Department of Army permit Biological Opinion Terms and Conditions No. 11);
- As-Built Certification by Professional Engineer form and as-built drawings (as per Department of Army permit Special Condition No. 3);
- As-built written statement of project completion to the FDEP and one electronic copy of as-built drawings if substantial deviations from permitted plans (as per FDEP permit General Condition No. 11); and
- Engineering report to FDEP (as per FDEP permit Monitoring Condition No. 2 and Specific Condition No. 5) (one electronic copy of report, and one electronic copy of survey data)
 - Narrative description of maintenance dredging and beach placement activities
 - Construction period
 - Plan view map showing actual excavation and beach placement sites
 - Pre-dredging bathymetric contours in sand trap
 - Post-dredging bathymetric contours in sand trap



- Pre-dredging to post-dredging bathymetric change contours in sand trap
- Pre-dredging to post-dredging volume changes in sand trap
- Discussion of errors associated with surveys
- Daily record of dredge cutterhead vertical and horizontal positions

Finally, we will prepare and submit one hard copy and one electronic copy of the dredging records for this project to JID for its records.

JID COST: \$17,500
 FIND COST: \$32,500
 TOTAL COST: \$50,000

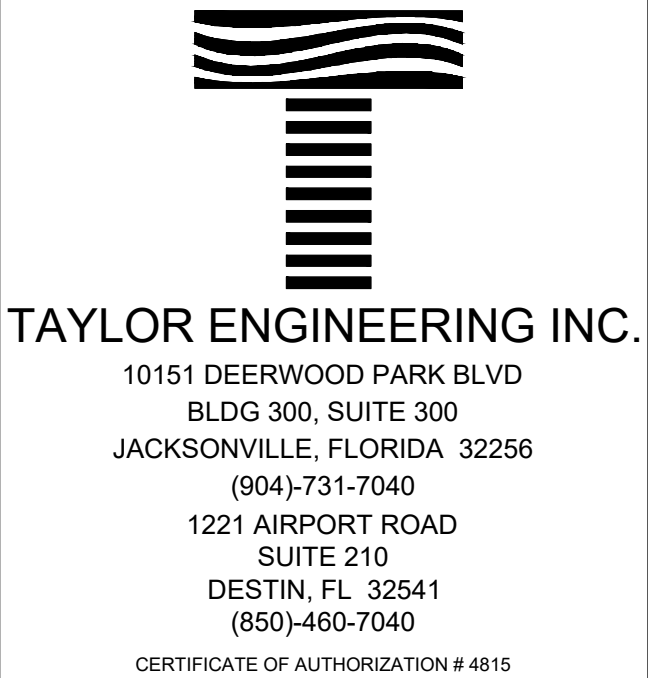
TOTAL COSTS

	JID	FIND	TOTAL
Task 1	\$11,500	\$14,500	\$26,000
Task 2	\$4,500	\$4,500	\$9,000
Task 3	\$17,500	\$32,500	\$50,000
Total	\$33,500	\$51,500	\$85,000

SCHEDULE

The figure below shows the proposed schedule for successful completion of the combined 2017 Jupiter Inlet sand trap and ICWW dredging.

Schedule - Combined Jupiter Inlet Sand Trap and ICWW Dredging - 2017													
Organization	Task	Jun-16	Jul-16	Aug-16	Sep-16	Oct-16	Nov-16	Dec-16	Jan-17	Feb-17	Mar-17	Apr-17	May-17
FIND/JID	Interlocal agreement												
JID	Sand trap survey												
JID (Taylor)	Prepare bid docs												
JID (Taylor)	Advertise												
JID (Taylor)	Award												
JID (Taylor)	ICWW Dredging												
JID (Taylor)	Sand trap Dredging												
JID (Taylor)	Project Closeout												



ENG
PROJECT TITLE

SUNSHINE STATE ONE CALL OF
FLORIDA
SUNSHINE 811 - ALWAYS CALL 811
BEFORE YOU DIG
1-800-638-4097

PRELIMINARY DRAWINGS: THESE DRAWINGS
ARE NOT IN FINAL FORM, BUT ARE BEING
TRANSMITTED FOR AGENCY REVIEW.

PROJECT NO	C2016-027
DATE	00-00-15
DESIGNED	DSGNBY
DRAWN	DRWNBY
CHECKED	CHECKBY
REVIEWED	REVBY
SCALE	AS SHOWN

C-2

SHEET 2 OF XX

**INTERLOCAL AGREEMENT BY AND BETWEEN THE
ST. AUGUSTINE PORT, WATERWAY AND BEACH DISTRICT
AND THE
FLORIDA INLAND NAVIGATION DISTRICT**

This AGREEMENT made and entered into this ____ day of _____, 2016, by and between the Florida Inland Navigation District, an independent special taxing District of the State of Florida (hereinafter the “District”), and the St. Augustine Port, Waterway and Beach District, also an independent special taxing District of the State of Florida (hereinafter the “SAPWBD”).

WITNESSETH

WHEREAS, the SAPWBD has requested that the District make available dredged material from District’s Site SJ-1 more particularly described in attached Exhibit A, (“the Site”) for closure of the inlet created by Hurricane Matthew along the Summer Haven beach (hereinafter the “Project”); and

WHEREAS, the District is willing to participate in the Project because of the benefit to the District and the Atlantic Intracoastal Waterway, subject to the terms and provisions of this Interlocal Agreement; and

WHEREAS, Section 163.01, Florida Statutes, authorizes local governments to make the most efficient use of their powers by enabling them to cooperate with other localities on a basis of mutual advantage and thereby to provide services and facilities that will harmonize geographic, economic, population, and other factors influencing the needs and development of local communities; and

NOW, THEREFORE, in consideration of the mutual representations, terms and covenants hereafter set forth, the parties agree as follows:

1. Effect of Recitals. The above recitals are incorporated by reference into the body of this Agreement.
2. Purpose. The purpose of this Agreement is to clarify the parties’ roles and obligations regarding the Project.
3. Responsibilities of the Parties.
 - a. Responsibilities of the District. The District agrees to allow the SAPWBD to remove dredged material for the Project from the Site at the SAPWBD’s expense. The District will provide the SAPWBD with any Site documents that the District may already have on hand and will endeavor to answer any technical questions regarding the Site to assist SAPWBD’s efforts.
 - b. Responsibilities of the SAPWBD. SAPWBD shall complete, or cause to be completed, the Project in an orderly and efficient manner. The SAPWBD shall restore the Site and the access road to the Site to their pre-Project condition unless otherwise agreed to by the District. The SAPWBD shall not excavate into the existing dikes nor lower than the basin’s constructed bottom elevation as shown on Exhibit B.
 - i. Prior to initiating the Project the SAPWBD will provide the District with a Dredged Material Removal Plan clearly indicating how the dredged material will

- be removed from the site, the equipment utilized for the removal, and shall contain a site plan showing all loading areas, haul routes, staging areas, and equipment storage areas.
- ii. The SAPWBD shall, through its agents and employees, prevent the unauthorized use of the District's property or any use thereof not in conformance with this Agreement.
 - iii. Any structures, improvements or signs constructed by the SAPWBD in accordance with a plan approved by the District shall be removed by the SAPWBD at the termination of this Agreement. No trees, other than non-native species, shall be removed or major land alterations done without the prior written approval of the District. Removable equipment and removable improvements placed on the Site by the SAPWBD which do not become a permanent part of the Site will remain property of the SAPWBD and shall be removed by the SAPWBD upon termination of this Agreement.
 - iv. In consideration for the privilege herein granted, the SAPWBD shall not claim any damages from the District in connection with, or on account of, the SAPWBD's performance under this Agreement; and, as between the parties, the SAPWBD shall be solely responsible for any injuries or damages arising in or on the Site while being used by the SAPWBD and its agents, representatives, and employees. The SAPWBD shall maintain a program of insurance covering its liabilities as prescribed by Section 768.28 Florida Statutes, and shall be responsible for the acts and omission of its officers, employees, representatives and agents in the event that such acts or omissions result in injury to persons or damage to property. The District does not warrant or represent that the Site is safe or suitable for the purpose for which the SAPWBD is permitted to use it, and the SAPWBD assumes all risks in its use.
 - v. The SAPWBD shall require any third-party contractors, engineers, consultants. Etc., to maintain additional insurance in the minimum coverage amount of one million dollars (\$1,000,000) for liability and workmen's compensation, which insurance shall provide that the District be an additional insured.
 - vi. The SAPWBD agrees that it will not do, or cause to be done, in, on, or upon the Site or as affecting said Site, any act which may results in damage or depreciation of value to the Site, or any part thereof.
 - vii. The SAPWBD shall perform the repair work as more particularly described in Addendum A attached hereto and made a part hereof by reference.
 - viii. The SAPWBD agrees that, during the term of this Agreement, the SAPWBD:
 - A. Shall keep or cause the Site to be kept free of hazardous wastes or substances. In no event shall the SAPWBD store or mix any such substances on the Site.
 - B. Shall not cause or permit, as a result of any intentional or unintentional act or omission on the part of the SAPWBD or any assignees, a release of hazardous wastes or substances onto the Site.
 - C. Shall comply with and ensure compliance by its employees and all others under its direction with all applicable federal, state, and local laws, ordinances, rules, and regulations.

- D. The terms “hazardous waste”, “hazardous substance”, “disposal”, “release”, and “threatened release”, if used in this Agreement, shall have the same meaning as set forth in the Comprehensive Environmental Response, Compensation, and Liability Act of 1980, as amended, 42 U.S.C. Section 9601, et seq. (“CERCLA”), the Superfund Amendments and Reauthorization Act of 1986, Pub. L. No. 99-499 (“SARA”), the Hazardous Materials Transportation Act, 49 U.S.C. Section 1801, et seq., the Resource Conservation and Recovery Act, 49 U.S.C. Section 6901 et seq., the Florida Resource and Management Act, Chapter 403, Florida Statutes, the Pollution, Spill, Prevention, and Control Act, Chapter 376, Florida Statue, or any other applicable state or federal laws, rules, or regulations adopted pursuant to any of the forgoing.
 - E. Shall immediately provide District with notice of any release or threatened release of hazardous waste within the Site and shall immediately provide District with notice of any injury or action taken by local, state, or federal governmental body with respect to hazardous waste within the Site.
 - F. Shall remove any hazardous waste or hazardous substances which exceed allowable levels in the ground or groundwater within the Site, arising from the SAPWBD’s use of the Site.
 - G. Fuel tanks must be placed in secondary containment, and all refueling must take place on an impervious surface.
4. Effective Date and Term. This Agreement shall take effect upon execution and shall terminate in one hundred and twenty (20) days after execution. Unless such time has been extended by the District.
 5. Compliance with Codes and Laws. The SAPWBD agrees to abide by all applicable laws, orders, rules, and regulations. The SAPWBD is also responsible for obtaining and abiding by federal, state, and local permits necessary for the development and completion of the Project.
 6. Independent Contractor. The parties agree that the District is an independent contractor and not an agent or servant of the SAPWBD. No person employed bay any party to this Agreement, shall in connection with the performance of this Agreement or any services or functions contemplated hereunder, at any time, be considered the employee of the other party, nor shall an employee claim any right in or entitlement to any pension, worker’s compensation benefit, unemployment compensation, civil service, or other employee rights or privileges granted by operation of law or otherwise, except through and against the entity by whom they are employed.
 7. Liability. The parties to this Agreement shall not be deemed to assume any liability for the negligence or wrongful acts, or omissions of the other party. Nothing contained herein shall be construed as a waiver, by either party, of the liability limits established in Section 768.28 Florida Statutes. The SAPWBD acknowledges that the District, its employees, commissioners and agents are solely providing the dredged material for the Project and are not involved in the design, construction, operation or maintenance of the Project.

8. Breach and Opportunity to Cure. The parties expressly covenant and agree that in the event either party is in default of its obligations under this Agreement, the party not in default shall provide to the defaulting party thirty (30) days written notice before exercising any of its rights.
9. Litigation Costs/Venue. In the event the District or the SAPWBD institutes any action or suit to enforce the provisions of this Agreement, each party shall be responsible for its own attorney's fees and costs at trial, appellate and post-judgment levels. The venue of any such litigation shall be had only in Palm Beach or St. Johns County, Florida, and any trial shall be nonjury.
10. Notice. Any notices required to be given under this Agreement shall be in writing and deemed sufficient to each party when sent by United States Mail, postage prepaid, to the following:

As to the District:
 Florida Inland Navigation District
 1314 Marcinski Road
 Jupiter, Florida 33477-9494
 Attn: Executive Director

As to the SAPWBD:
 St. Augustine Port, Waterway & Beach District
 P.O. Box 4512
 St. Augustine, FL 32085
 (904) 824-0113
 Attn: Chairman-Commissioner

11. Modification and Amendment. Except as expressly permitted herein to the contrary, no modification, amendment, or alteration in the terms or conditions contained herein shall be effective unless contained in a written document executed with the same formality and equality of dignity herewith.
12. Remedies. This Agreement shall be construed by and governed by the laws of the State of Florida. No remedy herein conferred upon by any party is intended to be exclusive of any other remedy, and each and every such remedy shall be cumulative and shall be in addition to every other remedy given hereunder or hereafter existing at law or in equity or by statute or otherwise. No single or partial exercise by any party of any right, power, or remedy hereunder shall preclude any other or further exercise thereof.
13. Joint preparation. The preparation of this Agreement has been a joint effort of the parties, and the resulting document shall not, solely as a matter of judicial construction, be constructed more severely against one of the parties than the other.
14. Captions. The captions and section designations herein set forth are for convenience only and shall have no substantive meaning.
15. Severability. In the event that any section, paragraph, sentence, clause, or provision hereof is held invalid by a court of competent jurisdiction, such holding shall not affect

the remaining portions of this Agreement and the same shall remain in full force and effect.

16. Assignment. The SAPWBD may not assign this Agreement or any interest hereunder without the express prior written consent of the District.
17. Entirety of Agreement. This Agreement represents the entire understanding between the parties, and supersedes all other negotiations, representations, or agreements, either written or oral, relating to this Agreement.
18. This Agreement may be executed in counterparts, all of which taken together shall be deemed an original, with a facsimile and/or an e-mail signature serving as an original thereof.

IN WITNESS WHEREOF, the parties hereto have caused these presents to be executed the day, month, and year aforesaid.

WITNESSES:

FLORIDA INLAND NAVIGATION
DISTRICT

By: _____
Executive Director

DATE: _____

WITNESSES:

ST. AUGUSTINE PORT,
WATERWAY AND BEACH
DISTRICT

By: _____
Chairman-Commissioner

DATE: _____

FLORIDA INLAND NAVIGATION DISTRICT

DREDGED MATERIAL MANAGEMENT AREA SJ-1

EROSION REPAIR ADDENDUM

SCOPE OF WORK TO BE PERFORMED:

The Florida Inland Navigation District (District) is establishing an Interlocal Agreement with the St. Augustine Port, Waterway and Beach District (the SAPWBD), whereby the SAPWBD desires to actively to mine material out of the District's SJ-1 Dredge Material Management Area (DMMA) for beach placement. Currently, there are approximately three (3) erosion-damaged washouts on this site. One of these washouts is believed to have been created during the last material mining activity (conducted by St. Johns County). When the previous lessee hardened the road surface through the placement of rock and road base, a substantial washout was created on the upper berm and the eastern berm road. The District is requesting that **this material be removed**, and all of the washouts be repaired as a condition of this new agreement. *(Please see Exhibit "A" for specific locations of referenced washouts.)* This would repair the integrity of the road and berm to its original post-construction condition, and allow a safe corridor for equipment to operate in and around the site during the sand mining process.

The District is requesting the repairs take place following the use of this site. At the end of the project, any new eroded areas which may have originated from the material removal event shall also be repaired prior to SAPWBD (or their Contractor) leaving the site. In addition, the haul road (Smith Grade Road) shall be graded to allow normal traffic ingress and egress throughout the entire project, and to maintain the common intersections throughout the extent of the road from US-1 to DMMA site SJ-1.

MATERIALS, LABOR AND TRANSPORTATION

The SAPWBD (or their Contractor) will furnish all equipment, labor, and materials, and transportation for this project. Parent soils shall be integrated into the repair of the erosion repair project. The materials that were asked to be removed can and shall be used as fill materials within the repairs of the washouts, but shall not be used as a road surface base.

PERMITS

Permits are not anticipated to be necessary for this work. If permits are found to be required, the SAPWBD (or their Contractor) shall obtain such permits and/or licenses at no cost to the District. In the event that a state, local or federal permit and /or license is required for the performance of this contract, and is applied for in good faith and is refused, neither the District nor the SAPWBD (or their Contractor) shall have an obligation to perform this work.

SUPERVISION

The SAPWBD and/or Contractor shall give prior notice to the District's Project Manager regarding the start date for this project. The Project Manager will make available personnel to assist in the refinement of project details, if needed.

INSURANCE

Activities of the Contractor or SAPWBD shall be that of an independent contractor and the District shall not be responsible for the actions of the Contractor or the SAPWBD in performing the work under this agreement. The Contractor shall furnish evidence of insurance including \$1,000,000 liability and workers' compensation coverage, consistent with the Interlocal Agreement.

FLORIDA INLAND NAVIGATION DISTRICT DREDGED MATERIAL MANAGEMENT AREA SJ-1 EROSION REPAIR ADDENDUM

SCOPE OF WORK TO BE PERFORMED:

The Florida Inland Navigation District (District) is establishing an Interlocal Agreement with the St. Augustine Port, Waterway and Beach District (Lessee), whereby the Lessee desires to actively to mine material out of the District's SJ-1 Dredge Material Management Area (DMMA) for beach placement. Currently, there are approximately three (3) erosion-damaged washouts on this site. One of these washouts is believed to have been created during the last material mining activity (conducted by St. Johns County). When the previous lessee hardened the road surface through the placement of rock and road base, a substantial washout was created on the upper berm and the eastern berm road. The District is requesting that **this material be removed**, and all of the washouts be repaired as a condition of this new agreement. (*Please see **Exhibit "A"** for specific locations of referenced washouts.*) This would repair the integrity of the road and berm to its original post-construction condition, and allow a safe corridor for equipment to operate in and around the site during the sand mining process.

The District is requesting the repairs take place following the use of this site. At the end of the project, any new eroded areas which may have originated from the material removal event shall also be repaired prior to Lessee (or their Contractor) leaving the site. In addition, the haul road (Smith Grade Road) shall be graded to allow normal traffic ingress and egress throughout the entire project, and to maintain the common intersections throughout the extent of the road from US-1 to DMMA site SJ-1.

MATERIALS, LABOR AND TRANSPORTATION

The Lessee (or their Contractor) will furnish all equipment, labor, and materials, and transportation for this project. Parent soils shall be integrated into the repair of the erosion repair project. The materials that were asked to be removed can and shall be used as fill materials within the repairs of the washouts, but shall not be used as a road surface base.

PERMITS

Permits are not anticipated to be necessary for this work. If permits are found to be required, the Lessee (or their Contractor) shall obtain such permits and/or licenses at no cost to the District. In the event that a state, local or federal permit and /or license is required for the performance of this contract, and is applied for in good faith and is refused, neither the District nor the Lessee (or their Contractor) shall have an obligation to perform this work.

SUPERVISION

The Lessee and / or Contractor shall give prior notice to the District's Project Manager regarding the start date for this project. The Project Manager will make available personnel to assist in the refinement of project details, if needed.

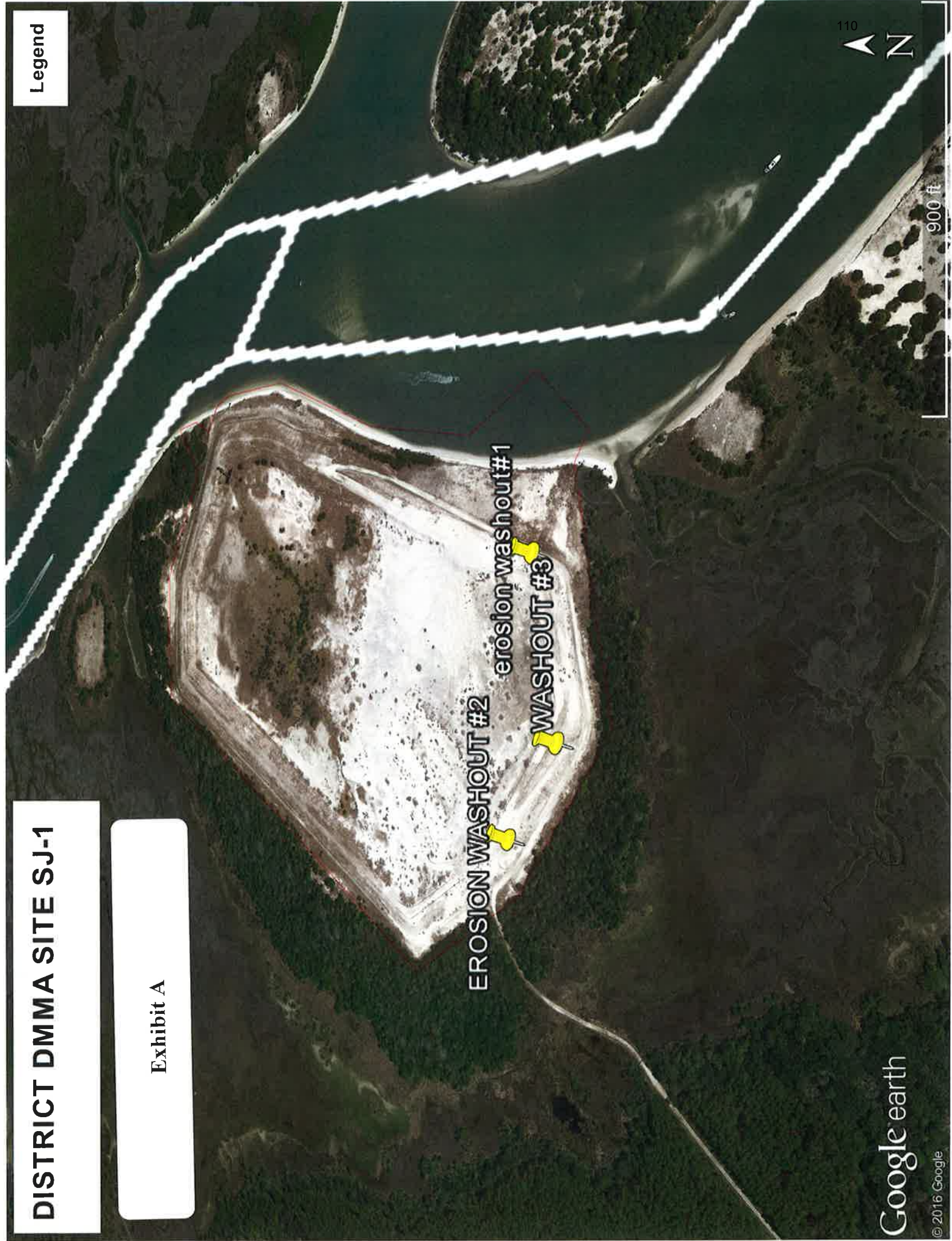
INSURANCE

Activities of the Contractor or Lessee shall be that of an independent contractor and the District shall not be responsible for the actions of the Contractor or Lessee in performing the work under this agreement. The Contractor shall furnish evidence of insurance including \$1,000,000 liability and workers' compensation coverage, consistent with the lease agreement.

DISTRICT DMMA SITE SJ-1

Exhibit A

Legend



110
N

900 ft

Google earth

© 2016 Google

FLORIDA INLAND NAVIGATION DISTRICT



COMMISSIONERS

DONALD J. CUOZZO
CHAIR
MARTIN COUNTY

JON NETTS
VICE-CHAIR
FLAGLER COUNTY

SUSANNE McCABE
TREASURER
VOLUSIA COUNTY

DON DONALDSON
SECRETARY
ST. LUCIE COUNTY

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MARK T. CROSLEY
EXECUTIVE DIRECTOR

JANET ZIMMERMAN
ASSISTANT EXECUTIVE
DIRECTOR

To: Commissioners
From: Mark Crosley, Executive Director
Date: October 3, 2016

Subject: Five-Year Dredging & DMMA Development Plan 2016

Staff has updated the District's Five-Year Dredging and Dredged Material Development Area (DMMA) Plan. This planning document is utilized to give staff guidance over the next five years for expected significant capital improvement projects that will need to occur to keep the Intracoastal Waterway maintained at optimum conditions.

As with any plan or schedule, this document is subject to change. Typical changes would likely result from varying waterway and DMMA site conditions, funding availability, other opportunities, and priorities.

This five-year plan is presented for review and discussion. In support of this document is an inventory of our public facilities, as required by law, and our engineer's preliminary analysis of the District's overall dredging and site construction needs.

Please contact me if you have any questions or need additional information.

**Intracoastal and Okeechobee Waterway
Dredging and Dredged Material Management Area Development
5 Year Schedule**

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Fiscal Year	Project & Primary	Phase	Estimated FIND Cost	Notes:
2016-2017	??	??	??	Potential project scheduled by opportunity
	NASSAU REACH I AIWW - FIND	Dredging	\$ 4,000,000	DMMA NA-1 available
	DMMA DU-2 - FIND	Weir Reconstruction	\$ 500,000	Ongoing
	DMMA DU-9 - FIND	Construction	\$ 1,100,000	Forthcoming (Yr 1 of 2)
	ST. AUGUSTINE IWW - USACE	Dredging	*	Ongoing
	MATANZAS - USACE	Dredging	*	Ongoing
	DMMA BV-4B - FIND	Construction	\$ 2,300,000	Forthcoming (1st year costs)
	DMMA BV-11 - FIND	P & S, Permitting	\$ 260,000	Forthcoming
	DMMA BV-24A - FIND	Permitting	\$ 160,000	Ongoing (2-yr project)
	ST. LUCIE REACH I - FIND	Dredging	\$ 2,200,000	Forthcoming
	DMMA M-8 - FIND	E/D	\$ 300,000	Initiating
	CROSSROADS DREDGE & SETTLING BASIN - FIND	Dredging	\$ 1,500,000	Pending
	DMMA O-7 - USACE	Construction	*	Ongoing
	DMMA O-23 - USACE	Permitting	*	
	JUPITER INLET IWW - USACE	P&S, Dredging	\$ 1,500,000	Forthcoming
	PB REACH IV DEEPENING - FIND	Plans & Specifications	\$ 300,000	Ongoing
	MSA 726 - FIND	Construction	\$ 250,000	Ongoing
	BROWARD REACH I - USACE	Plans & Specifications	*	Ongoing
	BROWARD RCH III DEEPEN - FIND	Dredging	\$ 8,000,000	Ongoing (2nd yr costs)
	BAKER'S HAULOVER IWW - USACE	P&S, Dredging	*???	USACE Shore Protection??
	DMMP UPDATES (2) - FIND	Study	\$ 210,000	Underway
		SUBTOTAL	\$ 22,580,000	
2017-2018				
	SAWPIT IWW - USACE	P & S, Dredging	\$ 3,100,000	Beach Placement
	Flagler-Reach I Dredge- FIND	Plans & Specifications	\$ 350,000	
	DMMA DU-9 - FIND	Construction	\$ 1,100,000	Ongoing (yr 2)
	PONCE INLET IWW - USACE/FIND	P & S, Permitting	\$ 750,000	(1st year costs)
	DMMA BV-4B - FIND	Construction	\$ 2,200,000	Ongoing (yr 2)
	DMMA BV-24A - FIND	Engineering, P & S	\$ 410,000	Ongoing (yr 2)
	DMMA O-23 - USACE	Construction	\$ 4,200,000	
	DMMA M-8 - FIND	Permitting & Construction	\$ 5,300,000	Follows E & D
	OWW REACH III - FIND/USACE?	Permitting	\$ 460,000	3-year project
	PB REACH IV DEEPENING - FIND	P & S, Permitting	\$ 20,000	Ongoing (Yr 2)
	BROWARD REACH I - USACE	Dredging	\$ 3,500,000	Ongoing (2nd year costs)
	NEARSHORE DISPOSAL - USACE	Permitting	\$ 175,000	Ongoing
	DMMP UPDATES (2) - FIND	Study	\$ 155,000	
		SUBTOTAL	\$ 21,720,000	

* Denotes previous FY funding OR 100% USACE Funding

**Intracoastal and Okeechobee Waterway
Dredging and Dredged Material Management Area Development
5 Year Schedule**

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Fiscal Year	Project & Primary	Phase	Estimated FIND Cost	Notes:
2018-2019				
	FLAGLER REACH 1 - FIND	Dredging	\$ 4,500,000	Follows comp. of DMMA FL-3
	PONCE INLET IWW - USACE	Dredging & offload	\$ 9,200,000	Offload MSA 434
	DMMA BV-11 - FIND/USACE?	Construction	\$ 4,345,000	
	DMMA BV-24A - FIND	Construction	\$ 3,400,000	
	DMMA IR-14 - USACE	Permitting, P & S	\$ 290,000	
	OWW REACH IV - FIND/USACE?	Permitting, P & S	\$ 80,000	Follows O-7 DMMA completion
	OWW ROUTE I AND II Dredge	Preliminary E/D	\$ 150,000	
	DMMA HD-1	E/D	\$ 150,000	OWW DMMA
	DMMA LT-4A	E/D	\$ 120,000	OWW DMMA
	PB REACH IV - Deepening	E/D & Permitting	\$ 730,000	Ongoing (Yr 3 of 3)
	DMMP UPDATES (2) - FIND	Study	\$ 175,000	Ongoing
		SUBTOTAL	\$ 23,140,000	
2019-2020				
	DMMA V-25 - FIND	Groundwater Modeling	\$ 380,000	Inland site w/ adj. development
	DMMA BV-R - USACE	Permitting, P & S	\$ 260,000	
	DMMA IR-14 - USACE	Construction	\$ 4,300,000	
	DMMA M-8 - FIND/USACE?	Construction	\$ 4,100,000	
	DMMA O-23 - USACE	Permitting, P & S	\$ 4,250,000	
	JUPITER INLET IWW - USACE	Permitting, P & S	\$ 190,000	
	OWW ROUTE II - FIND	Permitting, P & S	\$ 200,000	Cooperative proj. w/PBC?
	OWW REACH IV - FIND/USACE?	Dredging	\$ 1,500,000	Exemption/RGP
	BAKERS HAULOVER IWW - USACE	Permitting, P & S	\$ 110,000	Every 2-3 years
	DMMP UPDATES (2) - FIND	Study	\$ 175,000	Ongoing
		SUBTOTAL	\$ 15,465,000	
2020-2021				
	DMMA DU-20 - FIND	Permitting, P & S	\$ 286,000	To dredge DU Reach II & III
	MATANZAS - USACE	Plans & Spec	\$ 450,000	SJ-1 offload?
	DMMA FL-12 - FIND	Permitting, P & S	\$ 275,000	
	DMMA V-22A - FIND	Permitting, P & S	\$ 280,000	
	BREVARD RCH 1 (Haulover) - FIND	Permitting, P & S	\$ 296,000	Material to BV-2C
	CROSSROADS IWW - FIND	Permitting, P & S	\$ 425,000	Offload DMMA M-5
	OWW Reach III Dredge - FIND	Dredging	\$ 2,800,000	
	OWW ROUTE 2 - FIND	Dredging	\$ 2,000,000	Cooperative proj. w/PBC?
	JUPITER IWW - USACE	P & S, Construction	\$ 2,000,000	Every 3 years
	PB REACH IV DEEPEN - FIND	Dredging	\$ 7,200,000	
	BAKERS HAULOVER - USACE	Dredging	\$ 2,200,000	Every 3 years
	DMMP UPDATES (2) - FIND	Study	\$ 175,000	Ongoing
		SUBTOTAL	\$ 18,387,000	

* Denotes previous FY funding OR 100% USACE Funding

ALCALDE & FAY
GOVERNMENT & PUBLIC AFFAIRS CONSULTANTS

November 4, 2016

MEMORANDUM

TO: Mark Crosley, Executive Director
Janet Zimmerman, Assistant Executive Director

FROM: Jim Davenport

SUBJECT: Federal Legislative Report

FY 2017 FUNDING FOR THE INTRACOASTAL WATERWAY

Congress remains in recess until November 14th. Prior to adjourning to campaign for the elections, lawmakers approved a 10-week continuing resolution (CR) to fund the government through December 9th at FY 2016 funding levels. When Congress returns lawmakers will have roughly four weeks to complete work on the appropriations bills for FY 2017. Both Senate Majority Leader Mitch McConnell (R-KY) and House Speaker Paul Ryan (R-WI) have indicated that their preferred approach will be to group several funding bills into smaller packages, commonly referred to as a “Minibus”, instead of a much larger catch-all Omnibus spending bill.

While Congress works to wrap up appropriations legislation, we are circulating a letter to your congressional delegation seeking funding in the Army Corps of Engineers FY 17 Work Plan. As of the date of this memo, Representatives Lois Frankel, Patrick Murphy, Ted Deutch and Alcee Hastings have signed onto the letter.

Yesterday, we forwarded FIND’s letter, signed by Chairman Cuozzo, to Secretary Jo-Ellen Darcy to Stacey Brown and Jeff McKee, both of whom develop the Work Plan.

We will continue to update you on the appropriations process and our efforts to secure funding for the IWW in the Work Plan.

LEGISLATIVE LANGUAGE PERTAINING TO COMPENSATORY MITIGATION

On November 3 we met with the Matt Strickler, Minority Staff of the House Natural Resources Committee’s Subcommittee on Water, Power and Oceans to discuss Magnuson Stevens Reauthorization legislation in 2017. We reminded Matt of FIND’s concerns with

having to mitigate for essential fish habitat while maintaining the IWW. We provided him with the most recent iteration of our language that was reviewed by the Army Corps of Engineers Headquarters, which was drafted for the Water Resources Development Act.

Matt shared that there is no time frame, at this point, for reintroduction of Magnuson in 2017. He indicated that if the bill is reintroduced in its current form, he does not believe it would advance if Hillary Clinton is elected President. As you recall, President Obama stated that he would veto the current Magnuson bill. Matt also did not believe the House Magnuson bill would advance in the Senate in its current form.

Minority staff understands our issue, but still questions why the Jacksonville Corps does not simply override recommendations of the National Marine Fisheries Service (NMFS), which they are allowed to do. I informed him of the new Regional General Permit (RGP), and what we believe it will achieve, and argued that we want FIND to have the same rights and abilities as the Corps when FIND is carrying out the Corps work on a federal project.

We agreed to continue to discuss the issue in 2017. We will likely meet with the Water, Power and Oceans Subcommittee staff again when you are here in Washington.

We will continue to keep you apprised of our efforts.

FLORIDA INLAND NAVIGATION DISTRICT



The Honorable Jo-Ellen Darcy
Assistant Secretary of the Army for Civil Works
Headquarters, US Army Corps of Engineers
441 G. Street, NW
Washington, DC 20314-1000

October 21, 2016

Dear Secretary Darcy:

I am writing on behalf of the Florida Inland Navigation District (FIND), the local sponsor of the Intracoastal Waterway (ICW) in Florida, to request fiscal year 2017 funding in the U.S. Army Corps of Engineers (USACE) Work Plan for maintenance dredging projects on the ICW. This includes dredging projects at Sawpit (Nassau/Duval County), Crossroads (Martin County), and Broward Reach 1 (Broward County).

Maintenance dredging is required on the Atlantic Intracoastal Waterway (AIWW) in the vicinity of the Nassau/Duval County border in an area known as "Sawpit". The -12' federal navigation channel is consistently demonstrating depths as shallow as -6', and approximately \$3 M in federal funding is needed to match \$3M in FIND funding to complete this important project. This project also enjoys Regional Sediment Management (RSM) benefits by utilizing the beach-compatible dredge sand for shoreline protection on south Amelia Island.

Maintenance dredging is required in Martin County at the important confluence of the St. Lucie Inlet, the Intracoastal Waterway (IWW) and the Okeechobee Waterway (OWW), known as the "Crossroads". This -10' project is now at -5' and its maintenance is integral to the operations of each of these waterways. Federal funding in the amount of \$1M is requested to be matched with \$1M in FIND funding to complete this important project. This beach-compatible material will also realize RSM benefits by placement on Martin County beaches.

In Broward County Dredging Reach 1, located near the Hillsboro Inlet, shoaling has been documented by hydrographic surveys, and areas of the waterway are as shallow as -6 feet within the federal -10-foot authorized channel. The project cost estimate is \$2 million, of which FIND will provide \$1 million to match \$1M in federal funding. This is another project that enjoys RSM benefits from placement of beach-compatible dredged material on beaches in Broward County.

The continued maintenance of the IWW is vital to Florida's economy. The ICW annually transports over 500,000 recreational vessels; provides \$30 billion in economic output, including \$3 billion in personal wages and 155,000 jobs; generates \$540+ million in tax revenues; and increases property values by \$19.4 billion. Studies have shown that these benefits would be reduced by 50% if the waterway is not properly maintained.

As the USACE reviews projects that are ready to be executed in FY 2017, we respectfully request that you give strong consideration to funding Florida's ICW dredging projects.

Thank you for your time and assistance regarding these important requests.

Sincerely,

Don Cuozzo, Chair - FIND

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